

IDNIYRA 2018 YEARBOOK

INTERNATIONAL DN ICE YACHT RACING ASSOCIATION



Table of Contents

EARLY HISTORY OF THE IDNIYRA.....	2
CORPORATE OFFICERS.....	3
INTERNATIONAL CLASS OFFICERS.....	4
NORTH AMERICAN REGIONAL COMMODORES.....	5
TECHNICAL COMMITTEE MEMBERS.....	6
EUROPEAN NATIONAL SECRETARIES.....	7-8
NORTH AMERICAN & EUROPEAN PAST OFFICERS.....	9-24
GOLD CUP HISTORY.....	25-31
GOLD CUP PERPETUAL TROPHIES.....	32-45
SILVER CUP PERPETUAL TROPHY.....	46-47
NORTH AMERICAN CHAMPIONSHIP HISTORY.....	48-56
NORTH AMERICAN PERPETUAL TROPHIES.....	57-74
EUROPEAN CHAMPIONSHIP HISTORY.....	75-85
INTERNATIONAL RANK LIST.....	86-95
OFFICIAL SPECIFICATIONS.....	97-114
OFFICIAL SPECIFICATIONS INTERPRETATIONS.....	115-127
OFFICIAL PLANS.....	128-135
REGIONS OF THE NORTH AMERICAN IDNIYRA.....	136
IDNIYRA CONSTITUTION.....	137-141
IDNIYRA BY-LAWS.....	142-150
SPECIFICATION MANAGEMENT SYSTEM.....	151-153
ORGANISATION OF THE IDNIYRA-EUROPE.....	155-162
STANDARD SAILING INSTRUCTIONS.....	164-165
RACING RULES OF THE NATIONAL ICE BOAT AUTHORITY.....	166-175
NATIONAL ICE BOAT AUTHORITY APPENDIX V- PROTEST PROCEDURE.....	176-177
COURSE FOR ICE YACHT RACES.....	178-180
NATIONAL ICE BOAT AUTHORITY PROTEST FORM.....	181-183
RACING RULES OF THE NATIONAL ICE BOAT AUTHORITY INTERPRETATIONS.....	184
MEMBERSHIP IN AUSTRIA.....	185
MEMBERSHIP IN BELARUS.....	187
MEMBERSHIP IN CZECH REPUBLIC.....	188
MEMBERSHIP IN DENMARK.....	189-191
MEMBERSHIP IN ESTONIA.....	192
MEMBERSHIP IN FINLAND.....	193
MEMBERSHIP IN GERMANY.....	194-199
MEMBERSHIP IN HUNGARY.....	200
MEMBERSHIP IN LATVIA.....	201
MEMBERSHIP IN LITHUANIA.....	202
MEMBERSHIP IN THE NETHERLANDS.....	203-209
MEMBERSHIP IN NORWAY.....	210
MEMBERSHIP IN POLAND.....	211-214
MEMBERSHIP IN RUSSIA.....	215
MEMBERSHIP IN SWEDEN.....	216-217
MEMBERSHIP IN SWITZERLAND.....	218-219
MEMBERSHIP IN THE UNITED KINGDOM.....	220
NORTH AMERICAN MEMBERSHIP BY SAIL NUMBER.....	221-224
NORTH AMERICAN MEMBERSHIP BY LAST NAME.....	225-233
NORTH AMERICAN ICE YACHT CLUBS.....	234-236
SUPPLIERS.....	238-248
ADVERTISERS INDEX.....	248

IDNIYRA YEAR BOOK 2018

Revised February 8, 2018. Swedish Secretary corrected & Rank List corrected

Revised April 23, 2018. Darling Course illustration corrected.

EARLY HISTORY OF THE IDNIYRA

On February 21 and 22, 1953 the Detroit Ice Yacht Club sponsored a regatta to bring the DN skippers together to help form an association. A meeting of the skippers was held at this regatta and two representatives from each club present were chosen to outline a constitution for the association. North Star Sail Club was represented by John Liston and Fred Kagel; Anchor Bay Ice Yacht Club by Tom King and A. J. Walker; Detroit Ice Yacht Club by William Sarns and Clifford Cartwright. The representatives met at the home of the Cartwrights the following Sunday afternoon and briefly outlined a constitution. William Sarns was elected to serve as temporary secretary and to compose the constitution. The secretary prepared a constitution and sent out copies to the representatives in the following fall. They were returned to the secretary along with desired changes. The changes that the majority wanted were incorporated and 200 copies were printed. A copy was sent out to each individual who attended the first regatta and others who were interested. On January 17, 1954 the Northwestern Ice Yachting Association passed a resolution that they would accept the DN Association's specifications for their DN races which are now included in their annual regatta.

On January 7, 1954 the North Star Sail Club offered to be host for the annual regatta. There were no other offers so theirs was accepted. Sixty-two programs were sent out on February 1 for the regatta scheduled for February 20 and 21. On February 19 the regatta was transferred to Cass Lake because of poor ice at the North Star Club House on Lake St. Clair. The first five DN's in this regatta were: (1) Jack Moran, (2) Skip Boston, (3) Paul Eggert, (4) Bill Sporer, and (5) Bill Sarns.

The IDNIYRA was incorporated as a Non-Profit Corporation in the State of Michigan in 1962 for the following stated purpose.

"To promote, protect and perpetuate DN one-design iceboating, to advance the art and skill of the DN Ice Yacht construction and sailing as well as the future development of the DN Class Ice Yacht and the sport of ice yachting on all the hard waters of the world."

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Front cover photo

2017 Gold Cup Champion and European Champion Karol Jabłoński P36

Back cover photo

2017 North American Champion Ron Sherry US44

INTERNATIONAL CLASS OFFICERS

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TOLEDO ICE YACHT CLUB



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Join us at Ottawa River YC for fun after the sun sets.

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NORTH AMERICAN PAST OFFICERS

Past Commodores

1954 Fred Kagel	1977 Stan Nadler
1955 J. Robert Greene	1978 Doug Harvey
1956 Remi Deblaere	1979 Doug Harvey
1957 Ralph Templin	1980 Jan Gougeon
1958 Harold Kowitz	1981 Meade Gougeon
1959 William Sporer	1982 Tyler Keys
1960 William Sarns	1983 Tyler Keys
1961 Jane Pegel	1984 Tyler Keys
1962 B. Swinderman	1985 Meade Gougeon
1963 D. Southworth	1986 Meade Gougeon
1964 Charles Grant	1987 Pete Johns
1965 George Williams	1988 Paul Goodwin
1966 Tom McRitche	1989 Evert Vanderberg
1967 Gene Treuter	1990 Evert Vanderberg
1968 Reuben Snodgrass	1991 Evert Vanderberg
1969 Ray Pio	1992 Bob Schumacher
1970 John Young	1993 Bob Schumacher
1971 Jim Redding	1994 Bob Cummins
1972 Jim Redding	1995 Bob Cummins
1973 Arthur Teutsch	1996 Bill Condon
1974 Ted Sprague IV	1997 Bill Condon
1975 Ed Kraft	1998 Dan Bierman
1976 Ed Kraft	1999 Dan Bierman

NORTH AMERICAN PAST OFFICERS

Past Commodores

2000 Mark Kiefer	
2001 Mark Kiefer	
2002 Lou Loenneke	
2003 Lou Loenneke	
2004 Lou Loenneke	
2005 Andre Baby	
2006 Andre Baby	
2007 John Harper	
2008 John Harper	
2009 John Harper	
2010 Dan Connell	
2011 Dan Connell	
2012 Richard Potcova	
2013 Richard Potcova	
2014 Kent Baker	
2015 Kent Baker	
2016 Kent Baker	
2017 Eric Anderson	

NORTH AMERICAN PAST OFFICERS

Past Vice Commodores

1954 James M. Spencer Jr.	1977 Horst Rademacher
1955 James M. Spencer Jr.	1978 Jan C. Gougeon
1956	1979
1957 Ralph Templin	1980 Tyler Keys
1958 Harold Kowitz	1981 John Larson
1959 Bill Sporer	1982 Everett Vanderberg
1960 William Sarns	1983
1961 William Bentsen	1984
1962 Doug Southworth	1985
1963 Chuck Grant	1986
1964 George Hills	1987
1965 Tom McRitchie	1988 Doug MacFarland
1966 Gene Treuter	1989 Doug MacFarland
1967 Rueben Snodgrass	1990 Bob Schumacher
1968 Ray Pio	1991 Robert Cummins
1969 Jack Young	1992 Robert Cummins
1970 Jim Redding	1993 Robert Cummins
1971 Art Teutsch	1994 Bill Condon
1972 Art Teutsch	1995 Bill Condon
1973 Ted Sprague	1996 Dan Bierman
1974 Ted Sprague	1997
1975 Meade Gougeon	1998
1976 Stan Nadler	1999

NORTH AMERICAN PAST OFFICERS

Past Vice Commodores

2000	
2001	
2002	
2003	
2004 Andre Baby	
2005 John Harper	
2006 John Harper	
2007 Greg Cornelius	
2008 Danny Connell	
2009 Danny Connell	
2010 Rick Kaiser	
2011 Rick Kaiser	
2012 Kent Baker	
2013 Kent Baker	
2014 Eric Anderson	
2015 Eric Anderson	
2016 Eric Anderson	
2017 Warren Nethercote	

NORTH AMERICAN PAST OFFICERS

Past Secretaries

1953 William Sarns	1975 Doug Harvey
1954 William Sarns	1976 Doug Harvey
1955 William Sarns	1977 Doug Harvey
1956 William Sarns	1978 Doug Harvey
1957 William Sarns	1979 Pete Johns
1958 William Sarns	1980 Pete Johns
1959 Ralph Templin	1981 Pete Johns
1960 Ralph Templin	1982 Pete Johns
1961 William Bentsen	1983 Pete Johns
1961 Jack F.Young	1984 Pete Johns
1962 Jack F.Young	1985 Evert Vanderberg
1963 Jack F.Young	1986 Evert Vanderberg
1964 Jack F.Young	1987 Evert Vanderberg
1965 Jack F.Young	1988 Bob Dill
1966 Jack F.Young	1989 Bob Dill
1967 Jack F.Young	1990 Bob Dill
1968 Jack F.Young	1991 Bob Dill
1969 Jack F.Young	1992 Rich Cobb
1970 Tom McRitchie	1993 Lee Ann & Eric Armstrong
1971 Tom McRitchie	1994 Lee Ann & Eric Armstrong
1972 Bill Connell	1995 Lee Ann & Eric Armstrong
1973 Bill Connell	1996 Ron & Jane Sherry
1974 Bill Connell	1997 Ron & Jane Sherry

NORTH AMERICAN PAST OFFICERS

Past Secretaries

1998 Ron & Jane Sherry	
1999 Ron & Jane Sherry	
2000 Ron & Jane Sherry	
2001 Ted Bowen	
2002 Ted Bowen	
2003 Ted Bowen	
2004 Ted Bowen	
2005 Greg Cornelius	
2006 Greg Cornelius	
2007 Jim McDonagh	
2008 Jim McDonagh	
2009 Jim McDonagh	
2010 Jim McDonagh	
2011 Jim McDonagh	
2012 Jim McDonagh	
2013 Jim McDonagh	
2014 Geoff Sobering	
2015 Geoff Sobering	
2016 Geoff Sobering	
2017 Geoff Sobering	

NORTH AMERICAN PAST OFFICERS

Past Treasurers

1953 William Sarns	1975 Doug Harvey
1954 William Sarns	1976 Doug Harvey
1955 William Sarns	1977 Doug Harvey
1956 William Sarns	1978 Doug Harvey
1957 William Sarns	1979 Pete Johns
1958 William Sarns	1980 Pete Johns
1959 Ralph Templin	1981 Pete Johns
1960 Ralph Templin	1982 Pete Johns
1961 William Bentsen	1983 Pete Johns
1961 Jack F. Young	1984 Pete Johns
1962 Jack F. Young	1985 Evert Vanderberg
1963 Jack F. Young	1986 Evert Vanderberg
1964 Jack F. Young	1987 Evert Vanderberg
1965 Jack F. Young	1988 Bob Dill
1966 Jack F. Young	1989 Bob Dill
1967 Jack F. Young	1990 Mike Smith
1968 Jack F. Young	1991 Mike Smith
1969 Jack F. Young	1992 Mike Smith
1970 Tom McRitchie	1993 Ken Van Wagnen
1971 Tom McRitchie	1994 Ken Van Wagnen
1972 Bill Connell	1995 Ken Van Wagnen
1973 Bill Connell	1996 Ken Van Wagnen
1974 Bill Connell	1997 Ken Van Wagnen

NORTH AMERICAN PAST OFFICERS

Past Treasurers

1998 Aaron Stange	
1999 Aaron Stange	
2000 Debbie Goodwin	
2001 Debbie Goodwin	
2002 Debbie Goodwin	
2003 Paul Goodwin	
2004 Paul Goodwin	
2005 Rob (Bob) Holman	
2006 John Davenport	
2007 Rob (Bob) Holman	
2008 Rob (Bob) Holman	
2009 Rob (Bob) Holman	
2010 Geoff Sobering	
2011 Geoff Sobering	
2012 Geoff Sobering	
2013 Geoff Sobering	
2014 Wes Wilcox	
2015 Wes Wilcox	
2016 Wes Wilcox	
2017 Wes Wilcox	



DN World Cup Winner Mast
2014 2015 2016 2017



DN MASTS PLANKS RUNNERS SAILS
ALL FITTINGS FOR HULLS

Contact Peter Hamrak

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EUROPEAN PAST OFFICERS

Past Commodores

1965 Kees Kortenoever	1988 Wim van Acker
1966 Kees Kortenoever	1989 Wim van Acker
1967 Kees Kortenoever	1990 Wim van Acker
1968 Kees Kortenoever	1991 Wim van Acker
1969 Kees Kortenoever	1992 Peter Schellhorn
1970 Kees Kortenoever	1993 Peter Schellhorn
1971 Gerhard Jettmar	1994 Bart Reedijk
1972 Gerhard Jettmar	1995 Bart Reedijk
1973 Gerhard Jettmar	1996 Bart Reedijk
1974 Gerhard Jettmar	1997 Bart Reedijk
1975 Gerhard Jettmar	1998 Bart Reedijk
1976 Wim van Acker	1999 Bart Reedijk
1977 Wim van Acker	2000 Bart Reedijk
1978 Wim van Acker	2001 Hans Adolfsson
1979 Wim van Acker	2002 Hans Adolfsson
1980 Wim van Acker	2003 Hans Adolfsson
1981 Wim van Acker	2004 Hans Adolfsson
1982 Wim van Acker	2005 Rob Schutte
1983 Wim van Acker	2006 Niklas Müller-Hartburg
1984 Wim van Acker	2007 Niklas Müller-Hartburg
1985 Wim van Acker	2008 Niklas Müller-Hartburg
1986 Wim van Acker	2009 Niklas Müller-Hartburg
1987 Wim van Acker	2010 Niklas Müller-Hartburg

EUROPEAN PAST OFFICERS

Past Commodores

2011 Jörg Bohn	2016 Jörg Bohn
2012 Jörg Bohn	2017 Attila Pataki
2013 Jörg Bohn	
2014 Jörg Bohn	
2015 Jörg Bohn	



Karol Jabłoński P36

EUROPEAN PAST OFFICERS

Past Vice Commodores

1990 Peter Schellhorn	2013 Attila Pataki
1991 Peter Schellhorn	2014 Attila Pataki
1992 Bart Reedijk	2015 Attila Pataki
1993 Bart Reedijk	2016 Attila Pataki
1994 Andreas Müller-Hartburg	2017 Maciej Brosz
1995 Andreas Müller-Hartburg	
1996 Andreas Müller-Hartburg	
1997 Andreas Müller-Hartburg	
1998 Hans Adolfsson	
1999 Hans Adolfsson	
2000 Hans Adolfsson	
2001 Rob Schutte	
2002 Rob Schutte	
2003 Rob Schutte	
2004 Rob Schutte	
2005 Christian Seegers	
2006 Christian Seegers	
2007 Christian Seegers	
2008 Christian Seegers	
2009 Christian Seegers	
2010 Jörg Bohn	
2011 Attila Pataki	
2012 Attila Pataki	

EUROPEAN PAST OFFICERS

Past Secretaries

1992 Chris Williams	2015 Chris Williams
1993 Chris Williams	2016 Chris Williams
1994 Chris Williams	2017 Chris Williams
1995 Chris Williams	
1996 Chris Williams	
1997 Chris Williams	
1998 Chris Williams	
1999 Chris Williams	
2000 Chris Williams	
2001 Chris Williams	
2002 Chris Williams	
2003 Chris Williams	
2004 Chris Williams	
2005 Chris Williams	
2006 Chris Williams	
2007 Chris Williams	
2008 Chris Williams	
2009 Chris Williams	
2010 Chris Williams	
2011 Chris Williams	
2012 Chris Williams	
2013 Chris Williams	
2014 Chris Williams	

EUROPEAN PAST OFFICERS

Past Treasurers

1976 Wim van Acker	1999 Bart Reedijk
1977 Wim van Acker	2000 Bart Reedijk
1978 Wim van Acker	2001 Bart Reedijk
1979 Wim van Acker	2002 Rob Schutte
1980 Wim van Acker	2003 Rob Schutte
1981 Wim van Acker	2004 Rob Schutte
1982 Wim van Acker	2005 Rob Schutte
1983 Wim van Acker	2006 Christian Seegers
1984 Wim van Acker	2007 Christian Seegers
1985 Wim van Acker	2008 Christian Seegers
1986 Wim van Acker	2009 Christian Seegers
1987 Wim van Acker	2010 Christian Seegers
1988 Wim van Acker	2011 Christian Seegers
1989 Wim van Acker	2012 Christian Seegers
1990 Wim van Acker	2013 Christian Seegers
1991 Wim van Acker	2014 Christian Seegers
1992 Peter Schellhorn	2015 Christian Seegers
1993 Peter Schellhorn	2016 Christian Seegers
1994 Bart Reedijk	2017 Jerzy Henke
1995 Bart Reedijk	
1996 Bart Reedijk	
1997 Bart Reedijk	
1998 Bart Reedijk	

EUROPEAN PAST OFFICERS

Past Webmaster

2005 Dietmar Gottke	2013 Dietmar Gottke
2006 Dietmar Gottke	2014 Dietmar Gottke
2007 Dietmar Gottke	2015 Dietmar Gottke
2008 Dietmar Gottke	2016 Dietmar Gottke
2009 Dietmar Gottke	2017 Dietmar Gottke
2010 Dietmar Gottke	
2011 Dietmar Gottke	
2012 Dietmar Gottke	



2017 Gold Cup line up. Photo: Peter Johanson

EUROPEAN PAST OFFICERS

Past Insurance Manager

2010 Niklas Müller-Hartburg	
2011 Niklas Müller-Hartburg	
2012 Niklas Müller-Hartburg	
2013 Niklas Müller-Hartburg	
2014 Niklas Müller-Hartburg	
2015 Niklas Müller-Hartburg	
2016 Niklas Müller-Hartburg	
2017 Niklas Müller-Hartburg	

EUROPEAN PAST OFFICERS

Past Junior Programme Manager

2001 Stan Macur	2012 Stan Macur
2002 Stan Macur	2013 Stan Macur
2003 Stan Macur	2014 Stan Macur
2004 Stan Macur	2015 Jerzy Sukow
2005 Stan Macur	2016 Jerzy Sukow
2006 Stan Macur	2017 Jerzy Sukow
2007 Stan Macur	
2008 Stan Macur	
2009 Stan Macur	
2010 Stan Macur	
2011 Stan Macur	

GOLD CUP WORLD DN CHAMPIONSHIP

The Gold Cup World DN Championship, sailed alternate years in North America and Europe, has become the most prestigious regatta in iceboat racing. This race was formulated at a meeting held in Sweden at the time of the 1972 Europameisterschaft. The IDNIYRA was represented at these first planning meetings by Vice Commodore Art Teutsch and Secretary/Treasurer Bill Connell. The details were worked out later by Art Teutsch, representing North America, and Willi Schmidl, representing Europe.

1973: The first Gold Cup DN World Championship was organized by Commodore Art Teutsch with the help of Bill Connell, the DNIYC of Detroit, and the Cass Lake Yacht Club. John Bush was race chairman. Everyone had hoped the first Gold Cup would be sailed on Lake Saint Clair, the birthplace of the DN. However, as luck would have it, the regatta had to be moved to Gull Lake for better ice conditions.

1974: The second Gold Cup was scheduled for Lake Steinhude, West Germany in January. Again the primary site had no ice. The race was moved to Warsaw, Poland and sailed in very good conditions.

1975: This Gold Cup, scheduled for Geneva Lake, Wisconsin on February 7, 8 and 9 was cancelled because of a snowfall on February 5. The race was sailed the following week on the shifting and treacherous ice of Saginaw Bay.

1976: The Gold Cup World DN Championship was planned for Neusiedler See, Austria, January 12 - 16. However, it was a warm January in Europe and the only ice was in Sweden. The regatta was sailed January 19 at Stora Värtan, Stockholm, Sweden.

1977: The North American winter was one of the coldest on record. Because of heavy snow cover on all northern ice, the race was moved from New Jersey to Saint Michaels, Maryland on Chesapeake Bay. Near perfect ice with very light wind and a fleet of 55 DNs made up all the elements for a really great race.

1978: The regatta was sailed at Krynica Morska, Poland. The regatta was very well organized with hot soup being served on the ice at noon. The wind stayed away until the last day when 3 Gold Cup races were sailed in the morning followed by 3 European Championship races in the afternoon. This was done with three race courses, one for each fleet!

1979: The regatta, after a snowed-out start in Sandusky, Ohio, was completed on great ice at Lake Champlain, Vermont. Approximately seventy boats made the second long trip to Vermont.

1980: The regatta was held on Lake Mälaren in Västervik, Sweden.

GOLD CUP WORLD DN CHAMPIONSHIP

1981: The Gold Cup was held at Hamilton Bay, Ontario, Canada on the time scheduled and every one had a great ten days of ice boating.

1982: The Gold Cup's primary site was Steinhude, Germany but at the last moment switched to Wittensee, Germany. Perfect ice conditions aided the German committee in getting off a fine regatta.

1983: The Gold Cup was scheduled for Jan.31 - Feb.5 at Sodus Bay, New York but moved to Trenton, Ontario. After considerable rain the regatta was completed. Many dampened spirits were brightened by the "auction" at the regatta conclusion.

1984: The Gold Cup was sailed at Krynica Morska, Poland. The race was extremely well organized. Hot soup was served on the ice at noon. The ice was good quality salt ice. The winds were moderate.

1985: The Gold Cup was scheduled for Green Lake, Wisconsin. Snow dictated a move to Traverse City, Michigan. Light wind, cold weather and ultimately more snow sent the regatta to Barnegat Bay, New Jersey. The first two days at Barnegat saw 40 mph winds. FINALLY on Sunday the regatta was sailed in near perfect conditions. The course was huge; the wind was 12- 18mph; the ice was hard flat salt ice with a pebbly surface. Five races in each fleet were sailed without problems. A perfect ending to a difficult regatta.

1986: The Gold Cup was scheduled for Neusiedler See, Austria. Warm weather led to a move to Germany. Warm weather again caught up with the regatta. This time the race had to be abandoned.

1987: The regatta was scheduled for and sailed on Lake St Clair, Michigan. Two races were sailed in diminishing winds on Jan 27. The following day the remaining three races were sailed in 15-20 mph winds. Ice conditions were good, lumpy areas with black ice around them. 1987 marked the 50th year since the DN was originally designed.

1988: The Gold Cup was sailed in Leningrad, USSR on March 28. Conditions were wet with soft ice and moderate wind. Six races were sailed in the Gold Fleet over two days. It was a perfectly organized and exciting regatta. The outcome was not determined until the last 100 yards of the last race.

1989: The Gold Cup was sailed on Lake Champlain at Sandbar State Park, near Burlington, Vermont on January 23 and 24. 148 racers sailed in three fleets. Ice conditions were bumpy hard ice with about 30% coverage of thin snow drifts. The wind was moderate to light. The race was sailed in single digit conditions with high winds. Five races were sailed.

GOLD CUP WORLD DN CHAMPIONSHIP

1990: The regatta was sailed as the European Gold Cup World Championship as a result of a dispute over what specifications would be followed at the regatta. It had been planned for Poland but was moved for lack of ice. It was sailed in difficult conditions (very rough ice and very high wind) in Arsunda Sweden on March 6th to the 8th.

1991: The regatta was sailed on Saginaw Bay at Bay City Michigan. It was sailed over two days in medium wind on rapidly thawing ice.

1992: The regatta was again held near Arsunda, as a result of a warm winter in Germany.

1993: The Gold Cup was sailed at Lake Geneva, Wisconsin on Geneva Lake at Buttons Bay. The surface of the ice was slightly bumpy, but firm snow ice. The first day was characterized by very puffy wind conditions with large shifts, and by the end of the day everyone's mast was either broken, or well tested. (Twenty-three change of equipment forms on the first day were to change broken masts.) The second day the wind got very light the Gold Fleet coasted quietly around the course before the wind stopped for the day. The third scheduled race day the wind was howling again, and only a few people with small sails braved to try to sail. At 1500 hours the race committee cancelled racing and Jeff Kent was our new World Champion!

1994: Originally scheduled for Gizycko, Poland this regatta was moved 120 kilometers south because of too much snow on the original race site. The new race site offered good ice, but very little wind for almost a full week while the frustrated sailors waited on the starting line. Each day there was some racing in each fleet in light and shifty winds. After completing five races, Andreas Bock had won the Gold Cup and the North German Fleet had a strong showing in all fleets.

1995: The Gold Cup was sailed on Lac St. Louis in Montreal, Canada, due to poor ice conditions at the original location of Lake Canandaigua in New York. The ice was almost fifty percent snow covered which made the port side of the race course heavily favored. Early racing was dominated by the Swedish team, but Karol Jabłoński persisted and won his second World Championship.

1996: Monday, January 22nd, 173 entries started the World's in light ESE winds. Tuesday, moderate to strong wind and an overcast sky spelled a weather change. In the afternoon on the next day, it started to snow. By evening, it became clear that Neusiedler See, Austria, was unfit for iceboating. This enabled prize awarding at a planned dinner party as 4 races in both Gold and Silver, and 3 races in the Bronze fleet had been sailed.

GOLD CUP WORLD DN CHAMPIONSHIP

1997: The 1997 Gold Cup regatta was held on Lake St. Clair in Michigan January 14 through 17. The event was originally scheduled to be sailed on Green Lake in Wisconsin. Snow accumulation throughout the country made it difficult to select a new site. Due to extremely cold temperatures in the area, a new piece of ice formed on Anchor Bay in Michigan and it became the only sailable site in North America. There was an unfortunate start to racing when the all-terrain vehicle and trailer carrying the race committee went through the ice. Quick response from the people in the area resulted in a safe rescue with no injuries. Five races were held for Gold fleet with four races each held for Silver and Bronze. With 147 sailors participating, Karol Jabłoński won his fourth Gold Cup!

1998: The Gold Cup was held on Lake Pyhäjärvi in Lomasakyla, Finland. The first day of racing was on soft ice with 20 knots of wind. The mini-qualifiers and first race were held that day. The remainder of racing was on very cold, hard ice with 22 knots of wind. The course was 3 km long with the emphasis on top-end speed. The regatta was well organized with race headquarters located on the ice where the boats were.

1999: The regatta was held on Lac St. Louis, Québec, Canada after the primary site on Lake Champlain was snowed out. The ice was thick and clear of snow, and a large course allowed for good competitive racing.

2000: The Gold Cup was sailed on Lake Hjälmaren, 110 km south-southwest of Stockholm, Sweden. More than 190 sailors showed up to race on the long courses. The first day the ice was wet and fast with 18 mph wind speed, and 7 races were sailed. The second day had too much wind to sail. On the third day the ice was dry and hard, but there was a delay waiting for the wind to build and only two races were sailed.

2001: The regatta was scheduled for Lake St. Clair in Michigan but unseasonably warm and wet weather combined to shrink and perforate a large sheet of clear ice and the event was moved north to Bay City on Saginaw Bay. The area was enormous, but the surface was pebbly snow and ice melt and the warm and calm weather resulted in a truncated series. Fog, light air and the sticky surface put a premium on boat speed management which gave the European contingent chance to show their familiarity with adverse conditions.

2002: The regatta was sailed at Haapsalu, Estonia, as a combined regatta for the World Championship and the European Championship.

2003: The regatta was sailed at Valcour, just south of Plattsburgh, New York on Lake Champlain. The usual shifty winds made things very difficult for the race committee but tenacity won out and the regatta saw a full complement of races in a gorgeous setting.

GOLD CUP WORLD DN CHAMPIONSHIP

2004: The regatta was sailed in Hungary on Lake Balaton. The race committee found ice near Gyenesdias.

2005: The regatta was sailed at Lake Mendota, Madison, Wisconsin, after attempting and moving from Lake Winnebago, Oshkosh, Wisconsin. Weather made things very difficult for the race committee, but tenacity won out and the regatta completed just before snow made the new site unsailable.

2006: Race #1 for all fleets was completed on Lake Muertz near Berlin, Germany. The fleet sailed their way home in a blizzard Sunday night and woke up to heavy snow cover on the ice. The regatta was completed in Sweden on Thursday at Lake Vaettern on 3-4 inch thick ice. Conditions were slow with some snow coverage and Michal Bruczynski became the first "second generation" champion. His father Piotr was declared "best coach". The Polish sailors in general showed there skills in these conditions.

2007: The regatta was held on the near perfect ice of Green Bay at Menominee, Michigan. Matt Struble prevailed over Tomas Lindgren in the last race to win the see-saw battle between the two. Conditions were fast and temperatures were mid twenties. Everyone sailed home into the sunset after a perfect event.

2008: As a result of warm weather in Poland the regatta was held on Lake Lipno in the Czech Republic. Upon arrival sailors found a smooth sheet of ice on a mountain lake. The regatta was characterized by greatly varying wind conditions and competitive racing.

2009: The ice on Torch Lake in Michigan went from 4" of crusty snow, to 2" of standing water, to perfect conditions for the Gold Cup. The first day was sunny with moderate wind on fast ice and the second day was overcast. Matt Struble successfully defended his 2009 World Championship over four-time World Champion Ron Sherry.

2010: Snow-fall after opening ceremonies at the original site of Lake Balaton in Hungary resulted in the regatta moving to Neusiedler See, a lake on the Austrian/Hungarian border. The starts were in Hungary and the weather mark north in Austria. Seven races were completed and Michał Burczyński won his second Gold Cup.

GOLD CUP WORLD DN CHAMPIONSHIP

2011: Competitors gathered at the original site in Menominee, Michigan but snow fall on registration day forced a relocation eight hours south to Senachwine Lake near Putnam, Illinois. The first day's light air only allowed for qualifiers but better winds the on the next day enabled five Gold, four Silver, and four Bronze fleet races to be completed. Going into the fifth and final Gold race Ron Sherry was one point behind Michał Burczyński. Ron Sherry won his fifth Gold Cup with five second place finishes. Michael Rehe won the Silver fleet, and Tim Dixon won the Bronze fleet.

2012: The start of the Gold Cup was postponed for a few days until the best site was found on Lake Hjälmaren near Orebro, Sweden. The first races were sailed on February 1 on sticky snow over hard ice in light air. The cold temperatures and humidity made for uncomfortable conditions. The regatta was called complete on February 2 after three races were sailed in the Gold fleet that same day. Tomasz Zakrzewski P55 of Poland won his first Gold Cup, Poul Jorgensen D156 of Denmark placed first in B fleet, Wolfgang Boettger G597 of Germany placed first in C fleet, and Evgeny Chernov R98 of Russia placed first in D fleet.

2013: Lack of conditions in the host Central region caused the Worlds to be relocated to Lake Pepin, the largest lake on the Mississippi, in Lake City, Minnesota. The first two races were sailed on Sunday, January 27, in tough conditions including snow, sleet, and rain. After a one day postponement due to no wind, the regatta concluded after four races on Tuesday, January 29 with Tomasz Zakrzewski P55 of Poland repeating as Gold Cup champion, his second title.

2014: Warm conditions in the host country, Poland, forced the regatta to be moved to the Baltic Sea in Haapsalu, Estonia. Conditions in Estonia were on the warm side with below freezing temperatures at night and above freezing during the day. The Bronze fleet qualifier race was sailed in very low visibility and eventually was thrown out and resailed the next day but that didn't stop competitors from sailing an entertaining match race series on a small bay in front of the hotel. The Gold Cup was sailed in warm, light air conditions and was dominated by Karol Jabłoński P36 who won his 8th world championship.

2015: The historic Kingston Yacht Club on Lake Ontario in Canada hosted the Gold Cup. There were miles and miles of black, hard, ice. The first day, the Silver qualifier was completed but the Gold was black flagged because the wind shifted and then died. On day two, racers experienced very cold temperatures in windy conditions and two races were sailed in both Gold and Silver fleets. Brutally cold temperatures forced a postponement on the third day but temperatures warmed the next day and the regatta was completed. Poland's Karol Jabłoński P36 repeated as Gold Cup champion for the 9th time and relative newcomer Chris Burger US5166 won the Silver fleet title.

GOLD CUP WORLD DN CHAMPIONSHIP

2016: The last season under Commodore Jörg Bohn was a tricky one; continuous Atlantic depressions over Europe left the southern mainland iceless and the Scandinavian countries covered in snow. Just in time, the right ice conditions were found on Lake Glan, south of Stockholm, Sweden. Even more weather systems came through all week which made for changing conditions and a warm week, while the ice held surprisingly well. Nobody came close to preventing a tenth world title for Karol Jabłoński .

2017: Lake Kegonsa in Madison, WI was the only sailable ice in North America. 50F temperatures, light air, and soft ice made for tough conditions on the first day of sailing. All 3 fleets were able to sail 3 races until conditions deteriorated. A wet and heavy snow fell overnight which ended the sailing on Kegonsa. Scouts searched desperately for another site but didn't find anything suitable in time to relocate the World championship. The Gold Cup was called complete and Karol Jabłoński continued his ice sailing dominance. He won the 3 Gold fleet races on the first day and carried the Gold Cup back to Poland for the 11th time.



2017 Gold Cup line up. Photo: Peter Johanson

PERPETUAL TROPHIES

GOLD CUP FIRST PLACE WORLD CHAMPIONSHIP

Donated by Jim Redding, USA

Year	Skipper	Home	Regatta Site
1973	Ain Vilde	Estonia, USSR	Gull Lake, Michigan
1974	Tim Woodhouse	Michigan, USA	Zegrze, Poland
1975	Jan Gougeon	Michigan, USA	Saginaw Bay, Michigan
1976	Romuald Knasiecki	Poland	Stora Värtan, Sweden
1977	Henry Bossett	New Jersey, USA	St. Michaels, Maryland
1978	Bogdan Kramer	Poland	Krynica Morska
1979	Piotr Burczyński	Poland	Lake Champlain, Vermont
1980	Matti Kuulmann	Estonia, USSR	Lake Mälaren Västerås, Sweden
1981	Henry Bossett	New Jersey, USA	Hamilton Bay, Ontario
1982	Jan Gougeon	Michigan, USA	Wittensee, Germany
1983	Henry Bossett	New Jersey, USA	Trenton, Ontario
1984	Tiit Haagma	Estonia, USSR	Krynica Morska, Poland
1985	Jan Gougeon	Michigan, USA	BarNEGAT Bay, New Jersey
1986	No Regatta		
1987	Mike O'Brien	New Jersey, USA	Lake St Clair, Michigan
1988	Mike O'Brien	New Jersey, USA	Leningrad, USSR
1989	Mike O'Brien	New Jersey, USA	Lake Champlain, Vermont
1990	Wladyslaw Stefanowicz	Poland	Arsunda, Sweden
1991	Jan Gougeon	Michigan, USA	Saginaw Bay, Michigan
1992	Karol Jabłoński	Germany	Arsunda, Sweden
1993	Jeff Kent	Massachusetts, USA	Geneva Lake, Wisconsin
1994	Andreas Bock	Germany	Niepolet, Poland
1995	Karol Jabłoński	Poland	Montreal, Québec, Canada
1996	Karol Jabłoński	Poland	Podersdorf, Austria
1997	Karol Jabłoński	Poland	Lake St. Clair, Michigan

PERPETUAL TROPHIES

GOLD CUP FIRST PLACE WORLD CHAMPIONSHIP

Donated by Jim Redding, USA

Year	Skipper	Home	Regatta Site
1998	Ron Sherry	Michigan, USA	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Ron Sherry	Michigan, USA	Lac St. Louis, Québec Canada
2000	Karol Jabłoński	Poland	Lake Hjälmaren, Sweden
2001	Karol Jabłoński	Poland	Saginaw Bay, Michigan
2002	Ron Sherry	Michigan, USA	Haapsalu, Estonia
2003	Karol Jabłoński	Poland	Lake Champlain, New York
2004	Thomas Karlsson	Sweden	Lake Balaton, Hungary
2005	Ron Sherry	Michigan, USA	Lake Mendota, Wisconsin
2006	Michał Burczyński	Poland	Mueritz, Germany & Lake Vaettern, Sweden
2007	Matt Struble	Michigan, USA	Green Bay, Menominee, Michigan
2008	Matt Struble	Michigan, USA	Lake Lipno, Czech Republic
2009	Matt Struble	Michigan, USA	Torch Lake, Michigan
2010	Michał Burczyński	Poland	Neusiedler See, Austria
2011	Ron Sherry	Michigan, USA	Senachwine Lake, Putnam, Illinois
2012	Tomasz Zakrzewski	Poland	Lake Hjälmaren, Orebro, Sweden
2013	Tomasz Zakrzewski	Poland	Lake Pepin, Lake City, Minnesota
2014	Karol Jabłoński	Poland	Haapsalu, Estonia
2015	Karol Jabłoński	Poland	Kingston, Ontario, Canada
2016	Karol Jabłoński	Poland	Lake Glan, Norrköping, Sweden
2017	Karol Jabłoński	Poland	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP SECOND PLACE—COMMODORE JIM REDDING MEMORIAL TROPHY

Donated by Art Teutsch and Bill Connell

Year	Skipper	Home	Regatta Site
1973	Randy Johnson	Michigan, USA	Gull Lake, Michigan
1974	Endel Vooremaa	Estonia, USSR	Zegrze, Poland
1975	J. R. Watson	Michigan, USA	Saginaw Bay, Michigan
1976	Ed Kraft	Michigan, USA	Stora Värtan, Sweden
1977	Zbignew Stanislawski	Poland	St. Michaels, Maryland
1978	Vello Kuusk	Estonia, USSR	Krynica Morska, Poland
1979	Harald Stuertz	West Germany	Lake Champlain, Vermont
1980	Kunno Kalk	Estonia, USSR	Lake Mälaren, Västerås, Sweden
1981	Jan Gougeon	Michigan, USA	Hamilton Bay, Ontario
1982	Ron Sherry	Michigan, USA	Wittensee, Germany
1983	Piotr Burczyński	Poland	Trenton, Ontario
1984	Vaiko Vooremaa	Estonia, USSR	Krynica Morska, Poland
1985	Mike O'Brien	New Jersey, USA	Barneget Bay, New Jersey
1986	No Regatta	.	
1987	Henry Bossett	New Jersey, USA	Lake St Clair, Michigan
1988	Piotr Burczyński	Poland	Leningrad, USSR
1989	Tiit Haagma	Estonia, USSR	Lake Champlain, Vermont
1990	Vaiko Vooremaa	Estonia, USSR	Arsunda, Sweden
1991	Ron Sherry	Michigan USA	Saginaw Bay, Michigan
1992	Peter Hill	Vermont, USA	Arsunda, Sweden
1993	Karol Jabłoński	Poland	Geneva Lake, Wisconsin
1994	Karol Jabłoński	Poland	Nieporet, Poland
1995	Piotr Burczyński	Poland	Montreal, Québec, Canada
1996	Thomas Karlsson	Sweden	Podersdorf, Austria

PERPETUAL TROPHIES

GOLD CUP SECOND PLACE—COMMODORE JIM REDDING MEMORIAL TROPHY

Donated by Art Teutsch and Bill Connell

Year	Skipper	Home	Regatta Site
1997	Ron Sherry	Michigan, USA	Lake St. Clair, Michigan
1998	Daan Schutte	Holland	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Bernd Zeiger	Germany	Lac St. Louis, Québec Canada
2000	Ron Sherry	Michigan, USA	Lake Hjälmaren, Sweden
2001	Bernd Zeiger	Germany	Saginaw Bay, Michigan
2002	Karol Jabłoński	Poland	Haapsalu, Estonia
2003	Thomas Karlsson	Sweden	Lake Champlain, New York
2004	Karol Joblonski	Poland Lake	Balaton, Hungary
2005	Tomas Lindgren	Sweden	Lake Mendota, Wisconsin
2006	Łukasz Zakrzewski	Poland Mueritz, Germany &	Lake Vaettern, Sweden
2007	Tomas Lindgren	Sweden	Green Bay, Menominee, Michigan
2008	Łukasz Zakzewski	Poland	Lake Lipno, Czech Republic
2009	Bernd Zeiger	Germany	Torch Lake, Michigan
2010	Adam Baranowski	Poland	Neusiedler See, Austria
2011	Michał Burczyński	Poland	Senachwine Lake, Putnam, Illinois
2012	Karl-Hannes Tagu	Estonia	Lake Hjälmaren, Orebro, Sweden
2013	Robert Graczyk	Poland	Lake Pepin, Lake City, Minnesota
2014	Michał Burczyński	Poland	Haapsalu, Estonia
2015	Robert Graczyk	Poland	Kingston, Ontario, Canada
2016	Michał Burczyński	Poland	Lake Glan, Norrköping, Sweden
2017	Matt Struble	California, USA	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP THIRD PLACE

Donated by Wim Van Acker, Holland

Year	Skipper	Home	Regatta Site
1973	Jan Gougeon	Michigan, USA	Gull Lake, Michigan
1974	Vello Kuusik	Estonia, USSR	Zegrze, Poland
1975	Tim Woodhouse	Michigan, USA	Saginaw Bay, Michigan
1976	Ed Kraft	Michigan, USA	Stora Värtan, Sweden
1977	Jan Gougeon	Michigan, USA	St. Michaels, Maryland
1978	Vello Kuusk	Estonia, USSR	Krynica Morska, Poland
1979	Stanislaw Macur	Poland	Lake Champlain, Vermont
1980	Kunno Kalk	Estonia, USSR	Lake Mälaren, Västerås, Sweden
1981	Piotr Burczyński	Poland	Hamilton Bay, Ontario
1982	Vaiko Vooramaa	Estonia, USSR	Wittensee, Germany
1983	Stanislaw Macur	Poland	Trenton, Ontario
1984	Matti Kuulmann	Estonia, USSR	Krynica Morska, Poland
1985	Bogdan Kramer	Poland	Barnagat Bay, New Jersey
1986	No Regatta		
1987	Stan Macur	Poland	Lake St Clair, Michigan
1988	Andreas Bock	West Germany	Leningrad, USSR
1989	Jan Gougeon	Michigan, USA	Lake Champlain, Vermont
1990*	Rene Kuulmann	Estonia, USSR	Arsunda Sweden
1991	Henry Bossett	New Jersey	Saginaw Bay, Michigan
1992	Ron Sherry	Michigan, USA	Arsunda, Sweden
1993	Ron Sherry	Michigan, USA	Geneva Lake, Wisconsin
1994	Bernd Zeiger	Germany	Niepozet, Poland
1995	Mike O'Brien	New Jersey, USA	Montreal, Québec, Canada
1996	Stefan Jarudd	Sweden	Podersdorf, Austria
1997	Bernd Zeiger	Germany	Lake St. Clair, Michigan
1998	Piotr Burczyński	Poland	Lake Pyhäjärvi, Lomasakyla, Finland

PERPETUAL TROPHIES

GOLD CUP THIRD PLACE

Donated by Wim Van Acker, Holland

Year	Skipper	Home	Regatta Site
1999	Michał Burczyński	Poland	Lac St. Louis, Québec Canada
2000	Åke Luks	Sweden	Lake Hjälmaren, Sweden
2001	Tomas Gross	Sweden	Saginaw Bay, Michigan
2002	Michał Burczyński	Poland	Haapsalu, Estonia
2003	Ron Sherry	Michigan, USA	Lake Champlain, New York
2004	Ron Sherry	Michigan, USA	Lake Balaton, Hungary
2005	Matt Struble	Michigan, USA	Lake Mendota, Wisconsin
2006	Matt Struble	Michigan, USA	Mueritz, Germany & Lake Vaettern, Sweden
2007	Bernd Zeiger	Germany	Green Bay, Menominee, Michigan
2008	Andreas Bock	Germany	Lake Lipno, Czech Republic
2009	Bernd Zieger	Germany	Torch Lake, Michigan
2010	Łukasz Zakrzewski	Poland	Neusiedler See, Austria
2011	Łukasz Zakrzewski	Poland	Senachwine Lake, Putnam, Illinois
2012	Hans Ebler-Hansen	Denmark	Lake Hjälmaren, Örebro, Sweden
2013	John Dennis	Minnesota, USA	Lake Pepin, Lake City, Minnesota
2014	Tomas Zakrzewski	Poland	Haapsalu, Estonia
2015	Michał Burczyński	Poland	Kingston, Ontario, Canada
2016	Vaiko Vooremaa	Estonia	Lake Glan, Norrköping, Sweden
2017	Michał Burczyński	Poland	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP FOURTH PLACE

*Donated by Evert Vanderberg US4 of Michigan
in Memory of Wim Van Acker H31 and Jan Eindhoven H55*

Year	Skipper	Home	Regatta Site
1973	Russ Johnson	Michigan, USA	Gull Lake, Michigan
1974	Ain Vilde	Estonia, USSR	Zegrze, Poland
1975	Ain Vilde	Estonia, USSR	Saginaw Bay, Michigan
1976	Rolf Andersson	Sweden	Stora Värtan, Sweden
1977	Wim Van Acker	Holland	St. Michaels, Maryland
1978	Jiri Saraskin	Estonia, USSR	Krynica Morska, Poland
1979	Bogdan Kramer	Poland	Lake Champlain, Vermont
1980	Endel Vooremaa	Estonia, USSR	Mälaren Lake, Vastersas Sweden
1981	Tim Woodhouse	Michigan, USA	Hamilton Bay, Ontario, Canada
1982	Goran Tell	Sweden	Wittensee, Germany
1983	Leon LeBeau	Michigan, USA	Trenton, Ontario, Canada
1984	Henry Bossett	New Jersey, USA	Krynica Morska, Poland
1985	John Larson	New Jersey, USA	Barnagat Bay, New Jersey
1986	No Regatta		
1987	Greg Smith	Michigan, USA	Lake St Clair, Michigan
1988	Wieslaw Marcinczyk	Poland	Leningrad, USSR
1989	Tie: Wladyslaw Stefanowicz, Poland & Ron Sherry, MI, USA Lake Champlain Vermont		
1990*	Tarmo Tuisk	Estonia, USSR	Arsunda, Sweden
1991	Mike O'Brien	New Jersey, USA	Saginaw Bay, Michigan
1992	Gunars Rozenbergs	Latvia	Arsunda, Sweden
1993	Henry Bossett	New Jersey, USA	Geneva Lake, Wisconsin
1994	Jeff Kent	Massachusetts, USA	Nieporet, Poland
1995	Stefan Jarudd	Sweden	Montreal, Québec, Canada

PERPETUAL TROPHIES

GOLD CUP FOURTH PLACE

*Donated by Evert Vanderberg US4 of Michigan
in Memory of Wim Van Acker H31 and Jan Eindhoven H55*

Year	Skipper	Home	Regatta Site
1996	Andreas Bock	Germany	Podersdorf, Austria
1997	Thomas Karlsson	Sweden	Lake St. Clair, Michigan
1998	Thomas Karlsson	Sweden	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Thomas Karlsson	Sweden	Lac St. Louis, Québec, Canada
2000	Thomas Karlsson	Sweden	Lake Hjälmaren, Sweden
2001	John Harper	Michigan, USA	Saginaw Bay, Michigan
2002	Tomas Lindgren	Sweden	Haapsalu, Estonia
2003	Bernd Zeiger	Germany	Lake Champlain, New York
2004	Joerg Bohn	Germany	Lake Balaton, Hungary
2005	Thomas Karlsson	Sweden	Lake Mendota, Wisconsin
2006	Jacob Schneider	Poland Mueritz, Germany & Lake Vaettern, Sweden	
2007	Ron Sherry	Michigan, USA	Green Bay, Menominee, Michigan
2008	Tomas Lindgren	Sweden	Lake Lipno, Czech Republic
2009	Michal Burcynski	Poland	Torch Lake, Michigan
2010	Bernd Zieger	Germany	Neusiedler See, Austria
2011	John Dennis	Minnesota, USA	Senachwine Lake, Putnam, Illinois
2012	Vaiko Vooremaa	Estonia	Lake Hjälmaren, Orebro, Sweden
2013	Ron Sherry	Michigan, USA	Lake Pepin, Lake City, Minnesota
2014	Vaiko Vooremaa	Estonia	Haapsalu, Estonia
2015	Ron Sherry	Michigan, USA	Kingston, Ontario, Canada
2016	Łukasz Zakrzewski	Poland	Lake Glan, Norrköping, Sweden
2017	Robert Graczyk	Poland	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP FIFTH PLACE

Donated by Stan Whorwood, Canada

Year	Skipper	Home	Regatta Site
1973	Helmut Leppik	Estonia, USSR	Gull Lake, Michigan
1974	Hans Molter	West Germany	Zegrze, Poland
1975	Endel Vooremaa	Estonia, USSR	Saginaw Bay, Michigan
1976	Piotr Burczyński	Poland	Stora Värtan, Sweden
1977	Ed Kraft	Michigan, USA	St. Michaels, Maryland
1978	Helmut Leppik	Estonia, USSR	Krynica Morska, Poland
1979	Henry P. Bossett	New Jersey, USA	Lake Champlain, Vermont
1980	Vaiko Vooremaa	Estonia, USSR	Lake Mälaren, Vastersas, Sweden
1981	Matti Kuulmann	Estonia, USSR	Hamilton Bay, Ontario
1982	Harald Stuertz	West Germany	Wittensee, Germany
1983	Jan Gougeon	Michigan, USA	Trenton, Ontario
1984	Bogdan Kramer	Poland	Krynica Morska, Poland
1985	Karol Jabłoński	Poland	Barnagat Bay, New Jersey
1986	No Regatta		
1987	Wladyslaw Stefanowicz	Poland	Lake St Clair, Michigan
1988	Stanislaw Macur	Poland	Leningrad, USSR
1989	Tie for fourth place (See fourth place)		Lake Champlain, Vermont
1990*	Matti Kuulmann	Estonia, USSR	Arsunda Sweden
1991	Meade Gougeon	Michigan, USA	Saginaw Bay, Michigan
1992	Rene Kuulmann	Estonia	Arsunda, Sweden
1993	Bernd Zeiger	Germany	Geneva Lake, Wisconsin
1994	Ake Luks	Sweden	Nieporet, Poland
1995	Tomas Lindgren	Sweden	Montreal, Québec, Canada
1996	Aare Kööp	Estonia	Podersdorf, Austria
1997	Ken VanWagnen	Ohio, USA	Lake St. Clair, Michigan
1998	Tomas Lindgren	Sweden	Lake Pyhäjärvi, Lomasakyla, Finland

PERPETUAL TROPHIES

GOLD CUP FIFTH PLACE

Donated by Stan Whorwood, Canada

Year	Skipper	Home	Regatta Site
1999	Tomas Lindgren	Sweden	Lac St. Louis, Québec Canada
2000	Mati Hool	Estonia	Lake Hjälmaren, Sweden
2001	Fredrik Lönegren	Sweden	Saginaw Bay, Michgian
2002	Thomas Karlsson	Sweden	Haapsalu, Estonia
2003	Tomas Lindgren	Sweden	Lake Champlain, New York
2004	Tomas Lindgren	Sweden	Lake Balaton, Hungary
2005	Jakob Schneider	Poland	Lake Mendota, Wisconsin
2006	Andreas Bock	Germany	Mueritz, Germany & Lake Vaettern, Sweden
2007	John Dennis	Minnesota, USA	Green Bay, Menominee, Michigan
2008	Ron Sherry	Michigan, USA	Lake Lipno, Czech Republic
2009	Łukasz Zakrzewski	Poland	Torch Lake, Michgian
2010	Vaiko Vooremaa	Estonia	Neusiedler See, Austria
2011	Dariusz Kardas	Poland	Senachwine Lake, Putnam, Illinois
2012	Ron Sherry	Michigan, USA,	Lake Hjälmaren, Orebro, Sweden
2013	James “T” Thieler	Rhode Island, USA	Lake Pepin, Lake City, Minnesota
2014	Martin Schneider	Germany	Haapsalu, Estonia
2015	Tomasz Zakrzewski	Poland	Kingston, Ontario, Canada
2016	Ron Sherry	Michigan, USA	Lake Glan, Norrköping, Sweden
2017	James “T” Thieler	Rhode Island, USA	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP SENIOR TROPHY

Donated by Skeeter Ice Boat Club, Williams Bay, Wisconsin, USA

Year	Skipper	Home	Regatta Site
1973	Wim Van Acker	Holland	Gull Lake, Michigan
1974	Wim Van Acker	Holland	Zegrze, Poland
1975	Doug Harvey	Michigan, USA	Saginaw Bay, Michigan
1976	Wim Van Acker	Holland	Stora Värtan, Sweden
1977	Wim Van Acker	Holland	St. Michaels, Maryland
1978	Wim Van Acker	Holland	Krynica Morska, Poland
1979	Ed Kraft	Michigan, USA	Lake Champlain, Vermont
1980	Wim Van Acker	Holland	Lake Mälaren, Västerås, Sweden
1981	Ed Kraft	Michigan, USA	Hamilton Bay, Ontario
1982	Jan Eindhoven	Holland	Wittensee, Germany
1983	John Larson	New Jersey, USA	Trenton, Ontario
1984	Missing Results		Krynica Morska, Poland
1985	John Larson	New Jersey, USA	Bamegat Bay, New Jersey
1986	No Regatta		
1987	John Larson	New Jersey, USA	Lake St Clair, Michigan
1988	Visvaldis Briedis	Latvia, USSR	Leningrad, USSR
1989	Meade Gougeon	Michigan, USA	Lake Champlain, Vermont
1990	Missing Results		Arsunda Sweden
1991	Meade Gougeon	Michigan USA	Saginaw Bay, Michigan
1992	Missing Results		Arsunda, Sweden
1993	Meade Gougeon	Michigan, USA	Geneva Lake, Wisconsin
1994	Missing Results		Nieporet, Poland
1995	Meade Gougeon	Michigan, USA	Montreal, Québec, Canada
1996	Bart Reedijk	Holland	Neusiedler See, Austria
1997	Jan Gougeon	Michigan, USA	Lake St Clair, Michigan

PERPETUAL TROPHIES

GOLD CUP SENIOR TROPHY

Donated by Skeeter Ice Boat Club, Williams Bay, Wisconsin, USA

Year	Skipper	Home	Regatta Site
1998	Missing Results		Lake Pyhäjärvi, Lomasakyla, Finland
1999	Piotr Burczyński	Poland	Lac St. Louis, Québec, Canada
2000	Piotr Burczyński	Poland	Lake Hjälmaren, Sweden
2001	Meade Gougeon	Michigan, USA	Saginaw Bay, Michigan
2002	Andre Baby	Canada	Haapsalu, Estonia
2003	Greg Smith	Michigan, USA	Lake Champlain, New York
2004	Andre Baby	Canada	Lake Balaton, Hungary
2005	John Harper	Michigan, USA	Lake Mendota, Wisconsin
2006	Missing Results	Mueritz, Germany &	Lake Vättern, Sweden
2007	Missing Results		Green Bay, Menominee, Michigan
2008	Missing Results		Lake Lipno, Czech Republic
2009	John Harper	Michigan, USA	Torch Lake, Michigan
2010	Adam Baranowski	Poland	Neusiedler See, Austria
2011	John Dennis	Minnesota, USA	Senachwine Lake, Putnam, Illinois
2012	Hans Ebler-Hansen	Denmark	Lake Hjälmaren, Örebro, Sweden
2013	John Dennis	Minnesota, USA	Lake Pepin, Lake City, Minnesota
2014	Karol Jabłoński	Poland	Haapsalu, Estonia
2015	Karol Jabłoński	Poland	Kingston, Ontario, Canada
2016	Karol Jabłoński	Poland	Lake Glan, Norrköping, Sweden
2017	Karol Jabłoński	Poland	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

GOLD CUP JUNIOR TROPHY

Donated by Robert Pegel, USA

Year	Skipper	Home	Regatta Site
1973	Bob Redner		Gull Lake, Michigan
1974	Missing Results		Zegrze, Poland
1975	Chuck Lamphere		Saginaw Bay, Michigan
1976	No information		Stora Värtan, Sweden
1977	Danny Connell	New Jersey, USA	St. Michaels, Maryland
1978	Vaiko Vooremaa	Estonia, USSR	Krynica Morska, Poland
1979	Ron Sherry	Michigan, USA	Lake Champlain, Vermont
1980	Vaiko Vooremaa	Estonia, USSR	Lake Mälaren, Västerås, Sweden
1981	Harry Melges III	Wisconsin, USA	Hamilton Bay, Ontario
1982	Missing Results		Wittensee, Germany
1983	Scott Button	Wisconsin, USA	Trenton, Ontario
1984	Missing Results		Krynica Morska, Poland
1985	Bruce Hay		Bamegat Bay, New Jersey
1986	No Regatta		
1987	Matt Struble	Michigan, USA	Lake St Clair, Michigan
1988	Rene Kuulmann	Estonia, USSR	Leningrad, USSR
1989	Matt Struble	Michigan, USA	Lake Champlain, Vermont
1990	Missing Results		Arsunda, Sweden
1991	Matt Struble	Michigan, USA	Saginaw Bay, Michigan
1992	Missing Results		Arsunda, Sweden
1993	Aaron Stange	Ohio, USA	Geneva Lake, Wisconsin
1994	Missing Results		Nieporet, Poland
1995	Missing Results		Montreal, Québec, Canada
1996	Kirill Kapustin	Russia	Neusiedler See, Austria
1997	Kirill Kapustin	Russia	Lake St Clair, Michigan

PERPETUAL TROPHIES

GOLD CUP JUNIOR TROPHY

Donated by Robert Pegel, USA

Year	Skipper	Home	Regatta Site
1998	Michał Burczyński	Poland	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Michał Burczyński	Poland	Lac St. Louis, Québec, Canada
2000	Michał Burczyński	Poland	Lake Hjälmaren, Sweden
2001	Michael Rehe	Michigan, USA	Saginaw Bay, Michigan
2002	Michał Burczyński	Poland	Haapsalu, Estonia
2003	Missing Results		Lake Champlain, New York
2004	Missing Results		Lake Balaton, Hungary
2005	Not Awarded		Lake Mendota, Wisconsin
2006	Missing Results	Mueritz, Germany &	Lake Vaettern, Sweden
2007	Chester Upham		Green Bay, Menominee, Michigan
2008	Missing Results		Lake Lipno, Czech Republic
2009	Max Minarek	Germany	Torch Lake, Michigan
2010	Piotr Gunther	Poland	Neusiedler See, Austria
2011	Sam Myers	Michigan, USA	Senachwine Lake, Putnam, Illinois
2012	Martin Aljaste	Estonia	Lake Hjälmaren, Örebro, Sweden
2013	Griffin Sherry	Michigan, USA	Lake Pepin, Lake City, Minnesota
2014	Kevin Grass	Estonia	Haapsalu, Estonia
2015	Griffin Sherry	Michigan, USA	Kingston, Ontario, Canada
2016	Eddie Klemets	Sweden	Lake Glan, Norrköping, Sweden
2017	Eddie Klemets	Sweden	Lake Kegonsa, Madison, Wisconsin

PERPETUAL TROPHIES

SILVER CUP WORLD CHAMPIONSHIP

Donated by Leon LeBeau, USA

Year	Skipper	Home	Regatta Site
1973	No Silver Fleet		Gull Lake, Michigan
1974	No Silver Fleet		Zegrze, Poland
1975	No Silver Fleet		Saginaw Bay, Michigan
1976	No Silver Fleet		Stora Värtan, Sweden
1977	No Silver Fleet		St. Michaels, Maryland
1978	Missing Results		Krynica Morska, Poland
1979	Mike Harvey	Michigan, USA	Lake Champlain, Vermont
1980	Bertil Rehnfeldt	Sweden	Lake Mälaren, Västerås, Sweden
1981	J Larsen	New Jersey, USA	Hamilton Bay, Ontario
1982	Joachim Pinne	West Germany	Wittensee, Germany
1983	Missing Results		Trenton, Ontario
1984	Tom Sjöman	Finland	Krynica Morska, Poland
1985	Cliff Cartwright	Michigan, USA	Bamegat Bay, New Jersey
1986	No Regatta		
1987	Paul O'Neill	Connecticut, USA	Lake St Clair, Michigan
1988	Dieter Schulz	West Germany	Leningrad, USSR
1989	Dave Terwilliger		Lake Champlain, Vermont
1990	Kjell Andersson	Sweden	Arsunda, Sweden
1991	Thomas Halsey	New York, USA	Saginaw Bay, Michigan
1992	Adam Baranowski	Poland	Arsunda, Sweden
1993	Scott Carlson		Geneva Lake, Wisconsin
1994	Manfred Schreiber	Germany	Niepolet, Poland
1995	Todd Gamble	Wisconsin, USA	Montreal, Québec, Canada
1996	Daan Schutte	Holland	Neusiedler See, Austria
1997	J. Bruce Williams	Michigan, USA	Lake St Clair, Michigan

PERPETUAL TROPHIES

SILVER CUP WORLD CHAMPIONSHIP

Donated by Leon LeBeau, USA

Year	Skipper	Home	Regatta Site
1998	Christian Seegers	Germany	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Frederik Lönegren	Sweden	Lac St. Louis, Québec, Canada
2000	Elmars Smilteneiks	Latvia	Lake Hjälmaren, Sweden
2001	Johan Tolsma	Holland	Saginaw Bay, Michigan
2002	Mats Rimmö	Sweden	Haapsalu, Estonia
2003	Fredy Rudolf	Switzerland	Lake Champlain, New York
2004	Dan Backlund	Finland	Lake Balaton, Hungary
2005	Randy Johnson	Michigan, USA	Lake Mendota, Wisconsin
2006	Pawel Burczyński	Poland	Mueritz, Germany & Lake Vättern, Sweden
2007	Christian Jensen	Denmark	Green Bay, Menominee, Michigan
2008	Leon LeBeau	Michigan, USA	Lake Lipno, Czech Republic
2009	Jim McDonagh	Minnesota, USA	Torch Lake, Michigan
2010	Piotr Gunther	Poland	Neusiedler See, Austria
2011	Mike Rehe	Michigan, USA	Senachwine Lake, Putnam, Illinois
2012	Poul Jørgensen	Denmark	Lake Hjälmaren, Örebro, Sweden
2013	Richard Gustring	Sweden	Lake Pepin, Lake City, Minnesota
2014	Wolfgang Boettger	Germany	Haapsalu, Estonia
2015	Chris Berger	Illinois, USA	Kingston, Ontario, Canada
2016	Marek Lentsius	Estonia	Lake Glan, Norrköping, Sweden
2017	Tim Sugar	Wisconsin, USA	Lake Kegonsa, Madison, Wisconsin

NORTH AMERICAN CHAMPIONSHIPS

The North American Championship formerly known as the Annual Regatta has been sailed each year since 1953.

1953: North Star Sail Club on Lake St. Clair in Michigan was the only club that offered to host the first annual regatta. 62 programs were sent out for the regatta scheduled for February 20-21. On February 19 the regatta was moved to Cass Lake, Michigan because of poor ice at the North Star club house on Lake St. Clair where it was originally intended.

1954: North Star Sail Club again sponsored the regatta and it was held at Cass Lake. The maximum fleet was 30 boats in the fourth race.

1955 The regatta was sponsored by the West Michigan Ice Yacht Club and held again at Cass Lake. 32 skippers registered.

1956: The regatta was sponsored by Anchor Bay Ice Yacht Club and held on Lake St. Clair. 25 sailors registered.

1957: 21 skippers competed on Lake St. Clair and the regatta was sponsored by the North Star Sailing Club.

1958: The sixth annual regatta was sponsored by the Detroit Ice Yacht Club and held on Lake St. Clair with clear ice and moderate wind. There were 29 sailors registered.

1959: 44 sailors registered for the regatta which was sponsored by the North Star Sailing Club and sailed on Lake St. Clair. It was the first year that the first five placing yachts were measured and checked for compliance to the official specifications.

1960: The Toledo Ice Yacht Club sponsored the regatta which was a first for a yacht club outside of Michigan. The championship was scheduled for Maumee Bay on January 16-17 but was postponed due to lack of wind. It was completed on Devil's Lake in Michigan on February 6-7 with winds of 25-30 mph. 48 skippers registered.

1961: The ninth annual regatta was sailed in Wisconsin for the first time on Geneva Lake and was sponsored by the Skeeter Iceboat Club. 53 competitors were registered.

1962: The tenth annual championship went to the eastern United States for the first time with the Kewasa Ice Yacht club as host. The regatta was sailed on Honeoye Lake in New York on January 6-7. with 48 skippers registered.

NORTH AMERICAN CHAMPIONSHIPS

1963: The regatta was postponed for two weeks and then sailed on January 19-29 at Charles Mill Lake near Mansfield, Ohio with the Mohican Sailing Club as cosponsor with Saginaw Bay. The first day offered nothing but very light air for the 61 competing skippers, split into 2 fleets for the first time. Two heats were held for each fleet on Saturday while Sunday offered fresh to strong wind (gusts to 40 mph were reported) and three more heats were run. The fleets were changed to Championship and Consolation after the first Sunday heat, based on cumulative results. The last two races settled the results.

1964: After a week's postponement due to New Year's snow, the regatta was held on January 11 on Maumee Bay, Ohio under the sponsorship of the Toledo Ice Yacht Club. There were 77 entries which necessitated a split fleet system. Saturday brought light air on hard ice with crusty snow patches. Three races in each fleet were sailed. Sunday dawned with a temperature of 5F and a 35 knot northeast wind that drove Lake Erie into Maumee Bay. The fleet was hard pressed to get off the ice because of flooding and breaking ice. The first day results were made final.

1965: After 3 postponements, the thirteenth annual regatta was sailed on Cass Lake, Pontiac, Michigan on perfect ice under strong to almost half gale conditions. 54 entries started in split fleets the first day and increasing winds caused a toll in spars and planks. The pile of splintered sitka back of the starting line would have made a great bonfire. Winds continued to build Saturday night which made for tough racing conditions on Sunday through snow drifts.

1966: After one disappointing attempt to hold the regatta in late January, the weather improved and 57 skippers returned to Gull Lake, Michigan February 19-20. A full schedule of 5 races were run for each of the split fleets.

1967: The regatta was held at Lake Lamoka near Bath, New York on February 11 with 102 entries, a new record. Because of the fleet size and dimensions of the lake, the fleet was split into three flights for the first time. A 2" snow cover presented high drag conditions but the wind which averaged 15 knots gusting to 20, offered sufficient power for good racing. Overall, this presented an excellent all around test for boats, equipment, sailing skill, and endurance. The Race Committee did a great job on the ice and were inundated by the avalanche of scoring work for 3 fleets. One innovation that proved invaluable were the several tape recorders used to record laps and rule infractions as soon as they were observed by the RC.

NORTH AMERICAN CHAMPIONSHIPS

1968: 114 DNs registered for the championship at Sandusky, OH, but rain, sleet and snow drifts caused a two week postponement. The championship was sailed on February 10-11 with 60 boats at Cass Lake MI. After three races and with two races to go, two points separated the top four boats (Gene Treuter, defending champion Roland Redner, Jack Wesenberg, Jane Pegel). The fleet was placed in championship and consolation divisions. Wesenberg won the fourth race to go into the final race in a tie with Redner. Treuter won the final race to end in a tie on points with Redner. Treuter's three firsts won the tie for the championship. Redner was second, Wesenberg was third, Pegel fourth. It was the first time that an IDNIYRA Commodore (Gene Treuter) won the annual regatta.

1969: Though originally scheduled for the eastern United States, the regatta was ultimately sailed at Mt. Clemens, Michigan on Lake St. Clair four miles out off Huron Point with 64 entries. It had been previously scheduled for Oyster Bay, New York and Danbury, Connecticut and twice postponed. 1) Jack Wesenberg, Grosse Pointe; 2) Jane Pegel, Chicago; 3) Roy Holden, Metamora, MI; 4) Gene Treuter, Orchard Lake, MI; 5) Stan Woodruff, New Baltimore MI. Tim Woodhouse, Grosse Point placed 10th and was top Junior.

1970: 62 racers were registered for the regatta which took place on Lake St. Clair in Michigan. All sailed as a single fleet because conditions indicated a slow fleet. Three races were held in light to moderate winds on Saturday and no races were run on Sunday because of fog and light winds.

1971: For the first time in many years, the regatta was actually held on the weekend scheduled on February 16. The regatta was originally slated to be held on Geneva Lake in Wisconsin but 22" of snow prompted the fleet to move to Geist Reservoir in Indianapolis, Indiana. This also marked the first time that it became a truly international event with the arrival of 5 Europeans, Wim Van Acker, (Netherlands) Jan Eindhoven (Netherlands), Romar Rowecki (Poland), Alex Keufer (West Germany), and another West German.

1972: The championship was hosted by the North Shrewsbury Ice Boat and Yacht club on Bantam Lake in Connecticut February 5-6. Conditions varied with punishing winds on the first day and light air on the second which was a problem for boats that were not sufficiently tuned. Winner was Jan Gougeon, the defending champion. 2) Gordy Bowers; 3) Bill Allen. During a January campaign in Europe, Jan had placed second in the European championships. Upon returning home on January 28 he went to work and put together a new boat, with which he then won the class title.

1973: Two fleets competed on Sodus Bay in New York February 3-5. 1) Tim Woodhouse; 2) John Larson; 3) Stan Woodruff; 4) Jan Gougeon; 5) John Schuck.

NORTH AMERICAN CHAMPIONSHIPS

1974: 60 boats divided into two fleets of 30 each for qualifiers at Hamilton, Ontario. 1) Tim Woodhouse (defending champion); 2) Jan Gougeon; 3) Gordy Bowers; 4) Jane Pegel; 5) Don Vercruysse.

1975: The championship came off right on schedule February 1-2 at Williams Bay on Geneva Lake in Wisconsin. 104 skippers raced in two fleets on snow-free ice with a smooth to slight pebbly texture. The wind was on the light side for both days of the regatta.

1976: The regatta was sailed on Hamilton Bay in Ontario, Canada. Jan Gougeon placed first, John Schuch, second; Don Coggen, third; and Chuck Lamphere winning junior.

1977: Good ice was found on the Miles River (a part of the Chesapeake Bay) at St. Michaels, Maryland which was the first ice they had in 20 years. Racing was postponed on the first day because of excessive winds. Though the strong winds continued, the regatta was sailed on Sunday and Monday.

1978: After a two week postponement, the regatta was held on Lake Canandaigua at Canandaigua, NY on new ice.

1979: Polish competitors Piotr Burczynski placed first and Stan Macure second on Lake Champlain in Vermont. Ed Kraft was third and Richard Gibson was the top placing junior.

1980: Meade Gougeon placed first on Saginaw Bay, Alpena, Michigan; Henry Bossett, second; Randy Johnson, third; and Harry Melges III was the top junior.

1981: Sailed after the worlds on Hamilton Harbor at Hamilton, Ontario. Hard one inch drifts covered 40% of the ice. Medium to light winds allowed 5 races in two fleets to be sailed in one day. The following day brought snow then rain.

1982: The NA championship was moved from Lake Champlain, Vermont to Lake Wallenpaupack, Pennsylvania, then to Round Valley Reservoir, New Jersey, and finally to Canandaigua, New York. An excellent regatta was completed here over two days using a single fleet. John Day and Tyler Keys plus three race committees were really dedicated.

1983: The North Americans were completed on the last day of a week of warm weather and rain. It was originally scheduled for Sodus Bay and then moved to Trenton, Ontario, Canada. Heavy rain prevented racing the first day there and left the ice like Swiss cheese the next. Cooler weather finally allowed the sailing of the Gold Cup and North American Championship.

1984: The race was sailed in light winds and 60 degree temperatures with deteriorating ice conditions near Sandusky, Ohio.

NORTH AMERICAN CHAMPIONSHIPS

1985: The North American Championship didn't get off with the Gold Cup because the entire week was used up getting to and sailing on Barnegat Bay, New Jersey. Two weeks later another attempt was made on Lake Canandagua in New York. Rain, warm weather and deteriorating ice conditions moved the site to nearby Lake Honyoye. Gas holes destroyed one boat and caused everyone else to decide to abandon the regatta.

1986: The regatta was sailed at Muskegon, Michigan. The first day saw light wind, allowing only one race. The second day had 10-15 knots. The ice was slightly bumpy snow ice with a few areas of snow that had not completely melted.

1987: This race was sailed on Lake St. Clair, Michigan, a day after the Gold Cup. Ice conditions were good with about 3~4" of new snow on the ice. The wind was moderate.

1988: The race was scheduled for January 30 and 31 on Hamilton Harbor, Ontario, Canada. Lack of sailable ice there resulted in a move to the Bay Of Quinte in Trenton Ontario. Three races for three fleets with a total of 104 sailors were sailed on Saturday. Conditions were wet and windy. Puddles on the ice slowed the boats much like snow drifts except you couldn't tell which ones were the deep ones. The wind was about 15 mph. By the following day the puddles had found their way through the ice resulting in some large drain holes on the course. In spite of light wind and fast ice, further racing was canceled for safety reasons.

1989: The North Americans were sailed in one very cold afternoon on Lake Champlain at Sandbar State Park, just north of Burlington Vermont. The date was January 25. Three races were sailed in the Gold fleet and two in the Silver and Bronze. The ice was bumpy with about 30% thin snow coverage. The wind was about 10 mph. The regatta was called complete the next day as the wind was a steady 50 mph with a gust of 88 recorded nearby!

1990: The 1990 regatta was scheduled for Sodus Bay, New York. It wound up on Elk Lake near Traverse City, Michigan. The accommodations at the Elk Lake Lodge were wonderful. The sailing was on hard, flat frozen slush. The wind for most of the races was 15 to about 22 mph. The second day it was much lighter and variable in direction.

1991: The 1991 regatta was to be held in conjunction with Gold Cup but was postponed for several weeks due to warm weather and poor ice conditions. The regatta was held at Lake Lelanau Michigan, northwest of Traverse City. The regatta was completed in one day on a crowded course which resulted in several collisions.

1992: Lake Mendota, in Madison Wisconsin provided moderate winds and smooth ice until the regatta was ended by rain on the second day. Three fleets sailed the West end of the lake, under the direction of Robert E. Pegel.

1993: Geneva Lake in Wisconsin hosted seven races on hard snow ice. The site was just large enough to hold the regatta at the east end of the lake. The regatta lasted three days and all three fleets sailed seven races.

NORTH AMERICAN CHAMPIONSHIPS

1994: After a false start because of newly formed drain holes at Grand Lake in Ohio, the racing was postponed for six weeks in a snow filled winter. Finally, Elk Lake in Michigan reported raceable ice in mid-March. Many people could not attend after using vacations on the false start, but three races were held on the final day of the regatta to complete the series.

1995: The North American Championship was sailed on Lac St. Louis in Montreal, Québec, Canada due to poor ice conditions at the original location of Lake Canandaigua in New York. The ice was almost fifty percent snow covered which made the port side of the race course heavily favored. Karol Jabłoński's "relative" consistency paid off with a victory as the result.

1996: After a false start because of a downpour of rain on Thursday, the North American's were moved from Detroit, Michigan to Burlington, Vermont, on Saturday, February 10th. On Saturday evening, about 1/2 inch of snow fell. To the racers on Sunday, it gave the sensation that you were not moving very fast, when you actually were. Four races were held for both Gold and Silver fleet. Racing on Monday was cancelled due to extremely cold temperatures and winds out of the NW at 15 to 25 mph.

1997: The North American's were held after the Gold Cup on Lake St. Clair, Michigan, in extreme cold and sticky ice conditions. After five races in Gold Fleet, there was a three-way tie between Piotr Burczyński, Meade Gougeon, and Karol Jabłoński. After the tie was broken, Meade Gougeon was our new North American Champion. There were also five races held for Silver Fleet which was won by Commodore Bill Condon. In all, there were 118 participants for this event.

1998: The North American's were held on very noisy ice in Button's Bay on Lake Champlain, south of Burlington, Vermont. Dr. Greg Smith of Gull Lake, Michigan, was the new North American Champion after seven hard-fought races. Seven races were also held for the Silver Fleet, with a total of 84 participants in the regatta.

1999: The North American Championship was held immediately after the World Championship on a large piece of ice on Lac St. Louis, Québec Canada. The wind was not very cooperative on the first day, but the mini-qualifiers were held, as well as the first Gold fleet race. The next day of racing was started in light but fairly steady wind, which built to around 10 knots during the afternoon. This provided excellent racing, with 6 races in Gold and Silver fleets, and 5 races in Bronze Fleet.

2000: The regatta was postponed three times due to poor weather forecasts. The regatta was finally sailed at Lake Miltona, Minnesota. The ice was a pebbly snow ice, with a large area free of cracks or other hazards. The ice was hard in the morning, and turned quite soft by the afternoon with temperatures close to 60F. Jan Gougeon let everyone see his mastery of variable conditions by leading the fleet in all four Gold fleet races. The racing concluded after one day of racing due to deteriorating ice and forecast temperatures of 65F.

NORTH AMERICAN CHAMPIONSHIPS

2001: The North American's followed on the heels of the light air, abbreviated Gold Cup on the same piece of ice in Saginaw Bay, Michigan. The trip to the race area on the first morning promised more light conditions, but the wind finally began to come up and the race committee got in 4 races for all in the freshening winds. Conditions held (and the Coast Guard icebreaker stayed away) on Saturday with 3 more races all around. Thirty hours accomplished more in terms of sailing and fun than the previous 5 days. The results were familiar with Jabłoński giving an all-bullet clinic, followed by Ron Sherry, Greg Smith and John Harper in Gold Fleet with the Silver Fleet dominated by Jim Grogan.

2002: The regatta was sailed on Lake Mille Lac, Garrison, Minnesota, the only suitable piece of ice in North America east of Montana and south of Alaska. Across the movable ice bridge and into the fish house suburbs went the troops to find 80% clear pebbly ice with low sticky drifts that put a premium on runner selection and energy management. The first day saw only a single Silver race before the wind went away. The next day started light for the Gold fleet but picked up for their second race. During the following Silver race drizzle started falling and visibility was limited. Better conditions greeted the third Gold race but the wind died for good at its end. The wind and temperatures the next day were too extreme and the regatta was called complete with Ron Sherry the leader in Gold and Ken Mitchell the winner in Silver.

2003: Deteriorating ice at the Gold Cup site moved the NA regatta north to Plattsburgh, New York onto a fairly restricted piece of glare ice that resulted from rain followed by some serious cold and wind. Dangerous conditions stopped racing after the first qualifier and racing didn't resume for 3 days. With a stroke of luck and termination, the race committee squeezed the third Gold fleet race into the moments just before sunset on the very last day of the regatta which saw Ron Sherry the Gold winner, Eben Whitcomb the Silver winner and Bill Condon the winner in bronze.

2004: The site was moved after several days of warm temperatures and rain made the safety of the ice at the primary site questionable. The regatta was relocated to another bay in Sandusky, Ohio where the ice is always thicker and more stable. This new location in front of Cedar Point amusement park was the made famous in the "Welcome to Our Playground!" photo that graced the North American IDNIYRA website for many years. This site only has room for 3/4 mile courses, so the Race Committee divided the sailors into three fleets. The ice was suitable, but little or no wind on Saturday and Sunday and soft ice due to the high temperatures made for a slow start to the regatta. By the end of the day on Monday, the Race Committee had managed four races in Gold, and three races each in Silver and Bronze fleets.

2005: The regatta was to be sailed in Wisconsin in January with the Worlds but all sites became snowed out just as the Worlds finished. The regatta was rescheduled to a later February date and Elk Lake, Michigan became the site. This became a wonderful venue with great weather in which to hold and finish the North Americans.

NORTH AMERICAN CHAMPIONSHIPS

2006: The race site at Lake Winnebago in Wisconsin was abandoned because of one foot thick snow drifts. The regatta was moved three hours west to Lake Pepin; a wide spot of the Mississippi River, in Pepin, Wisconsin. The regatta was completed after two days of racing thru semi firm 1-2 inch snow drifts. You could watch eagles flying overhead as you checked your tell tales.

2007: The regatta was completed on Friday with Wednesday as a lay day after the Gold Cup, in Menominee, Michigan on Green Bay, Lake Michigan. Conditions were again very favorable with the same results for the first four places in Gold Fleet. What a week of sailing! Everyone drove home tired, happy and satisfied. Truly a fabulous week.

2008: The regatta was moved to Lake Pepin, a wide spot on the Mississippi River between Minnesota and Wisconsin after a last minute snow fall covered the perfect ice at the primary venue, Lake Winnebago in Wisconsin. Ninety eight sailors made the trip to the land of bald eagles to participate in a great regatta. Winds ranged from 10 to 20 mph with scattered small drifts. The ice was a bit rough but the five mile ice boat cruise on the Mississippi at the end of the day was truly a pleasure.

2009: After the Gold Cup, the ice at Torch Lake in Michigan deteriorated and the regatta moved to Green Bay, Wisconsin. The ice in Green Bay was cobblestone ice with hazards and 1/4" of snow on it. On the first day there was too much wind for ice conditions but on the second day there was the perfect 4-7 knots of ENE wind. On the third day there was no wind and a covering of snow so the regatta was done with one good day of racing. Awards were presented on the ice and Ron Sherry took home his eighth North American Championship trophy.

2010: After days of a frustrating search to find suitable ice anywhere in North America, Thunder Bay, Ontario, Canada was scouted and found to have miles of beautiful ice. It was the first time that the regatta has been held on Lake Superior. Fifty eight skippers registered for the regatta which allowed the fleets to be split into Gold and Silver. Then, after waiting days for the wind to make an appearance, the regatta was finally completed and Ron Sherry won his ninth North American Championship trophy.

2011: After the Gold Cup was called complete on Senachwine Lake in Putnam, Illinois, the North American championship was canceled to allow competitors time to outrace a powerful blizzard and make it home safely.

2012: Though scheduled for the Eastern Lakes Region, the lack of conditions forced the regatta to be held on Green Bay in Green Bay, Wisconsin. Monday morning the wind was blowing hard but calmed down enough mid afternoon to sail a qualifier and three races in the Gold and Silver fleets. On Tuesday, the only race of the day, a Gold fleet, was black flagged for not making the time limit due to light air. A growing crack in the ice that threatened to send the race course out in Green Bay became a concern and the regatta was called complete. Ron Sherry won his tenth North American Championship and Bob Struble won the Silver fleet.

NORTH AMERICAN CHAMPIONSHIPS

2013: The race course on Lake Pepin, the widest naturally occurring part of the Mississippi River, near Lake City, Minnesota where the Gold Cup was held was too full of ruts to use for the North American championship. The day after the Worlds, gusty winds caused postponement which allowed John Dennis, Ron Sherry, and Tomek Zakrzewski to spend the day scouting Lake Pepin for a better ice sheet. They found a good sheet a little ways north. After waiting a few days, the wind finally came up enough to complete the North American Championship on Saturday, February 2. John Dennis won his second North American championship. in three races.

2014: A snow storm forced the regatta to be relocated from Lake Senachwine in Peru, Illinois to Lake Champlain, Plattsburgh, New York. Once a permit was secured from the local authorities, the regatta officially began on January 23 with one Silver qualifier race in the afternoon. The next day, temperatures barely made 10F but everyone persevered and the day ended with 3 more races in Silver and 4 in the Gold fleet. Winds were light in the morning, building during the day to the point that snow devils were seen flying down the ice. Ron Sherry US44 won the Gold fleet by one point over Mark Christensen US4824. In the last race, Ron experienced an exciting leeward rounding where he was thrown out of the boat, jumped back in, and completed the race. The winds were too gusty and treacherous the next morning and the regatta was called complete.

2015: After the Gold Cup was completed on Lake Ontario in Kingston, Ontario, Canada, a snow storm forced the cancellation of the North American Championship.

2016: The 2016 North American Championship was sailed on Lake Monona in Madison, Wisconsin. 10 races were sailed in Gold and Silver Fleets. All-around skills were needed as the wind strength during the event ranged from very light to very heavy. Matt Struble took first in Gold Fleet and David Elsmo won the Silver Fleet.

2017: After snow ended the Gold Cup on Lake Kegonsa in Madison, Wisconsin, scouts were sent to find ice. After an extensive search, a regatta quality site was found on Green Bay near Oconto, Wisconsin. Sailors enjoyed two days of sailing for a total of 7 races in each fleet.. For the first time in regatta history, a father (Ron Sherry) and son (Griffin Sherry, competing as a junior) won the Gold and Silver fleet respectively.

NORTH AMERICAN DN CHAMPIONSHIP

FIRST PLACE HOWARD BOSTON TROPHY

Donated in 1953 by Howard Boston of Mt. Clemens, Michigan

Year	Skipper	Home	Regatta Site
1953	Jack Moran	Michigan, USA	Cass Lake, Michigan
1954	Jack Moran	Michigan, USA	Cass Lake, Michigan
1955	Bill Sporer	Michigan, USA	Cass Lake, Michigan
1956	Ed Grant	Michigan, USA	Lake St. Clair, Michigan
1957	Skip Boston	Michigan, USA	Lake St. Clair, Michigan
1958	Remi DeBlaere	Michigan, USA	Lake St. Clair, Michigan
1959	Skip Boston	Michigan, USA	Lake St. Clair, Michigan
1960	Jane Pegel	Illinois, USA	Devil's Lake, Michigan
1961	Jack Young	New York, USA	Geneva Lake, Wisconsin
1962	Reuben Snodgrass	New York, USA	Lake Honeoye, New York
1963	Jane Pegel	Illinois, USA	Mansfield, Ohio
1964	Skip Boston	Michigan, USA	Toledo, Ohio
1965	Ed Grant	Michigan, USA	Cass Lake, Michigan
1966	Ro Redner	Michigan, USA	Gull Lake, Michigan
1967	Ro Redner	Michigan, USA	Lake Lamoka, New York
1968	Gene Treuter	Michigan, USA	Cass Lake, Michigan
1969	Jack Wesenberg	Michigan, USA	Lake St. Clair, Michigan
1970	Tim Woodhouse	Michigan, USA	Lake St. Clair, Michigan
1971	Jan Gougeon	Michigan, USA	Geist Reservoir, Indiana
1972	Jan Gougeon	Michigan, USA	Bantam Lake, Connecticut
1973	Tim Woodhouse	Michigan, USA	Sodus Bay, New York
1974	Tim Woodhouse	Michigan, USA	Hamilton Harbor, Canada
1975	Vello Kuusk	Estonia, USSR	Geneva Lake, Wisconsin
1976	Jan Gougeon	Michigan, USA	Hamilton Bay, Ontario, Canada
1977	John Schuck	Michigan, USA	St. Michaels, Maryland

NORTH AMERICAN DN CHAMPIONSHIP

FIRST PLACE HOWARD BOSTON TROPHY

Donated in 1953 by Howard Boston of Mt. Clemens, Michigan

Year	Skipper	Home	Regatta Site
1978	Ed Kraft	Michigan, USA	Canandaigua, New York
1979	Piotr Burczyński	Poland	Lake Champlain, Vermont
1980	Meade Gougeon	Michigan, USA	Saginaw Bay, Alpena, Michigan
1981	Jan Gougeon	Michigan, USA	Hamilton Bay, Ontario, Canada
1982	Henry Bossett	New Jersey, USA	Canandaigua, New York
1983	Henry Bossett	New Jersey, USA	Trenton, Ontario
1984	John Koeck	New Jersey, USA	Sandusky, Ohio
1985	After two separate attempts, the regatta was cancelled.		
1986	Jan Gougeon	Michigan, USA	Muskegon, Michigan
1987	Mike O'Brien	New Jersey, USA	Lake St Clair, Michigan
1988	Jan Gougeon	Michigan, USA	Trenton, Ontario, Canada
1989	Ron Sherry	Michigan, USA	Lake Champlain, Vermont
1990	Henry Bossett	New Jersey, USA	Elk Lake, Michigan
1991	Ron Sherry	Michigan, USA	Lake Leelanau, Michigan
1992	Ron Sherry	Michigan, USA	Lake Mendota, Madison, Wisconsin
1993	Henry Bossett	New Jersey, USA	Geneva Lake, Wisconsin
1994	Jan Gougeon	Michigan, USA	Elk Lake, Michigan
1995	Karol Jabłoński	Poland	Montreal, Québec, Canada
1996	Ken VanWagnen	Ohio, USA	Lake Champlain, Vermont
1997	Meade Gougeon	Michigan, USA	Lake St. Clair, Michigan
1998	Greg Smith	Michigan, USA	Lake Champlain, Vermont
1999	Ron Sherry	Michigan, USA	Lac St. Louis, Québec Canada
2000	Jan Gougeon	Michigan, USA	Lake Miltona, Minnesota
2001	Karol Jabłoński	Poland	Saginaw Bay, Michigan
2002	Ron Sherry	Michigan, USA	Garrison, Minnesota

NORTH AMERICAN DN CHAMPIONSHIP

FIRST PLACE HOWARD BOSTON TROPHY

Donated in 1953 by Howard Boston of Mt. Clemens, Michigan

Year	Skipper	Home	Regatta Site
2003	Ron Sherry	Michigan, USA	Lake Champlain, New York
2004	John Dennis	Minnesota, USA	Sandusky, Ohio
2005	Matt Struble	Michigan, USA	Elk Lake, Michigan
2006	Ron Sherry	Michigan, USA	Lake Pepin, Pepin, Wisconsin
2007	Matt Struble	Michigan, USA	Green Bay, Menominee, Michigan
2008	Matt Struble	Michigan, USA	Lake City, Minnesota
2009	Ron Sherry	Michigan, USA	Green Bay, Menominee, Michigan
2010	Ron Sherry	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No Regatta		
2012	Ron Sherry	Michigan, USA	Green Bay, Green Bay, Wisconsin
2013	John Dennis	Minnesota, USA	Lake Pepin, Lake City, Minnesota
2014	Ron Sherry	Michigan, USA	Lake Champlain, Plattsburgh, New York
2015	No Regatta		
2016	Matt Struble	California, USA	Lake Monona, Madison, Wisconsin
2017	Ron Sherry	Michigan, USA	Green Bay, Peshtigo, Wisconsin

NORTH AMERICAN DN CHAMPIONSHIP

SECOND PLACE ROBERT PEGEL TROPHY

Donated in 1962 by Robert Pegel of Williams Bay, Wisconsin

Year	Skipper	Home	Regatta Site
1953	Bill Sarns	Michigan, USA	Cass Lake, Michigan
1954	Skip Boston	Michigan, USA	Cass Lake, Michigan
1955	Chuck Cartwright	Michigan, USA	Cass Lake, Michigan
1956	Jack Wesenberg	Michigan, USA	Lake St. Clair, Michigan
1957	Leon Tempiin	Michigan, USA	Lake St. Clair, Michigan
1958	Jane Pegel	Illinois, USA	Lake St. Clair, Michigan
1959	Jane Pegel	Illinois, USA	Lake St. Clair, Michigan
1960	Bill Sporer	Michigan, USA	Devil's Lake, Michigan
1961	Jane Pegel	Illinois, USA	Geneva Lake, Wisconsin
1962	John Meirs	New York, USA	Lake Honeoye, New York
1963	Jim Nolan	Illinois, USA	Mansfield, Ohio
1964	Ed Stack	Michigan, USA	Toledo, Ohio
1965	Jane Pegel	Illinois, USA	Cass Lake, Michigan
1966	Jane Pegel	Illinois, USA	Gull Lake, Michigan
1967	Gene Treuter	Michigan, USA	Lake Lamoka, New York
1968	Ro Redner	Michigan, USA	Cass Lake, Michigan
1969	Jane Pegel	Illinois, USA	Lake St. Clair, Michigan
1970	Russ Johnson	Michigan, USA	Lake St. Clair, Michigan
1971	Stan Woodruff	Michigan, USA	Geist Reservoir, Indiana
1972	Gordon Bowers	Minnesota, USA	Bantam Lake, Connecticut
1973	John Larsen	New Jersey, USA	Sodus Bay, New York
1974	Joe Gougeon	Michigan, USA	Hamilton Harbor, Ontario, Canada
1975	Endel Vooremaa	Estonia, USSR	Geneva Lake, Wisconsin
1976	John Schuch	Michigan, USA	Hamilton Bay, Ontario, Canada
1977	Zbigniew Stanislawski	Poland	St. Michaels, Maryland

NORTH AMERICAN DN CHAMPIONSHIP

SECOND PLACE ROBERT PEGEL TROPHY

Donated in 1962 by Robert Pegel of Williams Bay, Wisconsin

Year	Skipper	Home	Regatta Site
1978	Jane Pegel	Wisconsin, USA	Canandaigua, New York
1979	Stan Macur	Poland	Lake Champlain, Vermont
1980	Henry Bossett	New Jersey, USA	Saginaw Bay, Alpena, Michigan
1981	Piotr Burczyński	Poland	Hamilton Bay, Ontario, Canada
1982	Ron Sherry	Michigan USA	Canandaigua, New York
1983	Jan Gougeon	Michigan, USA	Trenton, Ontario
1984	Mike O'Brien	New Jersey, USA	Sandusky, Ohio
1985	No Regatta		
1986	Henry Bossett	New Jersey, USA	Muskegon, Michigan
1987	Greg Smith	Michigan, USA	Lake St Clair, Michigan
1988	Mike O'Brien	New Jersey, USA	Trenton, Ontario, Canada
1989	Greg Smith	Michigan, USA	Lake Champlain, Vermont
1990	Ron Sherry	Michigan, USA	Elk Lake, Michigan
1991	Jan Gougeon	Michigan, USA	Lake Leelanau, Michigan
1992	Ted Flack	Michigan, USA	Lake Mendota, Madison, Wisconsin
1993	Jan Gougeon	Michigan, USA	Geneva Lake, Wisconsin
1994	Ron Sherry	Michigan, USA	Elk Lake, Michigan
1995	Andreas Bock	Germany	Montreal, Québec, Canada
1996	Cliff Cartwright	Michigan, USA	Lake Champlain, Vermont
1997	Piotr Burczyński	Poland	Lake St. Clair, Michigan
1998	Mike O'Brien	New Jersey, USA	Lake Champlain, Vermont
1999	Karol Jabłoński	Poland	Lac St. Louis, Québec, Canada
2000	John Harper	Michigan, USA	Lake Miltona, Minnesota
2001	Ron Sherry	Michigan, USA	Saginaw Bay, Michigan
2002	Greg Smith	Michigan, USA	Garrison, Minnesota

NORTH AMERICAN DN CHAMPIONSHIP

SECOND PLACE ROBERT PEGEL TROPHY

Donated in 1962 by Robert Pegel of Williams Bay, Wisconsin

Year	Skipper	Home	Regatta Site
2003	Michał Burczyński	Poland	Lake Champlain, New York
2004	John Harper	Michigan, USA	Sandusky, Ohio
2005	John Harper	Michigan, USA	Elk Lake, Michigan
2006	Matt Struble	Michigan, USA	Lake Pepin, Pepin, Wisconsin
2007	Tomas Lindgren	Sweden	Green Bay, Menominee, Michigan
2008	John Dennis	Minnesota, USA	Lake City, Minnesota
2009	Matt Struble	Michigan, USA	Green Bay, Menominee, Wisconsin
2010	Matt Struble	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No regatta		
2012	John Dennis	Minnesota, USA	Green Bay, Green Bay, Wisconsin
2013	Ron Sherry	Michigan, USA	Lake Pepin, Lake City, Minnesota
2014	Mark Christensen	Minnesota, USA	Lake Champlain, Plattsburgh, NY
2015	No Regatta		
2016	Ron Sherry	Michigan, USA	Lake Monona, Madison, Wisconsin
2017	Michał Burczyński	Poland	Green Bay, Peshtigo, Wisconsin

NORTH AMERICAN DN CHAMPIONSHIP

THIRD PLACE OLD DOMINION TROPHY

Presented in 1967 by the Old Dominion IYC, Virginia, Commodore Larry Hartwick

Year	Skipper	Home	Regatta Site
1967	Stan Woodruff	Michigan, USA	Lake Lamoka, New York
1968	Jack Wesenberg	Michigan, USA	Cass Lake, Michigan
1969	Roy Holden	Michigan, USA	Lake St. Clair, Michigan
1970	Robert Jones	Michigan, USA	Lake St. Clair, Michigan
1971	Spike Boston	Michigan, USA	Geist Reservoir, Indiana
1972	William Allen	Minnesota, USA	Bantam Lake, Connecticut
1973	Stan Woodruff	Michigan, USA	Sodus Bay, New York
1974	Gordon Bowers	Minnesota, USA	Hamilton Harbor, Canada
1975	Bogdan Kramer	Poland	Geneva Lake, Wisconsin
1976	Don Coggen	Michigan, USA	Hamilton Bay, Ontario, Canada
1977	Tim Woodhouse	Michigan, USA	St. Michaels, Maryland
1978	Bob Redner	Michigan, USA	Canandaigua, New York
1979	Ed Kraft	Michigan, USA	Lake Champlain, Vermont
1980	Randy Johnson	Michigan, USA	Saginaw Bay, Alpena, Michigan
1981	Ain Vilde	Estonia, USSR	Hamilton Bay, Ontario, Canada
1982	Jan Gougeon	Michigan, USA	Canandaigua, New York
1983	Leon LeBeau	Michigan, USA	Trenton, Ontario, Canada
1984	Jan Gougeon	Michigan, USA	Sandusky, Ohio
1985	No Regatta		
1986	Leon LeBeau	Michigan, USA	Muskegon, Michigan
1987	Bogdan Kramer	Poland	Lake St Clair, Michigan
1988	Dan Connell	New Jersey, USA	Trenton, Ontario, Canada
1989	Jan Gougeon	Michigan, USA	Lake Champlain, Vermont
1990	Mike O'Brien	New Jersey, USA	Elk Lake, Michigan
1991	Meade Gougeon	Michigan, USA	Lake Leelanau, Michigan

NORTH AMERICAN DN CHAMPIONSHIP

THIRD PLACE OLD DOMINION TROPHY

Presented in 1967 by the Old Dominion IYC, Virginia, Commodore Larry Hartwick

Year	Skipper	Home	Regatta Site
1992	Jan Gougeon	Michigan, USA	Lake Mendota, Madison, Wisconsin
1993	Bernd Zeiger	Germany	Geneva Lake, Wisconsin
1994	Paul Goodwin	Michigan, USA	Elk Lake, Michigan
1995	Jan Gougeon	Michigan, USA	Montreal, Québec, Canada
1996	Ron Sherry	Michigan, USA	Lake Champlain, Vermont
1997	Karol Jabłoński	Poland	Lake St. Clair, Michigan
1998	John Harper	Michigan, USA	Lake Champlain, Vermont
1999	Tomas Lindgren	Sweden	Lac St. Louis, Québec Canada
2000	Bruce Williams	Michigan, USA	Lake Miltona, Minnesota
2001	Greg Smith	Michigan, USA	Saginaw Bay, Michigan
2002	Matt Struble	Michigan, USA	Garrison, Minnesota
2003	John Davenport	Wisconsin, USA	Lake Champlain, New York
2004	Jan Gougeon	Michigan, USA	Sandusky, Ohio
2005	John Dennis	Minnesota, USA	Elk Lake, Michigan
2006	John Dennis	Minnesota, USA	Lake Pepin, Pepin, Wisconsin
2007	Bernd Zeiger	Germany	Green Bay, Menominee, Michigan
2008	Ron Sherry	Michigan, USA	Lake City, Minnesota
2009	Dan Backlund	Finland	Green Bay, Menominee, Michigan
2010	Steve Orlebeke	Wisconsin, USA	Thunder Bay, Ontario, Canada
2011	No regatta		
2012	Matt Struble	California USA	Green Bay, Green Bay, Wisconsin
2013	James "T" Thieler	Rhode Island, USA	Lake Pepin, Lake City, Minnesota
2014	Steve Orlebeke	Wisconsin, USA	Lake Champlain, Plattsburgh, NY
2015	No Regatta		
2016	James "T" Thieler	Rhode Island, USA	Lake Monona, Madison, Wisconsin
2017	Matt Struble	California, USA	Green Bay, Peshtigo, Wisconsin

NORTH AMERICAN DN CHAMPIONSHIP SENIOR TROPHY (OVER 50)

Presented in 1974 by Meade Gougeon

Year	Skipper	Home	Regatta Site
1974	Ted Sprague	Pennsylvania, USA	Hamilton Harbor, Canada
1975	Wim van Acker	Holland	Geneva Lake, Wisconsin
1976	Ro Redner	Michigan, USA	Hamilton Bay, Ontario, Canada
1977	Ro Redner	Michigan, USA	St. Michaels, Maryland
1978	Ro Redner	Michigan, USA	Canandaigua, New York
1979	Ed Kraft	Michigan, USA	Lake Champlain, Vermont
1980	Ed Kraft	Michigan, USA	Saginaw Bay, Alpena, Michigan
1981	Wim van Aker	Holland	Hamilton Bay, Ontario, Canada
1982	Doug Harvey	Michigan, USA	Canandaigua, New York
1983	John Larson	New Jersey, USA	Trenton, Ontario, Canada
1984	John Larson	New Jersey, USA	Sandusky, Ohio
1985	No Regatta		
1986	Jane Pegel	Wisconsin, USA	Muskegon, Michigan
1987	Stan Woodruff	Michigan, USA	Lake St Clair, Michigan
1988	Ed Kraft	Michigan, USA	Trenton, Ontario, Canada
1989	Meade Gougeon	Michigan, USA	Lake Champlain, Vermont
1990	Stan Woodruff	Michigan, USA	Elk Lake, Michigan
1991	Meade Gougeon	Michigan, USA	Lake Leelanau, Michigan
1992	Meade Gougeon	Michigan, USA	Lake Mendota, Madison, Wisconsin
1993	Meade Gougeon	Michigan, USA	Geneva Lake, Wisconsin
1994	Meade Gougeon	Michigan, USA	Elk Lake, Michigan
1995	Meade Gougeon	Michigan, USA	Montreal, Québec, Canada
1996	Jan Gougeon	Michigan, USA	Lake Champlain, Vermont
1997	Meade Gougeon	Michigan, USA	Lake St. Clair, Michigan
1998	Jan Gougeon	Michigan, USA	Lake Champlain, Vermont

NORTH AMERICAN DN CHAMPIONSHIP SENIOR TROPHY (OVER 50)

Presented in 1974 by Meade Gougeon

Year	Skipper	Home	Regatta Site
1999	Piotr Burczyński	Poland	Lac St. Louis, Québec Canada
2000	Jan Gougeon	Michigan, USA	Lake Miltona, Minnesota
2001	Meade Gougeon	Michigan, USA	Saginaw Bay, Michigan
2002	Don Jones	Michigan, USA	Garrison, Minnesota
2003	Meade Gougeon	Michigan, USA	Lake Champlain, New York
2004	John Harper	Michigan, USA	Sandusky, Ohio
2005	John Harper	Michigan, USA	Elk Lake, Michigan
2006	Don Jones	Michigan, USA	Lake Pepin, Pepin, Wisconsin
2007	Jeff Kent	Massachusetts, USA	Green Bay, Menominee, MI
2008	Greg Smith	Michigan, USA	Lake City, Minnesota
2009	Dan Backlund	Finland	Green Bay, Menominee, Michigan
2010	Jim Grogan	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No regatta		
2012	John Dennis	Minnesota, USA	Green Bay, Green Bay, Wisconsin
2013	John Dennis	Minnesota, USA	Lake Pepin, Lake City, Minnesota
2014	Ron Sherry	Michigan, USA	Lake Champlain, Plattsburgh, NY
2015	No Regatta		
2016	Ron Sherry	Michigan, USA	Lake Monona, Madison, Wisconsin
2017	Ron Sherry	Michigan, USA	Green Bay, Peshtigo, Wisconsin

NORTH AMERICAN DN CHAMPIONSHIP MASTERTROPHY (OVER 60)

Donated by Ed Kraft in 1988

Year	Skipper	Home	Regatta Site
1989	Ed Kraft	Michigan, USA	Lake Champlain, Vermont
1990	Doug Harvey	Michigan, USA	Elk Lake, Michigan
1991	Malcolm Ball	Illinois, USA	Lake Leelanau, Michigan
1992	Doug Harvey	Michigan, USA	Lake Mendota, Madison, Wisconsin
1993	Bart Reedjik	Holland	Geneva Lake, Wisconsin
1994	Doug Harvey	Michigan, USA	Elk Lake, Michigan
1995	Bart Reedjik	Holland	Montreal, Québec, Canada
1996	Bob Theissen	Michigan, USA	Lake Champlain, Vermont
1997	Bart Reedjik	Holland	Lake St. Clair, Michigan
1998	Pete Johns	Illinois, USA	Lake Champlain, Vermont
1999	Meade Gougeon	Illinois, USA	Lac St. Louis, Québec Canada
2000	Meade Gougeon	Michigan, USA	Lake Miltona, Minnesota
2001	Meade Gougeon	Michigan, USA	Saginaw Bay, Michigan
2002	No Results		Garrison, Minnesota
2003	Meade Gougeon	Michigan, USA	Lake Champlain, New York
2004	J. Bruce Williams	Michigan, USA	Sandusky, Ohio
2005	Andre Baby	Québec, Canada	Elk Lake, Michigan
2006	No Results		Pepin, Wisconsin
2007	J. Bruce Williams	Michigan, USA	Green Bay, Menominee, Michigan
2008	Jan Gougeon	Michigan, USA	Lake City, Minnesota
2009	Jan Gougeon	Michigan, USA	Green Bay, Menominee, Michigan
2010	J. Bruce Williams	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No regatta		
2012	Rick Lemberg Sr.	Indiana, USA	Green Bay, Green Bay, Wisconsin
2013	J. Bruce Williams	Michigan, USA	Lake Pepin, Lake City, Minnesota

NORTH AMERICAN DN CHAMPIONSHIP MASTERTROPHY (OVER 60)

Donated by Ed Kraft in 1988

Year	Skipper	Home	Regatta Site
2014	Don Brush	Vermont, USA	Lake Champlain, Plattsburgh, New York
2015	No Regatta		
2016	John Harper	Michigan, USA	Lake Monona, Madison, Wisconsin
2017	John Harper	Michigan, USA	Green Bay, Peshtigo, Wisconsin



2017 Gold Cup.: Photo: Peter Johanson

NORTH AMERICAN DN CHAMPIONSHIP ELLIOT SHARP TROPHY

Donated in 1962 by Eliot Sharp of New York. Awarded to the highest scoring Junior Skipper.

Year	Skipper	Home	Regatta Site
1962	Louis Loenneke	Illinois USA	Lake Honeoye, New York
1963	Louis Loenneke	Illinois USA	Mansfield, Ohio
1964	Jan Gougeon	Michigan, USA	Toledo, Ohio
1965	Louis Wake	Michigan, USA	Cass Lake, Michigan
1966	Doug Evans	Illinois, USA	Gull Lake, Michigan
1967	Dale Marshall	Michigan, USA	Lake Lamoka, New York
1968	Jim Morrison	Michigan, USA	Cass Lake, Michigan
1969	Tim Woodhouse	Michigan, USA	Lake St. Clair, Michigan
1970	Tim Woodhouse	Michigan, USA	Lake St. Clair, Michigan
1971	Tim Woodhouse	Michigan, USA	Geist Reservoir, Indiana
1972	Mat Patterson	Michigan, USA	Bantam Lake, Connecticut
1973	Ron VanWagnen	Ohio, USA	Sodus Bay, New York
1974	Bob Rednor	Michigan, USA	Hamilton Harbor, Canada
1975	Chuck Lamphere	Illinois, USA	Geneva Lake, Wisconsin
1976	Chuck Lamphere	Illinois, USA	Hamilton Bay, Ontario, Canada
1977	Rick Gibson	Michigan, USA	St. Michaels, Maryland
1978	William Gibson	Michigan, USA	Canandaigua, New York
1979	Richard Gibson	Michigan, USA	Lake Champlain, Vermont
1980	Harry Melges III	Wisconsin, USA	Saginaw Bay, Alpena, Michigan
1981	Harry Melges III	Wisconsin, USA	Hamilton Bay, Ontario, Canada
1982	William Gibson	Michigan, USA	Canandaigua, New York
1983	William Gibson	Michigan, USA	Trenton, Ontario, Canada
1984	Bruce Hay		Sandusky, Ohio
1985	No Regatta		
1986	Matt Struble	Michigan, USA	Muskegon, Michigan

NORTH AMERICAN DN CHAMPIONSHIP ELLIOT SHARP TROPHY

Donated in 1962 by Eliot Sharp of New York. Awarded to the highest scoring Junior Skipper.

Year	Skipper	Home	Regatta Site
1987	Matt Struble	Michigan, USA	Lake St Clair, Michigan
1988	Matt Struble	Michigan, USA	Trenton, Ontario, Canada
1989	Matt Struble	Michigan, USA	Lake Champlain, Vermont
1990	Matt Struble	Michigan, USA	Elk Lake, Michigan
1991	Matt Struble	Michigan, USA	Lake Leelanau, Michigan
1992	Aaron Stange	Ohio, USA	Lake Mendota, Madison, Wisconsin
1993	Aaron Stange	Ohio, USA	Geneva Lake, Wisconsin
1994	Missing Information		Elk Lake, Michigan
1995	Missing Information		Montreal, Québec, Canada
1996	No Junior Sailed		Lake Champlain, Vermont
1997	No Junior Sailed		Lake St. Clair, Michigan
1998	Brian Parker	Ohio, USA	Lake Champlain, Vermont
1999	Michał Burczyński	Poland	Lac St. Louis, Québec Canada
2000	Sam Kiefer	Minnesota, USA	Lake Miltona, Minnesota
2001	Michael Rehe	Michigan, USA	Saginaw Bay, Michigan
2002	Michael Rehe	Michigan, USA	Garrison, Minnesota
2003	Michael Rehe	Michigan, USA	Lake Champlain, New York
2004	Missing Information		Sandusky, Ohio
2005	No Junior Sailed		Elk Lake, Michigan
2006	No Junior Sailed		Lake Pepin, Pepin, Wisconsin
2007	Chester Upham	Michigan, USA	Green Bay, Menominee, Michigan
2008	Martin Schneider	Germany	Lake City, Minnesota
2009	Max Minarek	Germany	Green Bay, Menominee, Michigan
2010	Sam Myers	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No regatta		

NORTH AMERICAN DN CHAMPIONSHIP ELLIOT SHARP TROPHY

Donated in 1962 by Eliot Sharp of New York. Awarded to the highest scoring Junior Skipper.

Year	Skipper	Home	Regatta Site
2012	Sam Myers	Michigan, USA	Green Bay, Green Bay, Wisconsin
2013	Griffin Sherry	Michigan, USA	Lake Pepin, Lake City, Minnesota
2014	Griffin Sherry	Michigan, USA	Lake Champlain, Plattsburgh, New York
2015	No Regatta		
2016	Mike Clapp	New Jersey, USA	Lake Monona, Madison, Wisconsin
2017	Eddie Klemets	Sweden	Green Bay, Peshtigo, Wisconsin



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NORTH AMERICAN DN CHAMPIONSHIP

B FLEET TROPHY

Donated in 1977 by Reuben Snodgrass of Lake Ronkonkoma Ice Boat and Yacht Club

Year	Skipper	Home	Regatta Site
1977	Reuben Snodgrass	New York, USA	St. Michaels, Maryland
1978	Michael Harvey	Michigan, USA	Canandaigua, New York
1979	Ed Snyder	New York, USA	Lake Champlain, Vermont
1980	Tie: Leon LeBeau & Lou Loenneke	Michigan, USA Wisconsin, USA	Saginaw Bay, Alpena, Michigan
1981	Jane Pegel	Wisconsin, USA	Hamilton Bay, Ontario, Canada
1982	John Koeck	New Jersey, USA	Canandaigua, New York
1983	Bill Van Gee	New York, USA	Trenton, Ontario, Canada
1984	Duane McDaniels	Michigan, USA	Sandusky, Ohio
1985	No Regatta		
1986	Jim Grogan	Michigan, USA	Muskegon, Michigan
1987	Matt Struble	Michigan, USA	Lake St Clair, Michigan
1988	Don Brush	Vermont, USA	Trenton, Ontario, Canada
1989	Rich Cobb,	Connecticut, USA	Lake Champlain, Vermont
1990	John Harper	Michigan, USA	Elk Lake, Michigan
1991	Eric Loenneke	Wisconsin, USA	Lake Leelanau, Michigan
1992	Dennis Butts	Ohio, USA	Lake Mendota, Madison, Wisconsin
1993	Andreas Müller-Hartburg	Austria	Geneva Lake, Wisconsin
1994	Chris Clark	Michigan, USA	Elk Lake, Michigan
1995	Dennis Butts	Ohio, USA	Montreal, Québec, Canada
1996	Paul Gervais	Vermont, USA	Lake Champlain, Vermont
1997	Bill Condon	New Jersey, USA	Lake St. Clair, Michigan
1998	Ken Smith, Jr	Illinois, USA	Lake Champlain, Vermont
1999	James Hadley	New Jersey, USA	Lac St. Louis, Québec Canada
2000	Butch Babcock	Michigan, USA	Lake Miltona, Minnesota
2001	Jim Grogan	Michigan, USA	Saginaw Bay, Michigan

NORTH AMERICAN DN CHAMPIONSHIP

B FLEET TROPHY

Donated in 1977 by Reuben Snodgrass of Lake Ronkonkoma Ice Boat and Yacht Club

Year	Skipper	Home	Regatta Site
2002	Ken Mitchell	Ontario, Canada	Garrison, Minnesota
2003	Eben Whitcomb III	Connecticut, USA	Lake Champlain, New York
2004	Rich Lemberg Jr.	Indiana, USA	Sandusky, Ohio
2005	William Coberly	Michigan, USA	Elk Lake, Michigan
2006	Merrilli Randmaa	Estonia	Lake Pepin, Pepin, Wisconsin
2007	Mike Derusha	Michigan, USA	Green Bay, Menominee, Michigan
2008	Peter Woodruff	Michigan, USA	Lake City, Minnesota
2009	Matthias Grothues-Spork	Germany	Green Bay, Menominee, Michigan
2010	Randy Rogoski	Michigan, USA	Thunder Bay, Ontario, Canada
2011	No Regatta		
2012	Bob Struble	Michigan, USA	Green Bay, Green Bay, Wisconsin
2013	Lenny Liscio		Lake Pepin, Lake City, Minnesota
2014	Hal Bowman	North Carolina, USA	Lake Champlain, Plattsburgh, New York
2015	No Regatta		
2016	David Elsmo	Wisconsin, USA	Lake Monona, Madison, Wisconsin
2017	Griffin Sherry	Michigan, USA	Green Bay, Peshtigo, Wisconsin

THE VAN ACKER AWARD

Donated anonymously to recognize Regatta Chairmen and to remember Wm. Van Acker, worked diligently to promote the growth of the DN and to unify Europe and North America. Wm Van Acker (1921-1991) served as European Commodore for 16 years and was instrumental in the organization of the first Gold Cup.

1973	Art Teutsch	2002	John Davenport
1974	Stan Whorwood	2003	Steve Madden
1975	Jane Pegel	2004	David Zoll
1976	Stan Whorwood	2005	John Davenport
1977	Bill Connell	2006	Steve Madden
1978	Royce Fellows	2007	Dave Zoll
1979	Ed Kraft	2008	Mark Isabell
1980	Joe Norton	2009	Dan Connell
1981	Dan Gibson	2010	Richard Potcova
1982	John Day	2011	Ken Smith
1983	Bill Van Gee	2012	Dan Connell
1984	Ed Kraft	2013	Kent Baker
1985	Joe Norton	2014	Julie Jankowski
1986	Ed Kraft	2015	Kent Baker
1987	Dan Bierman	2016	Kent Baker
1988	Paul Goodwin	2017	Mike Miller
1989	Bob Schumacher		
1990	Bill Van Gee		
1991	Ed Kraft		
1992	Lou Loenneke, Jr.		
1993	Andre Baby		
1994	Harold Chamberlain		
1995	Bob Cummins		
1996	John Harper		
1997	Lou Loenneke , Jr		
1998	Bill Condon and Bob Schumacher		
1999	Andre Baby		
2000	John Floyd		
2001	Randy Rogoski		

EUROPEAN CHAMPIONSHIPS

1966 After four years of DNs in Europe, the first European (EC) was held at the Union Yacht Club in Neusiedl, Austria. The conditions were excellent and it was the first (and last) time a long distance race around the lake was part of the EC.

1967 The races were held in the centre of Stockholm, Sweden on softening spring ice. Conditions were unfamiliar to the Dutch, who had a long night of changing their angle runners after day one and finishing ahead of the pack the next few days.

1968 The sailors returned to the Union Yacht Club on the Neusiedler See. After the first race there was a storm that blew the ice out and racing continued further south on the lake for one more day.

1969 The regatta was organised by the Baltische Segler Verein, on a smooth and wet Steinhuder Meer. The BSV is a combination club of the pre-war German iceboaters from Estonia and Riga that were displaced by the Molotov-Ribbentrop pact in 1939 and the ones from East Prussia that were chased out by the Red Army in 1944.

1971 Wim van Acker set the date for this EC in Monnickendam a year before. The ice appeared three days before the event. It was a once per century event just north of Amsterdam on glass ice.

1972 The regatta was sailed in a small fleet in Gottskär, Sweden

1973 Sailing conditions were excellent in Gizycko, Poland.

1976 The EC was sailed on ice covered in wet snow on one windy day in Stockholm. and without Estonian competitors by decision from Moscow bureaucrats.

1977 The regatta was held on a new area for the first time in Krynica Morska, Poland. Ice conditions were not so good, with a lot of shell ice, giving a premium to very strong DNs.

1978 The EC was sailed in Krynica Morska again, very popular at that time. The ice was soft, leading to the sailing being done before 9 in the morning on all days, before the ice got too soft.

1979 The EC was sailed at the Union Yacht Club, Neusiedl for the third time on rough ice in light wind conditions. In a fleet of 46, Robert Ettl was unbeatable, he won all four races.

1980 The EC was sailed over two days on snow covered ice near Västerås on which the Estonian and Polish sailors had 9 in the top 10.

1981 The EC was sailed on Lake Dettern, near Vänern, Sweden on snow covered rough ice. In medium to high winds the Eastern Europeans were once again unbeatable.

EUROPEAN CHAMPIONSHIPS

1982 The regatta was sailed on the Kleine Wittensee, northern Germany. Three races were sailed the first day, on smooth ice in a stiff breeze. The next two days gave no wind followed by heavy rain, leading to an end for the regatta.

1983 For the third time in the center of Stockholm, the EC was sailed over two days on smooth ice with a thin snow cover .

1984 It was the third turn for Krynica Morska, Poland where only three races could be sailed over three days despite perfect ice conditions, because of light winds.

1985 It was the first time the EC could be sailed on Lake Vänern, the big lake we always drive past in Sweden. The smooth hard ice had snow drifts, making sailing difficult in the light conditions

1986 The conditions deteriorated on Lake Neusiedl, forcing a move to the Schlei, northern Germany where all competitors had to wait in pouring rain conditions before the first start, but the ice deteriorated over the next few days forcing a cancellation and a move to Denmark, all to no avail.

1987 The German organisers rescued the event by moving to Kungsbacka, near Gothenburg, Sweden where the regatta was sailed on smooth ice with a thin, hard snow cover that needed enough wind, which was lacking most days.

1988 The regatta was sailed from Strelna, near Leningrad, in the Russian part of the USSR the only time ever. Conditions were hard, with deep snow on soft ice.

1989 The championship was sailed from Vasa, Finland. Conditions were miserable, with lots of rain and soft wet snow. Many hotel rooms were ruined by wet equipment and sharpening in the rooms.

1990 The Polish organisers were forced north, to Lake Storsjön, Årsunda, Sweden where the regatta was sailed on the roughest ice in history. The ice was best described as paving stones and caused many breakages.

1991 The championship was held in Estonia, in the middle of its struggle for independence, in Haapsalu.

1992 The regatta was sailed in Årsunda, Sweden again. On day one, no sailing was possible because of snow and no wind. On day two, many people left because of the deep snow. Miraculously, most of the snow had been blown away for the last day. Sailing was dangerous because of snow, but things went well.

1993 The EC was sailed on Lake Mälaren, on the south coast from Sundbyholm, near Eskilstuna, Sweden from the site of Sunbyholm castle. There was snow on the ice and light winds prevented racing until Thursday, when four races were sailed. More snow and no racing after that.

EUROPEAN CHAMPIONSHIPS

1994 After a Worlds week on Lake Zegrze, Nieporet, Poland with light winds and snow there was only one day left for the EC. More snow and no wind prevented sailing.

1995 The EC was held in Västerås, Sweden in good conditions, but with the ice deteriorating later in the week because of rain.

1996 After being snowed out of the worlds in Podersdorf, Austria, the fleet moved to the small Wallersee, near Salzburg for the EC. Luckily there were smaller numbers of competitors, making reduced fleets possible on the glass ice.

1997 After a first race on soft ice on the Baltic Sea from Kalmar, Sweden, we woke up on day 2 to the ice sheet having drifted off. The regatta was finished at Växjö, Sweden.

1998 Both the Worlds and EC were sailed on Lake Pyhajarvi in Finland. Conditions for the EC had totally changed from the Worlds before with deep, wet snow drifts on day one and dangerous shell ice on day two.

1999 The event was held at a new site on on Sniardwy, the biggest lake in Poland, at Mikolajki with smooth ice as far as the eye could see. This year was a preview to future times, with the small, overextended Swiss organising group hiring the local ice boat club to do the on the ice race organising.

2000 In Fiskeboda, Sweden, there was a thin layer of snow on the smooth ice that hid some cracks, one of which ruined Tomas Lindgrens chances in race 4, on the way to his fourth race win.

2001 A difficult season that saw us end up on Lake Lipno, Czech Republic. Known for fickle winds, we sailed on the widest area, from the ferry ramp at Dolni Vltavice, south of Cerna. Conditions were difficult, with snow, but just low enough to make sailing possible.

2002 It was an experimental year, with only one event counting as both the Worlds and EC with more races planned. We sailed from Haapsalu, Estonia, but storms and windless days kept us from sailing more than normal.

2003 We all travelled to Paldiski, a former naval base town for the Soviet army in Estonia. It had just too much hard snow, and we moved across the Baltic Sea to Björkvik, south of Stockholm. The smooth ice there made for good racing.

2004 The EC was sailed on Lake Balaton, Hungary. Initially at Balatonfüred, where the conditions worsened so we moved to the southwest end of the lake where we had smooth ice at Keszthely

EUROPEAN CHAMPIONSHIPS

2005 Was another big moving year, with a start very far up north at Bergvik, near Söderhamn, Sweden. Where snow and appearance of a crack during qualifiers forced a move all the way to Pyhajarvi, Finland. After a few days of no wind and snow the EC was finally completed.

2006 The EC got under time pressure after the worlds were moved from Germany to Motala, Sweden and the primary ice sheet there blew away. But a valiant effort was made that resulted in cancellation after one race in the closing hour.

2007 The EC was sailed on a big thick sheet of ice in Haapsalu, without a second below zero all week. The top layer became softer all day every day, and firmed back up overnight. The wind conditions made for difficult races as well, but the first event organised by an experienced Polish Race Committee was a big success.

2008 The Austrian ice scouts this year travelled all over Europe to find a suitable site, to find it 100 kilometers from home in the end, at Lake Lipno in the Czech Republic. There was good wind, but the hills around made for a one-sided course. Dangerous!

2009 This year marked a return to Saint Petersburg, formerly Leningrad, in Russia, formerly Soviet Union. There was a tight organisation, making use of the best conditions early in the day.

2010 The EC was sailed at the extreme south end of Lake Neusiedl, Austria with the starting area across the border in Hungary few times. Hard smooth ice made for excellent racing.

2011 The EC was sailed from Kuressaare on the Island of Saaremaa, Estonia. The racing area was a few miles out on the Baltic Sea, towards Riga on a giant sheet of ice.

2012 After the worlds in Örebro, Sweden, the snow came, forcing a move for the EC. Despite forecasts for more snow everybody packed up and moved to Mariestad, where more snow was falling so the EC was cancelled without getting a sail.

2013 The EC this year was sailed from the iceboating city of Gizycko, Poland with a move to Lake Siemianowka on the Belarussian border after getting snowed out for a last race.

2014 The EC was sailed over two days in Haapsalu, Estonia. Good conditions out on the course, but there were holes in the ice on the way out, causing some trouble.

2015 The EC was on Lake Võrtsjärv, central Estonia. A new lake for the DN EC leading to underestimation of ice deterioration after enough races were sailed to have a valid event, leading to racing getting cancelled on the last day.

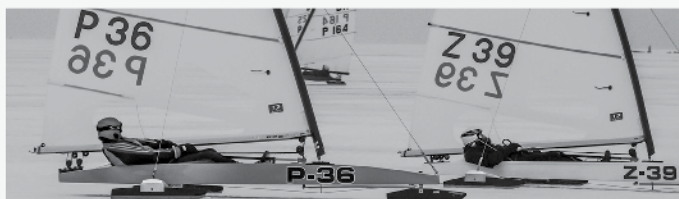
EUROPEAN CHAMPIONSHIPS

2016 After a week of high temperatures during the Worlds on Lake Glan in Norrköping, Sweden, the EC was sailed on the last morning, which was a surprise to some. The fleets sizes were smaller than during the Worlds which allowed for enough races before a forecasted storm hit.

2017 It was a difficult winter with a constant stream of weather depressions alternately ruining all our usual lakes except the extreme south lake Balaton in Hungary where a high pressure area formed over it from mid December until almost March. The massive ice was used for a good EC at Balatonfüred, until the last possible day on that ice.

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EUROPEAN DN CHAMPIONSHIP

A FLEET TROPHY

Year	Winner	Home	Regatta Site
1966	Kees Kortenoever	Holland	Neusiedler See, Austria
1967	Bouw van Wijk	Holland	Stora Värtan, Sweden
1968	Bouw van Wijk	Holland	Neusiedler See, Austria
1969	Jan Eindhoven	Holland	Steinhuder Meer, W. Germany
1970	(No Race - too much snow)		
1971	Endel Vooremaa	Estonia, USSR	Monnickendam, Holland
1972	Endel Vooremaa	Estonia, USSR	Gottskär, Sweden
1973	Endel Vooremaa	Estonia, USSR	Gizycko, Poland
1974	(No race—too much snow)		
1975	(No race—too much snow)		
1976	Olle Blomquist	Sweden	Stora Värtan, Sweden
1977	Piotr Burczyński	Poland	Krynica Morska, Poland
1978	Vello Kuusk	Estonia, USSR	Krynica Morska, Poland
1979	Robert Ettl	West Germany	Neusiedler See, Austria
1980	Bogdan Kramer	Poland	Lake Mälaren, Västerås, Sweden
1981	Bogdan Kramer	Poland	Dettern, Sweden
1982	Matti Kuulmann	Estonia, USSR	Kleine Wittensee, Germany
1983	Vaiko Voorema	Estonia, USSR	Stora Värtan, Sweden
1984	Bogdan Kramer	Poland	Krynica Morska, Poland
1985	Vaiko Vooremaa	Estonia, USSR	Gränna, Sweden
1986	(No race)		
1987	Wladyslaw Stefanowicz	Poland	Kungsbacka, Sweden
1988	Piotr Burczyński	Poland	Leningrad, USSR
1989	Piotr Burczyński	Poland	Vasa, Finland
1990	Sören Snäckerström	Sweden	Arsunda, Sweden
1991	Matti Kuulmann	Estonia,	Haapsalu, Estonia
1992	Karol Jabłoński	Germany	Arsunda, Sweden
1993	Stan Macur	Poland	Sundbyholm Mälaren, Sweden
1994	(No race, snowed out.)		
1995	Vaiko Voorema	Estonia	Västerås Mälaren, Sweden

EUROPEAN DN CHAMPIONSHIP

A FLEET TROPHY

Year	Winner	Home	Regatta Site
1996	Thomas Karlsson	Sweden	Wallersee, Austria
1997	Thomas Karlsson	Sweden	Kalmar & Väckjö, Sweden
1998	Thomas Karlsson	Sweden	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Tomas Lindgren	Sweden	Lake Sniardwy, Poland
2000	Bernd Zeiger	Germany	Lake Hjälmaren, Fiskeboda, Sweden
2001	Karol Jabłoński	Poland	Lake Lipno, Czech Republic
2002	Ron Sherry	USA	Haapsalu, Estonia
2003	Thomas Karlsson	Sweden	Björkvik, Sweden
2004	Michał Burczyński	Poland	Lake Balaton, Hungary
2005	Michał Burczyński	Poland	Lake Pyhäjärvi, Lomasakyla, Finland
2006	No Regatta		
2007	Andreas Bock	Germany	Haapsalu, Estonia
2008	Matt Struble	USA	Lake Lipno, Czech Republic
2009	Vaiko Vooremaa	Estonia	St. Petersburg, Russia
2010	Bernd Zeiger	Germany	Neusiedler See, Austria
2011	Karol Jabłoński	Poland	Kuressaare, Saaremaa, Estonia
2012	(No Race)		
2013	Karol Jabłoński	Poland	Lake Niegocin & Lake Siemianowka, Poland
2014	Karol Jabłoński	Poland	Haapsalu, Estonia
2015	Karol Jabłoński	Poland	Lake Võrtsjärv, Estonia
2016	Ron Sherry	USA	Lake Glan, Norrköping, Sweden
2017	Karol Jabłoński	Poland	Lake Balaton, Hungary

EUROPEAN DN CHAMPIONSHIP

SECOND PLACE

Year	Winner	Home	Regatta Site
1966	Jan Dick Wevers	Holland	Neusiedler See, Austria
1967	Wim van Acker	Holland	Stora Värtan, Sweden
1968	Dieter Handl	Austria	Neusiedler See, Austria
1969	Heinrich Reiter	Austria	Steinhuder Meer, W. Germany
1970	(No Race - too much snow)		
1971	Helge Rodén	Sweden	Monnickendam, Holland
1972	Jan Gougeon	USA	Gottskär, Sweden
1973	Helmuth Leppik	Estonia, USSR	Gizycko, Poland
1974	(No race—too much snow)		
1975	(No race— too much snow)		
1976	Hans Molter	West Germany	Stora Värtan, Sweden
1977	Endel Vooremaa	Estonia, USSR	Krynica Morska, Poland
1978	Bogdan Kramer	Poland	Krynica Morska, Poland
1979	Peter Koppány	West Germany	Neusiedler See, Austria
1980	Matti Kuulmann	Estonia, USSR	Lake Mälaren, Västerås, Sweden
1981	Matti Kuulmann	Estonia, USSR	Dettern, Sweden
1982	Piotr Burczyński	Poland	Kleine Wittensee, Germany
1983	Piotr Burczyński	Poland	Stora Värtan, Sweden
1984	Tiit Haagma	Estonia, USSR	Krynica Morska, Poland
1985	Karol Jabłoński	Poland	Gränna, Sweden
1986	(No race)		
1987	Piotr Burczyński	Poland	Kungsbacka, Sweden
1988	Andrzej Dalecki	Poland	Leningrad, USSR
1989	Andreas Bock	West Germany	Vasa, Finland
1990	Władysław Stefanowicz	Poland	Arsunda, Sweden
1991	Rein Veider	Estonia	Haapsalu, Estonia
1992	Andrzej Dalecki	Poland	Arsunda, Sweden
1993	Rein Veider	Estonia	Sundbyholm Mälaren, Sweden
1994	(No race, snowed out.)		
1995	Ake Luks	Sweden	Västerås Mälaren, Sweden

EUROPEAN DN CHAMPIONSHIP

SECOND PLACE

Year	Winner	Home	Regatta Site
1996	Karol Jabłoński	Poland	Wallersee, Austria
1997	Karol Jabłoński	Poland	Kalmar & Växjö, Sweden
1998	Bernd Zeiger	Germany	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Karol Jabłoński	Poland	Lake Sniardwy, Poland
2000	Thomas Karlsson	Sweden	Lake Hjälmaren, Fiskeboda, Sweden
2001	Michał Burczyński	Poland	Lake Lipno, Czech Republic
2002	Karol Jabłoński	Poland	Haapsalu, Estonia
2003	Bernd Zeiger	Germany	Björkvik, Sweden
2004	Ron Sherry	USA	Lake Balaton, Hungary
2005	Vaiko Vooremaa	Estonia	Lake Pyhäjärvi, Lomasakyla, Finland
2006	No Regatta		
2007	Michał Burczyński	Poland	Haapsalu, Estonia
2008	Ron Sherry	USA	Lake Lipno, Czech Republic
2009	Łukasz Zakrzewski	Poland	St. Petersburg, Russia
2010	Michał Burczyński	Poland	Neusiedler See, Austria
2011	Łukasz, Zakrzewski	Poland	Kuressaare, Saaremaa, Estonia
2012	No Regatta		
2013	Tomasz, Zakrzewski	Poland	Lake Niegocin & Lake Siemianowka, Poland
2014	Vaiko Vooremaa	Estonia	Haapsalu, Estonia
2015	Michał Burczyński	Poland	Lake Võrtsjärv, Estonia
2016	Michał Burczyński	Poland	Lake Glan, Norrköping, Sweden
2017	Michał Burczyński	Poland	Lake Balaton, Hungary

EUROPEAN DN CHAMPIONSHIP

THIRD PLACE

Year	Winner	Home	Regatta Site
1966	Hans Bergkvist	Sweden	Neusiedler See, Austria
1967	Sune Lind	Sweden	Stora Värtan, Sweden
1968	Jan Eindhoven	Holland	Neusiedler See, Austria
1969	Frits Meeuwissen	Holland	Steinhuder Meer, W. Germany
1970	(No race - too much snow)		
1971	Helmuth Leppik	Estonia, USSR	Monnickendam, Holland
1972	Helmuth Leppik	Estonia, USSR	Gottskär, Sweden
1973	Vello Jürjo	Estonia, USSR	Gizycko, Poland
1974	(No race—too much snow)		
1975	(No race— too much snow)		
1976	Rolf Andersson	Sweden	Stora Värtan, Sweden
1977	Rolands Berdash	Latvia, USSR	Krynica Morska, Poland
1978	Jüri Šaraškin	Estonia, USSR	Krynica Morska, Poland
1979	Bogdan Kramer	Poland	Neusiedler See, Austria
1980	Ain Vilde	Estonia, USSR	Lake Mälaren, Västerås, Sweden
1981	Stanislaw Macur	Poland	Dettern, Sweden
1982	Göran Tell	Sweden	Kleine Wittensee, Germany
1983	Bogdan Kramer	Poland	Stora Värtan, Sweden
1984	Piotr Burczyński	Poland	Krynica Morska, Poland
1985	Piotr Burczyński	Poland	Gränna, Sweden
1986	(No race)		
1987	Peter Koppany	West Germany	Kungsbacka, Sweden
1988	Andreas Bock	West Germany	Leningrad, USSR
1989	Stanislaw Macur	Poland	Vasa, Finland
1990	Matti Kuulmann	Estonia, USSR	Arsunda, Sweden
1991	Vaiko Vooremaa	Estonia	Haapsalu, Estonia
1992	Władysław Stefanowicz	Poland	Arsunda, Sweden
1993	Matti Kuulmann	Estonia	Sundbyholm Mälaren, Sweden
1994	(No race, snowed out.)		
1995	Stefan Jarrudd	Sweden	Västerås Mälaren, Sweden

EUROPEAN DN CHAMPIONSHIP

THIRD PLACE

Year	Winner	Home	Regatta Site
1996	Vaiko Vooremaa	Estonia	Wallersee, Austria
1997	Tomas Lindgren	Sweden	Kalmar & Väckjö, Sweden
1998	Karol Jabłoński	Poland	Lake Pyhäjärvi, Lomasakyla, Finland
1999	Bernd Zeiger	Germany	Lake Sniardwy, Poland
2000	Karol Jabłoński	Poland	Lake Hjälmaren, Fiskeboda, Sweden
2001	Daan Schutte	Holland	Lake Lipno, Czech Republic
2002	Michał Burczyński	Poland	Haapsalu, Estonia
2003	Ron Sherry	USA	Björkvik, Sweden
2004	Fredrik Lönegren	Sweden	Lake Balaton, Hungary
2005	Ron Sherry	USA	Lake Pyhäjärvi, Lomasakyla, Finland
2006	No Regatta		
2007	Vaiko Vooremaa	Estonia	Haapsalu, Estonia
2008	Michał Burczyński	Poland	Lake Lipno, Czech Republic
2009	Michał Burczyński	Poland	St. Petersburg, Russia
2010	Ron Sherry	USA	Neusiedler See, Austria
2011	Robert Graczyk	Poland	Kuressaare, Saaremaa, Estonia
2012	No Regatta		
2013	Peter Hamrak	Hungary	Lake Niegocin & Lake Siemianowka, Poland
2014	Ron Sherry	USA	Haapsalu, Estonia
2015	Łukasz Zakrzewski	Poland	Lake Võrtsjärv, Estonia
2016	Madars Alvikis	Latvia	Lake Glan, Norrköping, Sweden
2017	Łukasz Zakrzewski	Poland	Lake Balaton, Hungary

2018 RANKING LIST: BY RANK

The IDNIYRA Secretary shall maintain a fleet ranking. This ranking shall be updated yearly and shall be effective October 15 using regatta results from the previous two WCs (World Championships), NACs (North American Championships), and ECs (European Championships). This ranking shall be the lessor of the following:

1. Best finish in the Gold fleet in the previous two WCs, NACs, and ECs.
2. 22 plus your best finish in the Silver fleet in the previous two WCs, NACs, and ECs.
3. 40 plus your best finish in the Bronze fleet in the previous two WCs, NACs, and ECs.
4. 58 plus your best finish in the Aluminum fleet in the previous two WCs, NACs, and ECs. In the event of equal ranking for more than one sailor, the ranking which is based on the most recent race result will be considered higher.

RANK	NAME	SAIL
1	JABŁOŃSKI, KAROL	P36
1	STRUBLE, MATT	US183
1	SHERRY, RON	US44
2	BURCZYŃSKI, MICHAŁ	PI14
3	VOOREMAA, VAIKO	C6
3	ALVIKIS, MADARS	O31
3	ZAKRZEWSKI, Łukasz	PI55
3	THIELER, JAMES	US5224
4	SCHNEIDER, MARTIN	G679
4	KARDAS, DAREK	PI3
4	GRACZYK, ROBERT	P31
4	ZAKRZEWSKI, TOMASZ	P55
5	HARPER, JOHN	US60
6	KOLB, JOST	G936
6	ALVIKIS, MATISS	O311
6	ZARNOWSKI, MACIEJ	P338
7	EBLER, THOMAS	DI12
7	PETZKE, HOLGER	G890
7	HADLEY, JAMES	US1188

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
8	VOOREMAA,ARGO	C36
8	HAMRAK, PETER	M53
8	SIELICKI, RAFAL	P254
8	GROGAN, JIM	US3
8	ATKINS, CHAD	US4487
9	LONEGREN, FREDRIK	S8
9	DENNIS, JOHN	US4691
9	ORLEBEKE, STEVE	US4926
9	VUITHIER, JEAN-CLAUDE	Z39
10	EVANS, ROBBIE	US4975
10	MOORE, OLIVER	US5469
12	LENTSIUS, MAREK	C72
12	VAN RIEMSDIJK, DIDERIC	H467
13	ZEIGER, BERND	G107
13	GUSTRING, RICHARD	S713
13	CHRISTENSEN, MARK	US4824
14	EBLER, HANS	D92
14	CLAPP, DAVE	US5116
14	FRANCIS, JR	US807
15	BOHN, JOERG	G737
15	BERGER, CHRIS	US5166
15	MCDONAGH, JIM	US5214
16	FIEDLER, ANJA	G390
16	WINQUIST, JOHN	L601
16	KLEMENTS, EDDIE	S812
16	POTCOVA, RICHARD	US216
18	BARANOWSKI, WOJCIECH	P104
18	ORLEBEKE, PETER	US5501
19	VACULA, MARTIN	CZ92
19	DERUSHA, MIKE	US2545
19	ISABELL, MARK	US5014
19	REIS, GEORGE	US5053

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
20	RADZKI, JAREK	P431
20	BLOOM, MIKE	US5432
21	AKERMANN, JAAN	C64
21	STEFANIUK, MAREK	P107
22	HRIBAR, PHILIPP	OE777
22	LINDGREN, TOMAS	S81
22	WILLIAMS, J. BRUCE	US3283
23	BERZINS, ARTIS	O2
23	BROSZ, MACIEJ	P247
23	WOREK, WOJCIECH	P311
23	BERNAT, MAREK	P65
23	HOLMAN, ROBERT	US3705
23	SHERRY, GRIFFIN	US4
23	ELSMO, DAVID	US5486
23	SUGAR, TIM	US5508
23	DURR, PHILIPPE	Z50
24	SIEMS, TORSTEN	G666
24	SUOJANEN, REKO	L112
24	GRAUDUMS, GATIS	O10
24	BURCZYŃSKI, PAWEŁ	P164
24	LARYUSHENKOV, ANATOLY	R190
24	VOUGHT, DANIEL	US3937
24	RICHARDS, JULIE	US4868
24	L'HUILLER, ARNAUD	Z102
25	GRASS, KEVIN	C23
25	PTASNIK, VLADISLAV	CZ112
25	SEEGERS, CHRISTIAN	G551
25	HUBER, THOMAS	G8
25	GREVELING, PETER	H845
25	DE RUITER, DENNIS	H852
25	VAN ROSSEM, PETER	KC2766
25	BOKFORS, STEFAN	S107

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
25	BARNETT, MIKE	US5507
26	KLOOS, BEN	H962
26	ROZENBERGS, GUNARS	O6
26	VASILEV, OLEG	R1
26	LARSSON, RICHARD	S807
26	DAVENPORT, JOHN	US4961
26	BROWN, SCOTT	US5298
26	KJOLLER, JODY	US5435
26	MARTI, UELI	Z78
27	MARECEK, JOSEF	CZ101
27	MOELLER, LARS	D126
27	ORUM, LARS	D366
27	SCHLEIFER, STEFAN	P402
27	SCHNEIDER, JAKOB	P679
27	LEMBERG, RICHARD	US4155
27	LOVEJOY, GUY	US4638
27	HEARN, DANIEL	US5352
28	RIIM, RENE	C24
28	BÖTTGER, WOLFGANG	G597
28	NETHERCOTE, WARREN	KC3786
28	LUGOWSKI, RAFAL	P341
28	DIXON, TIM	US4148
28	STANGE, AARON	US4480
28	MEYER, TOM	US602
29	LAURITS, HARDI	C96
29	HOWLETT, DAVID	K13
29	CRINION, BOB	KC4536
29	BOLSCH, GEORGE	S890
29	FITZGERALD, PATRICK	US4203
29	MADDEN, STEVE	US4512
29	MEADE IV, JOE	US637
30	HOTH, MICHAEL	G102

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
30	HOTH, MATTHIAS	G677
30	CURTIS, JOHN	KC5514
30	BAKER, KENT	US5219
30	JOHANSON, PETER	US5633
30	HELMETS, EIGO	C73
31	TOLSMA, JOHAN	H580
31	WHITCOMB III, EBEN	US4775
31	GORITSKI, JACK	US5478
32	SEEGERS ANDREAS	G55
32	SUKOW, JERZY	P345
32	SCHLOEMER, ERICH	US4319
32	JONES, STAN	US4974
32	ANDERSON, ERIC	US5193
32	FROST, DAVID	US5358
32	GORDON, RICK	US5498
33	HENDRYCH, VÁCLAV	CZ123
33	JORGENSEN, POUL	D156
33	LEHMUSKALLIO, TIMO	LI03
33	TABER, JERZY	P74
33	WOLLAM, RICHARD	US4882
33	BUSHEY, JOHN	US5158
33	TEAL, CHRIS	US5285
33	MILLER, CHRIS	US5415
34	SOBERING, GEOFF	US5156
34	MILLER, MIKE	US5369
35	MADGE, MIKE	KC5449
35	MORGAS, ROMAN	P200
35	STENBERG, PATRIK	S726
35	NILES, DON	US5465
36	COBERLY, J. WILLIAM	US472
36	CUTTING, BILL	US5430
36	ALLEN, ANDY	US5451

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
36	FREDY, RUDOLF	Z42
37	SZAFRANEK, PIOTR	P96
37	DICHENKO, VALERIY	R166
37	RAST, ROBERT	US1313
37	COMTESSE, JEAN-PIERRE	Z47
38	MAALINN, RASMUS	C20
38	SIVULA, TIMO	L731
38	UHLMANN, PETER	OE213
38	JONES, DONALD	US610
38	GRAY, ROBERT	US65
39	CARAVITIS, TIMOLEON,	L37
39	EDER, BOGDAN	P51
39	BOWMAN, HAL	US1277
39	SMITH JR, KEN	US4137
39	MARC-MARTIN, PHILIPPE	Z61
40	SZCZESNY, ADAM	P243
40	HENKE, JERZY	P58
40	SODEIKA, SARUNAS	T11
40	GLICK, DAVE	US4335
40	VALENTINE, SCOTT	US4925
41	HEIDA, JAN	H534
41	PETTERSSON, MIKAEL	L65
41	PATAKY, ATTILA	M100
41	COBERLY, CHAD	US1301
41	LOENNEKE, LOUIS	US294
41	CUMMINS, ROBERT	US3433
41	SOLUM, JEFF	US4923
41	LASHAWAY, RYAN	US5393
41	HURD, CHARLIE	US5506
42	ROCEK, ROMAN	CZ100
42	ASTASHEV, YURI	R22

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
42	SJOBEG, BENGT	S609
42	LENON, JORI	US5397
42	METZLOFF, KYLE	US5522
42	CONUS, MATTHIEU	Z87
43	KROGLOWSKI, WULF	G749
43	DUNCAN, COLIN	KC5457
43	PATAKI JR., ATTILA	M101
43	BARANOWSKI, ADAM	P235
43	ZIOLKOWSKI, LESZEK	P71
43	KOZLOVA, VALENTINA	R79
43	CAVE, BOB	US445
44	VACULA, LIBOR	CZ97
44	LINDAHL, JOONAS	LI36
44	ERIKSSON, STEFAN	S881
44	KAISER, HANK	US107
44	JOHNS, PETE	US2360
44	MALCHESKI, MARTIN	US5852
45	KUKK, JÜRGEN	C89
45	EISTREIKE, RUTA	O14
45	SZUMOWSKI, RYSZARD	PI34
45	BETZOLDT, GLENN	US5172
45	COLEMAN, PAT	US5296
46	SCHWENEKER, STEFAN	G755
46	SAMPSON, ART	KC4684
46	ALVIKIS, VALDIS	O37
46	MÜLLER-HARTBURG, NIKLAS	OE221
46	PULKOV, SERGEY	R5
46	CLAPP, MIKE JR	US51161
47	MEYER, DIRK	G136
47	KÖLBL, WALTER	OE119
47	LINDEN, ROBERT	US5174
47	TRUESDELL, PETER	US5350

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
48	ZUZZANA, RYBICKA	PI19
48	OLSSON, NILS OLOV	S810
48	BROWN, BRUCE	US1610
48	RIAN, MICHAEL	US467
49	GOTTKE, DIETMAR	G4
49	SUNDSTRÖM, JERKER	L371
49	PETTERSSON, DANN	L68
49	HOEPER, PETER	US4140
49	MEREDITH, JOHN	US6001
49	VANANTY, BERNARD	Z124
50	NAJDROWSKI, JERZY	P24
50	GALICH, ALEXANDER	R10
50	JANKOWSKI, MIKE	US3271
50	GAUTHIER, RAY	US5576
50	BACHELIN, PIERRE	Z25
51	CARON, SANDRE	H707
51	WEBBER, MIKE	KC5591
51	HUBER, ROLAND	OE250
51	JANKOWSKI, JULIE	US4271
51	MAHER, ZACH	US4848
52	TYLE, JAROSLAV	CZ121
52	OBERMAIER, SEBASTIAN	G517
52	LAMMERS, ROBERT	H462
52	STUBITS, DAVID,	M20
52	LINDSTRÖM, DAG	S639
52	CLARK, CHRIS	US4789
52	THOMPSON, JASON	US5517
53	WOJTKIEWICZ, ANDRZEJ	PI5
53	MOYER, KEN	US4137
54	JUELGAARD, GEORG	D379
54	FORSTMANN, HEINER	G44
55	VITALE, NICK	US1006

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
56	PÄRTEL, VALDO	C26
56	KRING, GUNTER	G527
56	TABER, JANUSZ MAREK	P44
56	CAMARDA, ANDY	US5299
56	WOLFFE, TOM	US5593
57	KVARTSÉN, TORD	S515
58	HEILMANN, ERIK	D173
58	WILLIAMS, DONALD	US3909
58	CLEARY, TIM	US5471
58	EDOUARD, KESSI	Z29
59	PETERS, KNUT	G896
59	HOPMA, ZIJLEMA, PIETER	H667
59	WARGNIER, WIL	Z99
60	TRINK, KARL-ROBERT	C44
60	KOMAREK, VOJTECH	CZ113
60	HEFFLER, TERRY	KC4443
60	BURCZYŃSKI, PIOTR	P154
61	PENKALA, GRZEGORZ	P327
62	MATEJAK, PAWEL	P54
62	HILLYER, CHASE	US5577
64	SCHREIBER, MANFRED	G99
65	AARDEMAA, TOIVO	C4
68	SCHILLERT, HENNING	G499
68	SIMS, SPENCER	US4608
70	VAN KOMEN, ERIC	H59
71	LEUVERINK, ROBERT	H675
73	ANDERSSON, KJELL	S66
74	KISLY, MIROSLAW	P208
75	ALEKSANDROV, YURIY	R310
77	DIJKSTRA, PIETER	H485

2018 RANKING LIST: BY RANK

RANK	NAME	SAIL
79	KÖPCKE, JÖRN	G580
83	JAWORSKI, MICHAL	P14
85	ELFSTRÖM, H?KAN	S143
85	SCHORLING, ERNST	G631
86	VON WEES, ROUD	H469
87	MELLENTHIN, RAINER	G521
88	URS, KASPER	Z120
92	SVENSSON, PER-OVE	S788
95	PIECZKO, GRZEGORZ	P34



2017 North American championship. Photo: Peter Johanson



Founded in 1997, Jeff Kent and Composite Solutions has manufactured high quality composite components for the marine sector for 20 years....

In 2017, Jeff has partnered with Sam and Oliver Moore to deliver engineering, prototype design, custom and production fabrication for Iceboating and beyond...



Sam, Oliver, and Jeff along with our technical and sales engineers Chad Atkins USA-4487 (North America) and Tomek Zakrzewski P-55 (Europe) are working at a feverish pace revolutionizing mast design and construction.

Call, email, or visit us to secure the latest and greatest iceboat technology.

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IDNIYRA OFFICIAL SPECIFICATIONS

REVISED DECEMBER 2017

Note: Dimensions are given in inches in the English System (except as noted) and in millimeters (MM) in the metric system.

A. Fuselage	English		Metric	
	Max	Min	Max	Min
1. Length overall:	147	141	3733.0	3582.0
2. Beam:	21 ½	17 ½	546.0	445.0
3. Thickness of decks and cockpit floor:	¼	0.118	6.3	3.0
4. Thickness of bottom:	¼	0.118	6.3	3.0
5. Sides and bulkhead at front of cockpit:	1	5/8	25.4	15.9
6. Intersection of seat back and floor to pivot pin of steering runner:	110 ¼	98 ¼	2800.0	2496.0
7. Distance from bow to front of cockpit:	66	54	1676.0	1372.0
8. Stem block length is optional.				
9. Stem width at bow:	3 ¾	2	95.0	51.0
10. Stern block length is optional.				
11. Stern width at stern:	4	1 ¼	101.0	31.0
12. Seat backs shall be raked aft at an angle of 45 degrees plus or minus 10 degrees. They may be hinged for access to stowage compartment.				
13. Seat back shall be flat, measuring 11" (280. mm) in length at the center line, no maximum height, the crown on top of seat must be a minimum of 2" (50.8 mm) radius with minimum seat width 4".				

14. Depth of side panel at each fuselage station shall not be less than heights in the following table:

Layout of Side Panel/Minimum Side Heights			
Position along side panel starting at bow		Minimum Side Panel Height	
Inches	mm	Inches	mm
2	51	2-9/16	65
12	305	3-7/8	98
24	610	5-1/16	129
36	914	6-1/16	154
48	1220	6-7/16	164
60	1524	6-5/8	168
72	1829	6-5/8	168
84	2134	6-1/2	165
96	2438	6-3/16	157
108	2743	5-9/16	141
120	3048	4-5/8	117
132	3353	3-3/8	86
Last station: 2 inches (51mm) from stern		2	51

15. Side Panels

a. Bottom heights of side panels shall not exceed a maximum of 1" (25.4 mm) above zero line and/or a maximum of 1/2" (12.7 mm) below zero line.

Zero line shall be established by a straight line tangent to stem and stern on bottom.

b. Maximum height of side panels above zero line may be 8-1/2" (215.9 mm) including deck and bottom covering. All heights of side panels shall be proportional to height as shown in "Layout of Side Panel". The top edge of the side panel may not be concave at any point along its length.

16. Hull cross sections must be rectangular from a point 6" (153. mm) from the bow to a point 6" (153. mm) from the stern. A maximum 1/4" (6.3 mm) radius is allowed on the outside top corner of the fuselage where the deck and side panel intersect. Concavities in the deck are not permitted.

17. Cockpit floor shall be installed as shown in plans, cockpit bottom must be on top of listings. The total thickness of the cockpit floor including the bottom skin and cockpit floor skin must be a minimum of 5/8" (15.9mm). A minimum of 2 knees must be installed.

18. Structural members such as longerons, stringers, knees, listings, bulkheads, bottom, deck, etc. may be added. Deck may not protrude more than 3" (76mm) into the cockpit from the bulkhead at the front of the cockpit.

19. Design of the internal structure of the fuselage is optional.

20. Grab rails may be installed on the inside and/or outside vertical surface of the side panels. They may not extend beyond 8" (203. mm) from either end of cockpit. Rails shall not exceed 1" (25.4 mm) in depth or width. Rails are exempted from fuselage measurements.

21. Steering post and chock may be inclined in the vertical plane of symmetry.

22. Steering shall be accomplished by means of a tiller. Tiller may be of any length or shape but may not be more than 8 inches (203 mm) wide. Material is optional and may include para-aramid fabric.

23. Either steering rods, cables, or a single push pull link shall be used and shall be positioned beneath the fuselage as shown on the Official Plans.

24. Ballast, if used, shall be permanently installed.

25. Fuselage shall be constructed of wood only, except as provided in specification A.31. (Sitka spruce is most popular). Fiberglass may be added for reinforcement only. See Interpretations – General.

26. Sides shall not be hollow.

27. Minimum weight shall be 46 lbs. (21 kg) with all hardware, blocks, and tiller.

28. A full bulkhead must be installed at the front of cockpit.

29. Horizontal distance from the Bow to the pivot point of the front runner:

English		Metric	
Max	Min	Max	Min
7	3	178	76

30. National letter and sail number shall be affixed to each side of the fuselage below the mast stepping point. Color shall contrast with the fuselage and each letter and number shall be a minimum of 5.5 inches (140 mm) high and 0.7874 inches (20 mm) wide.

31. Foam is an allowed material for the internal construction of the fuselage, reference A.19

See Fuselage interpretations at the end of the Specifications

B. Runner Plank	English		Metric	
	Max	Min	Max	Min
1. Length overall (including hardware except pivot bolt):	96	94	2438	2388
2.Width at centerline	7-1/2	6-1/2	190.5	165.2
3.Thickness at centerline	1-5/8	1-1/8	41.2	28.6
4.Width at ends	7-1/2	5-1/2	190.5	139.5
5.Thickness at ends	1-5/8	1	41.2	25.5

6. Cross section is optional

7. Runner plank shall be constructed of wood. Number of laminations is optional. Fiberglass may be added. See Interpretations-General.

8.The underside of the runner plank shall be higher at the centerline than the underside of the outside ends.The profile of the plank lamination must assume a reasonable fair and continuous curve. Cutouts or hollow depressions are not permitted.

9.The runner plank, including all hardware and pivot bolts, may not weigh less than 20 lbs. (9.0 kg.).

See Runner Plank Interpretations at end of Specifications.

C. Mast	English		Metric	
	Max	Min	Max	Min
I. Dimensions				
a. Length overall (including hardware)	192	186	4876.8	4724.4
b. The maximum fore and aft dimensions, excluding hardware:	4	—	101.6	
c. The minimum fore and aft dimensions, measured from full section above boom jaw area to a point located 127-1/2" (3238.5 mm) above the mast base:	—	3-1/2	—	88.9
d. The minimum fore and aft dimensions, measured above a point located 127-1/2" (3238.5mm) above the mast base:	—	2	—	50.8
e. The maximum side to side dimension, excluding hardware:	2-1/2	—	63.5	—
f. The minimum side to side dimension, below a point located 127-1/2" (3238.5mm) above the mast base:	—	2	—	50.8
g. The minimum side to side dimension, measured above a point located 127-1/2" (3238.5mm) above the mast base:	—	1-1/8	—	28.6
h. The minimum depth of the boltrope tunnel from the outside trailing edge:	—	3/8	—	9.5

2. Materials: Aluminum; wood; carbon or glass fiber; adhesives; foam; fairing and finishing materials.

3. Other Specifications:

- Only one boltrope tunnel is permitted. The boltrope tunnel shall be substantially straight with the mast relaxed. Track is prohibited.
- Cross section profiles are optional. Mast may be solid or hollow.
- A halyard must be installed. An internal halyard is permitted.

- d. Devices which prevent or hinder movement of the boom on the mast are prohibited.
- e. The bottom of the mast must be fitted with a socket that will pivot freely on the mast step deck ball.
- f. The mast, without stays but complete with hardware and halyard used while sailing, may weigh no less than 15 lbs. (6.80 kg). Aluminum masts produced from a die existing prior to July 1, 1996 may weigh less than 15 lbs. (6.80 kg).
- g. When supported at one point in an approximate horizontal position, the mast, without stays but complete with hardware and halyard in sailing position, must balance at a point a minimum of 84 inches (2133.6 mm) from the mast base.
- h. A removable internal reinforcement of allowable materials is allowed. This reinforcement may not be altered, turned or removed during a regatta.

See Mast Interpretations at end of the Specifications.

D. Boom	English		Metric	
	Max	Min	Max	Min
1. Length (from mast):	108	Opt	2743	Opt
2. Depth (from outhaul bracket to 12", 305mm from, mast):	3	2-1/2	76.2	63.6
3. Thickness (from outhaul bracket to 12", 305mm from mast):	1-25/32	1-1/4	45.25	31.8

- 4. Only one boltrope tunnel is permitted. The boltrope tunnel shall be substantially straight with boom relaxed. An aluminum boltrope tube is allowed. It must weigh less than three ounces per foot (280 grams/meter). The internal diameter must be at least 3/8" (9.5mm). The cross section of the tube is optional.
- 5. Cross section profile is optional. Boom may be hollow.
- 6. Boom jaws are optional. Devices which prevent or hinder movement of the boom on the mast are prohibited except for check wire.
- 7. A 1/2" (12. mm) wide stripe shall be painted around the boom in contrasting color. Stripe shall be perpendicular to sail tunnel. Forward edge of the stripe shall be 8' 10" (2692 mm) or less from forward inner surface of mast sail slot projected fairly.
- 8. Boom shall be constructed of wood or aluminum. Fiberglass may be added for reinforcement only.

See General and Boom Interpretations at the end of the Specifications.

E. Runners	English		Metric	
	Max	Min	Max	Min
I. Steel plate type (steel body with stiffening elements)				
a. Plate thickness	0.27	0.23	6.8	5.9
b. Plate length:	30.0	26.0	762	660.4
c. Plate heights of at least that part contained in chock. (Note: allow for sharpening):	5.0	3.74	127	95
d. Thickness (contained by chock):	1-1/32	31/32	26.1	24.7

e. Length and height of runner assembly (plate plus stiffening elements) shall not exceed those allowed for the steel plate. Material, location and cross section of stiffening elements are optional, provided Specifications E-3 and E-4 are not violated.

f. Method of attaching stiffening elements is optional.

g. Type of steel plate is optional.

2. Wood type (wood body with attached steel angle, "T" section or steel plate). This means commercially available section. Technical Committee will rule against any special fabrications.

	English		Metric	
	Max	Min	Max	Min
a. Thickness of body	1-1/32	7/8	26.1	22.3
b. Length:	36	30	914	763
c. Height of at least that part contained by the chock:	5	4	127	101.7
d. Thickness (contained by chock):	1-1/32	31/32	26.1	24.7

e. Body shall be made of wood. Fiberglass may be added. Plate may be attached by bolts, or epoxy or both.

f. The steel sections allowed are

American Standard 3/4" (19.1 mm) x 3/4" (19.1 mm) x 1/8" (3.3 mm) "angle" and 1" (25.4 mm) x 1" (25.4 mm) x 3/16" (4.8 mm) or 1/4" (6.4 mm) "T". Equivalent metric measure (DIN) sections may be used provided the dimensions are within 20% of those listed.

g. The 'T' section must not be reduced to less than 75% of its original height by sharpening.

h.Wood type with steel insert.	English		Metric	
	Max	Min	Max	Min
Insert plate dimensions:				
Height :	3	2	76.2	50.8
Thickness	0.270	0.1875	6.8	4.8
Max width exposed below wood runner body:	1-1/2	—	38.1	—

i.The body of a wood runner may be tapered below the minimum 7/8" (22.3 mm) thickness at a point 6" (152 mm) from the front end of the runner body.The body shall not be less than 1/4" thick at the front end.

3. Profile of runner is optional with the exception that front ends of all runners shall have a 5/8" (16 mm) radius or larger.
4. Runner stiffening elements shall not project laterally more than 3" (76.2 mm) from runner edge. If runner stiffening elements are used as a weight-bearing component (in contact with ice or snow), they must be totally located within the runner cut specifications (F2).
5. Steering runner shall be equipped with a strong and effective parking brake at all times.
- 6.Weight of any individual runner shall not exceed 17 lbs. (7.7 kg).
7. Method of attaching runner to chock and chock to plank shall be accomplished as shown in plans.
8. Methods of providing relative movement of runner with respect to chock shall be as shown in plans.

9. The leading edge of the runner steel is that portion of the edge forward of the sharpened ice contact edge and higher than $\frac{3}{4}$ " (19 mm) above the ice, measured with the runner in normal sailing position. The sharpened ice contact edge of the runner steel is along the entire bottom edge and extends forward and upward along the edge of the steel to a point not exceeding $\frac{3}{4}$ " (19 mm) above the ice, measured with the runner in normal sailing position. The leading edge must be rounded to an edge radius of not less than $\frac{1}{16}$ " (1.6 mm) and shall be faired to the sides of the steel as allowed in E.12. The sharpened ice contact edge may be rounded or sharpened to an included angle of not less than 75 degrees. The camber (crown) and shape of the sharpened ice contact edge is optional. Along the sharpened ice contact edge, the rounded edge or the sharpened angle is allowed to be faired to the sides of the steel providing the thickness of the steel of plate, insert style or T runners is not reduced below the allowed minimum. (ref. E.1.a., E.2.f., E.2.h).

10. Each yacht shall be restricted to the use of nine runners during a regatta.

11.A maximum of four holes, which are not structurally required, may be drilled in each runner. Holes shall not exceed $\frac{1}{2}$ " (12.7mm) diameter.

12. Along the leading edge of the steel (ref. E.9.), the minimum allowed steel thickness of plate, insert style, and T runners must be reached within .394" (10 mm) measured from the normal tangents of the leading edge (does not apply to 'angle' type runners).

See Runner Interpretations at the end of the Specifications.

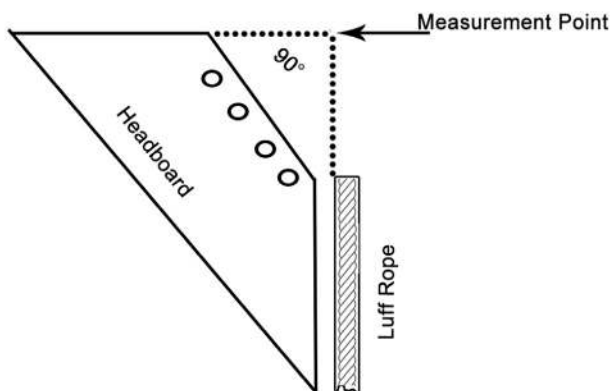
F. Runner Base and Cut (Track)

	English		Metric	
	Max	Min	Max	Min
1. Longitudinal distance from pivot axis of steering runner to pivot axis of aft runner:	103	97	2616	2464
2. Lateral distance between aft runner edges below pivot axis: (To be measured with skipper in cockpit in sailing trim.)	95	Opt	2413	Opt.

See Runner Base and Cut interpretations in the following sections.

G. Sail

1. The cloth must be made with high tenacity polyester (dacron) yarns. Sail cloth must be made with either of the following cloth constructions: 6.5 oz. cloth with 220 denier warp and 440 denier fill yarns or 280 grams/square meter cloth with 250 denier warp and 410 denier fill yarns.
 2. Luff shall be 14' (4267 mm) or less. A bolt rope is required.
 3. The foot shall be 8' 10" or less (2692 mm) or less. A bolt rope is required.
 4. Leech shall be 14'3" (4343 mm) or less.
 5. There shall be three girth measurements taken by folding the sail in quarters according to the procedure specified in G.18
Top girth measurement 3'3" (990.6 mm) or less.
Middle girth measurement 5'8" (1727 mm) or less.
Bottom girth measurement 7'6" (2286 mm) or less.
 6. Head of sail.
 - a. The width of the headboard shall be 4" (101.6mm) or less.
 - b. The overall dimension of the head of the sail, including bolt rope, shall be 5.5" (139.78 mm) or less.
 - c. The location of the measurement point at the forward upper corner of the headboard is determined by projecting a straight line parallel to the luff, excluding the luff bolt rope, to the point where it intersects with a line projected from the uppermost point of the headboard and perpendicular to the luff. Refer to diagram G.6.c. below.
- G.6.c.
FORWARD UPPER CORNER OF HEADBOARD



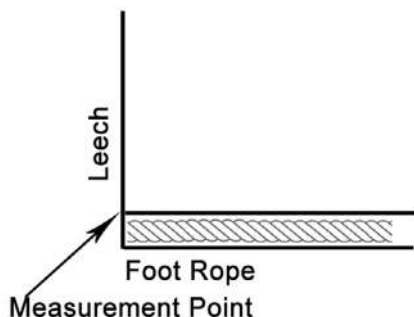
7. The sail shall be constructed with four batten pockets. The top edge of the top batten pocket must be a minimum of 27" (686 mm) below the top of the headboard. Spacing between battens shall not exceed 36" (914 mm) nor be less than 32" (813 mm). The top three battens shall be full length, extending from the leech of the sail to within 2" (50.8 mm) of the bolt rope. The bottom batten shall not exceed 36" (914 mm) in length.
8. Batten pockets shall lie at 90 degrees plus or minus 5 degrees to the leech as defined in G. 17 (Ref G.25a,5, Procedure for measuring a DN sail.)
9. Batten material and structural characteristics are optional. Batten width shall be 2" (50.8 mm) maximum.
10. Sail may have one row of reef points which cannot be located less than 18" (458mm) from the foot of the sail.
11. A yacht is restricted to the use of two sails in a regatta.
12. Altering the sail characteristics, such as area and camber, during a regatta by means other than the natural flexing of the structural members and positioning the clew grommet on the outhaul bracket are prohibited. This includes reefing. Batten adjustment is excluded.
13. National letter(s), yacht number and the letters "DN" shall be affixed to each side of the sail. Color of the letters and number shall contrast with the sail and be a minimum of 10" (255 mm) high. The letters and numbers shall be one piece, one color, block style letters on all sails made after July 1989.
14. A window in the sail is required. The window may be any shape and placed in several sections

Area (sq. in. and sq. cm.)	English		Metric	
	<u>Max</u>	<u>Min</u>	<u>Max</u>	<u>Min</u>
	1000	100	6452	645

15. A steel cable, at least 3/64" (1.2 mm) in diameter may be attached to the headboard, running inside the luff to a point outside the sail at the tack. Lower end of the cable shall form a loop which must be secured to the tack pin on boom when under sail. Distance from top of headboard to center of 1/4" (6.3 mm) bolt inserted in the loop shall not exceed 14' (4267 mm) with cable straight and under 10 lbs. (4.6 kg) of tension.
16. Sail shall not extend aft of the forward edge of the 1/2" (12 mm) boom stripe which is located 8'10" (2692 mm) from the forward edge of mast slot.

17. The leech, defined as the straight line between the forward upper corner of the headboard and the point where the leech intersects the foot at the clew, excluding the foot bolt rope, shall be measured with the sail free of battens and with 5 lbs. (2.3 kg) of tension. (Ref. G.6.c. and diagrams *Upper Forward Corner of Headboard*, G.6.c. and *Clew*, G.17)

G. 17 CLEW

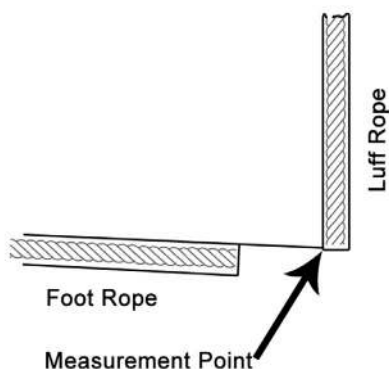


18. The three girth measurements shall be measured with battens removed and such tension as is required to remove wrinkles.
- The middle girth is measured between the mid points of the luff, excluding the bolt rope, and leech at the trailing edge. The mid point of the luff is found by folding the sail so the forward upper corner of the headboard lines up with the intersection of the luff and foot at the tack, excluding the bolt rope(s). The mid point of the leech at the trailing edge is found by folding the sail so the forward upper corner of the headboard lines up with the intersection of the leech and foot at the clew, excluding the bolt rope(s). (Ref. G 5 and diagrams *Upper Forward Corner of Headboard*, G.6.c; *Clew*, G17; and *Tack*, G.24)
 - The top girth is measured between the quarter points of the luff, excluding the bolt rope, and leech at the trailing edge. The top quarter girth point on the luff is found by folding the sail so the forward upper corner of the headboard lines up with the mid point on the luff, excluding the bolt rope. The top quarter girth point on the leech is found by folding the sail so the forward upper corner of the headboard lines up with the mid point on the leech at the trailing edge. (Ref G.5 and diagrams *Upper Forward Corner of Headboard*, G.6.c.)

c. The bottom girth is measured between the quarter points of the luff, excluding the bolt rope, and leech at the trailing edge. The bottom quarter girth point on the luff is found by folding the sail so the tack at the intersection of the luff and foot, excluding the bolt rope(s), lines up with the luff mid point. The bottom quarter girth point on the leech is found by folding the sail so the clew at the intersection of leech and foot, excluding the bolt rope, lines up with the mid point on the leech at the trailing edge. (Ref. G. 5, and diagrams *Upper Forward Corner of Headboard*, G.6.c.; *Tack*, G.24; and *Clew*, G.17.)

19. Sails shall not be colored red or orange.
20. The profile of the sail along the leech at the trailing edge shall be such that the maximum inward curve of the edge of the leech shall not exceed $\frac{3}{4}$ " from a line drawn from the back of the headboard to the outer end of the second batten pocket from the top; and from a line drawn from the outer end of the top batten pocket to the outer end of the third batten pocket from the top.
21. The profile of the leech at the trailing edge above the top batten pocket shall be such that no part of the sail extends outward more than $\frac{1}{4}$ " from a line drawn from the back of the headboard to the outer end of the top batten pocket.
22. The luff and the foot of the sail must have a boltrope that is inside the boltrope tunnel of the mast and boom while sailing, except in the area within 12 inches of the tack.
23. The tack of the sail must be fixed to a fitting at the forward end of the boom.
24. The luff, defined as a line between the forward upper corner of the headboard and the intersection of the foot and luff at the tack, excluding boltropes, shall be measured with the sail free of battens and under 5 lbs. (2.3 kg) of tension. (Ref. diagram *Upper Forward Corner of Headboard*, G.6.c. and *Tack*, G.24.)

G. 24 TACK



25. Procedure for measuring a DN sail.

- a. Preparing the sail for measuring:
 1. The sail shall be free of the mast and boom.
 2. Battens should be removed.
 3. Lay the sail on a hard surface (table, floor, or ice sailing surface).
 4. Measure each specified dimension one-at-a-time with tension applied only across the line of measurement as is sufficient to remove wrinkles except as specified in G.17, G.24, and G.25c.1.
 5. Use the following points of measurement when measuring location of battens and batten pockets and pocket angles.
 - aa. Locate the straight line leech by stretching a string or small diameter rope from the forward upper corner of the headboard to the clew, excluding the bolt rope. (ref. G.6.c. and G.17)
 - bb. The top edge of a batten pocket is located along the top of the inside width of the pocket. The centerline of the batten pocket is located in the center of the inside width of the pocket.
 6. Use the location of the Forward Upper Corner of the Headboard (ref. G.6.c.) as the measurement point when folding to measure the girths (ref. G.18)
- b. To determine the location of the top batten pocket and spacing between battens (ref. G. 7):
 1. In G.7 the measuring point on the top of the headboard is located at the forward upper corner of the headboard (ref. G.6.c.). All points along the top edge of the pocket must comply with the 27" (686 mm) minimum dimension from the head (ref. 25.a.5.bb.)
 2. The spacing between the battens is measured between the centerline of the batten pockets where the centerline intersects with the straight line leech (ref G.25.a.5.aa)
- c. To determine the angle of the battens pockets (ref. G.8.):
 1. Pin the head, tack and clew in place on the hard surface while applying tension along the luff, foot, and straight line leech to remove wrinkles. The leech at the trailing edge should not lift away from the hard surface.
 2. Measure the angle between the straight line leech and the top edge of the batten pocket. (ref. G.25.a.5.aa and bb).
 3. Measure one-pocket-at-a-time.
 4. Place the base of a protractor along the straight line leech, centered at the top edge of the batten pocket (ref. G.25.a.5.bb) and facing the luff.
 5. Read the angle at the top edge of the pocket. (ref. G.25.a.5.bb).

See Sail Interpretations at the end of the Specifications.

H. Rigging

1. Framing stays are prohibited. Any cable not shown in the plans is prohibited.
2. All stays shall be steel cable and shall be 0.118" (3 mm) or greater in diameter.
3. Halyard shall be steel cable and shall be 3/32" (2.4 mm) or greater in diameter.
4. Bobstay shall be installed and fitted to the bow tang forward and anchored at its after end on the stern block or immediately in front of the runner plank.
5. Tubes and other means of rigid adjustment of stay lengths are permissible.
6. Devices which adjust stay lengths while yacht is underway are prohibited.
7. Means for rigid adjustment of mast step location are permissible.
8. Means for adjustment of mast step location while yacht is underway are prohibited.
9. Mast step shall be rigidly mounted on the deck of the fuselage.
10. Mast step shall permit free orientation of mast.

	English		Metric	
	Max	Min	Max	Min
11. Horizontal distance from mast step pivoting point (center of ball) to pivot axis of steering runner:	41	35	1041	890
12. Horizontal distance from pivot axis of steering runner to pivot axis of steering post:	50	44	1270	1118
13. Distance from lower mast hound bolt (pivot bolt located in front of mast) to base of mast:	139-1/2	127-1/2	3543	3239
a. All stays shall connect to a common component (which may consist of one or more sub-components) which attaches to the mast hound at a single point.				

14. Six sheet blocks shall be installed.
15. Four sheet blocks shall be installed aft of the rear limit of the cockpit floor: two on the boom and two on the deck. None of these blocks may incorporate a one-way feature. The blocks on the fuselage must be fastened in a manner such that anything penetrating the deck may only be performing the function of fastening, and no other function.
16. One sheet block shall be located forward on the boom within 1' (304 mm) of the mast. This block may incorporate a one-way feature.
17. One sheet block shall be installed on the tiller post. This block may incorporate a one-way feature.
18. The sheet must be attached to the boom and pass through all blocks as shown in plans.
19. Blocks must be individual and have fixed positions on fuselage and boom.
20. Sheet may run inside the boom between the forward block and the 2nd block.

	English		Metric	
	Max	Min	Max	Min
21. The distance from the center of the runner plank to the base/foot of the side stay on the end of the plank must be:	—	43.07	—	1094

22. The components of the mast hound may affect the free orientation of the mast by either forcing mast rotation or limiting mast rotation.

See Rigging Interpretations at the end of the Specifications.

I. Fittings

1. Diameter of the sheaves of the sheet blocks shall not exceed 4" (101.6 mm).
2. Two of the six required blocks may incorporate a one-way feature.
3. Steering chock may incorporate a shock-absorbing feature.
4. Hardware need not conform to plans as long as specifications are not violated and hardware performs the same function that the plan item performs.

	English		Metric	
	Max	Min	Max	Min
a. Width of runner slot at the smallest dimension where the chocks come in contact with the runner's side or stiffening element:	1-3/32	31/32	27.7	24.7
b. Depth of chock:	3-1/2	2-7/8	88.9	73.1
c. Length of chock:	9	6-3/4	228	172
d. Width of chock:	4-3/4	—	121	—
6. Mast Step - Height of pivoting point (center of ball) above deck:	1-5/8	1-1/8	41	28.6

7. Location of Halyard catch on mast is optional.
8. Halyard must be capable of lowering and raising the sail with yacht in upright position.
9. Additional fittings to secure halyard to mast are permissible.
10. Only one mast, boom, fuselage, and runner plank may be used for an entire regatta, unless broken beyond reasonable repair (as interpreted by the Race Committee).
11. An adjustable footrest may be used.
12. A device may be placed in the back of the chock to prevent the front of the runner from tipping downwards. The device may not contact the runner when all three runners are on the ice.
13. The side chock and the steering chock shall be made of steel, stainless steel or aluminum.

14. Bow Tang	English		Metric	
	<u>Max</u>	<u>Min</u>	<u>Max</u>	<u>Min</u>
a. Width	3-3/4	—	95.3	—
b. Horizontal distance from the pivot axis of the steering runner to the aftermost end of the bow tang:	8	—	203.2	—
c. Height above a horizontal line extending forward from the deck at a point 2" (50.8 mm) behind the bow:	3	—	76.2	—
15. Runner Plank Mounting Hardware				
a. The runner plank mounting system shall not extend more than 4" (101.6 mm) outside of the side panel of the fuselage.				
b. The runner plank mounting system shall be located in an area that does not exceed 30" in the fore and aft dimension.				
16. Bobstay Strut, including Mounting Hardware	English		Metric	
	<u>Max</u>	<u>Min</u>	<u>Max</u>	<u>Min</u>
a. Height (from bottom of skin of fuselage):	—	3	—	76.2
b. Length:	6	—	152.4	—
c. Width:	4	—	101.6	—
17. Mast Step Mounting Hardware				
a. Length:	9	—	228.6	—
b. Width:	3	—	76.2	—

See Fitting Interpretations in the following section.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

Effective December 16, 2017

General:

12/16/2017: Material – Kevlar cloth may not be used in DN constructions with the exception of the tiller; reference Spec A22.

11/24/87: All DN racing equipment must be in accordance with the DN specifications. Whether or not he reveals the structural components of his equipment, the competitor is responsible for making sure his equipment is in compliance with the DN specifications.

3/18/89: Specification A.25. specifies wood in the fuselage with fiberglass added for reinforcement only. Carbon fibers and other materials are not allowed. Specification B.7. specifies wood in the runner plank and fiberglass may be added. Carbon fibers and other materials are not allowed. Specification D.8. specifies the boom will be of wood or aluminum. Carbon fibers and other materials are not allowed.

A. Fuselage

11/24/56: Members (such as fiberglass, stringers, knees and longer stem blocks), may be added after the specifications have been complied with and providing the addition does not exceed the specifications.

11/10/73: Sides must be solid wood.

9/23/82: Fiberglass may be used between laminations of the side panels for reinforcing.

10/17/83 amended 1/19/2013: With respect to the hull side profile: the term proportional” will be defined as a “smooth curve” without reversing the line abruptly. Concave sections will not be allowed. A side profile is acceptable if there are no concavities, the maximum panel height is not exceeded, and the height at each station is not less than the specified minimum. Reference specifications A14 and A15.

3/18/89: The tiller post must be located forward of the bulkhead at the front of the cockpit. The tiller must be attached to the post at a point above the level of the deck. The sheet block that is installed on the tiller post (Specification H.17) must be attached to the tiller post at a point higher than the point of attachment of the tiller

3/26/89: Definition of deck as used in A.18: the deck is the covering on the top of the fuselage in the areas foreword and aft of the cockpit. The deck extends uninterrupted from the outside of the side panel on one side to the outside of the side panel on the other side.

4/15/91: In A.14, the depth of the side panels does not include the deck and bottom.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

7/01/92: The bulkhead at the front of the cockpit must be solid wood and may not be hollow.

7/01/92: To comply with A.9, the forward most part of the stem or a portion of the stem if it is vertical, must comply with the minimum and maximum dimensions allowed.

12/15/2011: Rigging and fittings are not part of the integral structure or reinforcement of the fuselage and may be attached to the fuselage by mechanical fastenings and/or adhesives. The materials allowed in the rigging and fittings attached to the fuselage are optional providing they comply with the Specifications in sections H. and I. and the Interpretations in sections General, H., and I. The dimension and location of the rigging and fittings must comply with the Specifications in Sections A., H., and I. and the Interpretations in sections General, A., H., and I.

12/01/2012: In the side panels it is not allowed to use engineered or composite materials formed by binding particles or loose fibers of wood and fiberglass.

02/15/2013: The bow tang should not be included when measuring the length of the fuselage, specification A.1., and the distance from the bow to the front of the cockpit, specification A.7.

B. Runner plank

1/30/88: The overall length of the runner plank (including hardware except pivot bolt) is measured in a straight line from end to end without the skipper in the cockpit.

11/14/88: Gull wing runner plank: In the profile curve of the runner plank lamination, the curve must not reverse direction and in an unloaded condition no part of the curve may be lower than the ends.

7/01/92: The interpretation dated 1/30/88 is changed to read: The overall length of the runner plank (including hardware except pivot bolt) is measured in a straight line from end to end with the runner plank separate from the fuselage.

11/30/98 amended 10/6/2011: The runner plank must be constructed of wood and meet all minimum dimensions in specifications B.1., 2., 3., 4., 5. before the application of external reinforcement and coatings; and meet all the maximum dimensions in specifications B. 1., 2., 3., 4., 5. after the application of any external reinforcement and coatings. Internal fiberglass reinforcement is not allowed. Foam, honeycomb, and other non-wood core materials are not allowed.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

1/15/2010: When the use of the reinforcement bar or stiffening element that is associated with the “Kent” style chock does not comply with interpretations I. Fittings dated 1/15/2010, and E. Runners dated 1/15/2010, the bar will be considered part of the chock and must comply with the materials as specified in I. 13. and will be included as hardware in determining the overall length of the runner plank as specified in B. I.

C. Mast

10/17/83: It is allowed to use a mast which is reinforced with carbon fibers from the inside as well as the outside.

10/17/83: It is allowed to reinforce an aluminum mast with a piece of wood from the inside.

7/01/96: Masts built prior to July 1, 1996, shall be considered legal if they meet prior specifications.

11/30/98: The minimum weight and balance point specifications (C.3.g and C.3.h) must be met with and without any removable internal reinforcement.

11/30/98: The mast must be one piece when used in a regatta.

02/15/2013: In specifications C.3.f. and C.3.g, the term “stays” includes all components and devices used to connect the cables to the mast hound at the height of the lower mast hound bolt, reference specification H.13., All components and devices used to connect the cables to the mast hound at the height of the lower mast hound bolt must be removed from the mast when determining the weight, C.3.f., and the balance point, C.3.g.

D. Boom

01/01/79: The boom jaw may not have a projection on the bearing surface that will fit into the mast slot and thereby affect the rotation of the mast.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

E. Runners

1974: A slot may be machined on the top of allowed ``T'' sections to facilitate mounting to wood body.

10/17/83: It is permissible to reinforce wood runner bodies (of regulated thickness) with materials like carbons, aluminum, tin, and similar materials. No kevlar.

11/23/87: Specifications for the thickness of ``T'' sections and for insert steel are not the same.

11/23/87: Internal reinforcement of the wooden runner body: The use of threaded rod or bolts to attach ``T'' iron to the wooden body with the rod extending up to the top of the wood body is a well accepted method of constructing these runners. The wood body of insert runners must meet all the requirements of Section E of the Specifications.

3/18/89, amended 10/1/2010 : The thickness of the steel plate in insert runners may not be reduced below the minimum thickness of .1875" except as permitted in specification E.12.

3/18/89: Specification E.2.h. establishes the maximum thickness of the steel plate in insert runners at .270 inches. Therefore, the absolute maximum thickness is .270" and plate in the thickness range .271" through .279" is not allowed.

11/14/89: Wood or metal stiffeners may be added to insert runners (as in E.1.e. for plate runners) as long as the minimum wood body meets the dimensions in E.2.a. and the stiffener is outside the wood body. No wood or metal stiffener may come between the insert plate and the slot on the wood body. No metal stiffening is allowed inside the main wood body.

7/1/92: The specifications do not prohibit the changing of the runner stiffening elements during a regatta. However, when the stiffening element is changed, the runner is then counted as a new runner in total count of the nine runners allowed.

7/1/92: Commercially available T, angle, or plate is manufactured in a quantity to be sold to people who want to purchase it. If the T, angle or plate is made only in small quantities for a few people, it is a special fabrication and not allowed to be used. A modification of a commercially available T, angle, or plate by procedures such as surface grinding, milling, bending or flattening is allowed.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

11/30/98: The body of a wood body runner must be constructed of wood and adhesive only. No internal reinforcement is allowed except for threaded rod, or bolts, or screws used to attach the steel to the body. The wood body must meet all specifications before the application of external reinforcement. Reinforcement between the blade and body (inside the slot) on insert runners is considered to be outside the wood body and is allowed.

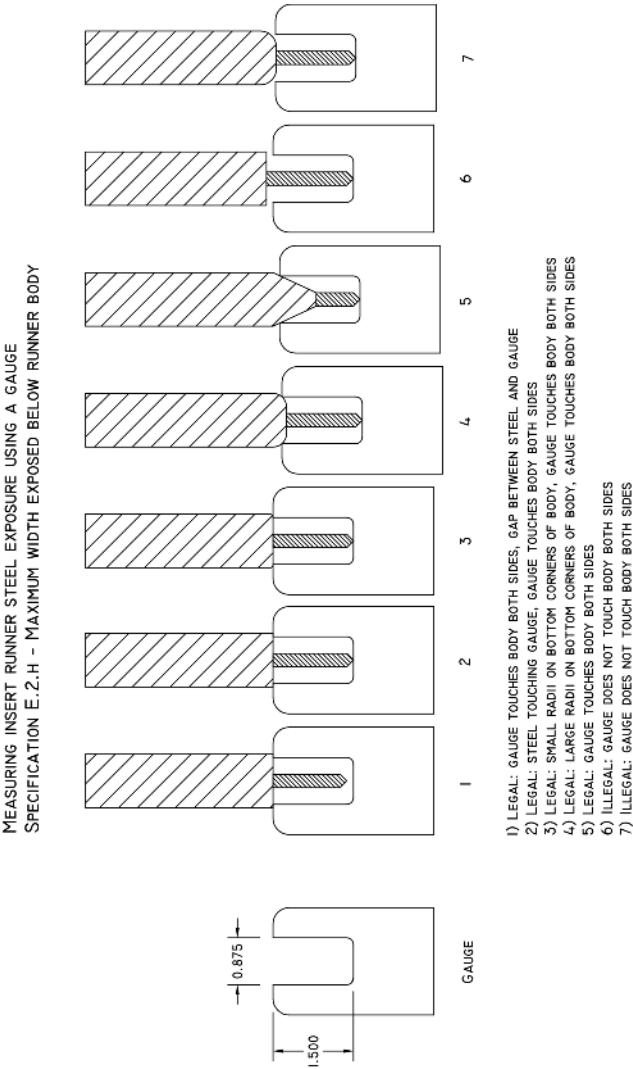
2/10/07: Measuring spec. E.2.h: A simple measuring gauge may be used to measure the amount of exposed steel on insert runners. The gauge should have a slot width at the minimum wood body thickness (7/8" or 22.3 mm) and depth at the maximum allowed exposure (1-1/2" or 38.12 mm) .

This gauge is used as shown on the next page (120) to measure exposed steel. The top of the gauge should touch the bottom of the runner body. The edge of the steel may touch the bottom of the slot, or there may be a gap between the edge of the steel and the gauge. If the edge of the steel is touching the bottom of the slot and both top edges do not touch the bottom of the runner body, then the exposed steel is greater than the maximum allowed.

Notes:

1. Because it is impossible to determine the thickness of any reinforcement on the bottom of the runner body, the gauge should be used to measure to the outside of the reinforcement.
2. This gauge should not be used on the front 6" (152 mm) of the runner (This gauge should not be used on the front 6" (152 mm) of the runner (where Spec. E.2.i. allows the body thickness to be reduced below 7/8")
3. Allowance should be made for small imperfections (porosity, damage, etc.) on the bottom surface of the runner body.

INTERPRETATIONS OF THE OFFICIAL MEASURE INSERT RUNNER STEEL EXPOSURE USING A GAUGE



INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

1/15/2010: The reinforcement bar or stiffening element that is associated with the “Kent” style chock is part of the runner and must meet all requirements of a runner stiffener, Reference Specifications E.1.e; E.1.f; E.4; Interpretations B. Runner Plank, 1/15/2010; E. Runners, 11/14/89, 7/1/92, 1/15/2010, and I. Fittings, 1/15/2010

1/15/2010: All runner stiffening elements are considered part of the runner and are included in the runner weight. Reference Specification E.6.

1/15/2010: The specification making optional the method of attachment or stiffening elements to plate runners, Specification E.1.f. also applies to the method of attachment of stiffening elements in wood body runners. Attachment is defined as a physical connection that firmly adheres the stiffening element to the runner such that when the chock pivot bolt is removed, the stiffening element remains physically connected to the runner. Reference Interpretation 7/1/1992.

1/15/2010: At all times while in use the bar or stiffening element that is associated with the “Kent” style chock must be attached to the runner. Any movement of the bar or stiffening element shall be independent of and not controlled by the movement of the chock pivot bolt.

4/14/2010: In a previous 1974 interpretation the second sentence referring to hard weld on the ice contact edge of allowed “T” sections is deleted. This interpretation now reads: A slot may be machined on the top of allowed “T” sections to facilitate mounting to wood body.

4/14/2010: Hard weld may be applied to the ice contact edge of all runners.

4/14/2010: A previous 1974 interpretation that disallowed the welding of a bead in the corner of “T” runners is deleted and replaced with a new interpretations: Runner “T” sections may not be formed by welding and may not be altered by welding a bead in the corners.

4/14/2010: The steel angle section allowed in E.2.f need not be mounted symmetrically on the wood body but must be mounted to the wood body in a manner that the ice contact edge corresponds to the apex of the included angle of the steel section.

10/1/2010: In Specification E.9., the $\frac{3}{4}$ ” (19 mm) dimension above the ice which establishes the upper limit of the sharpened ice contact edge shall be determined according to the diagram “19 mm Dimension”. Line A represents the official measurement line.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

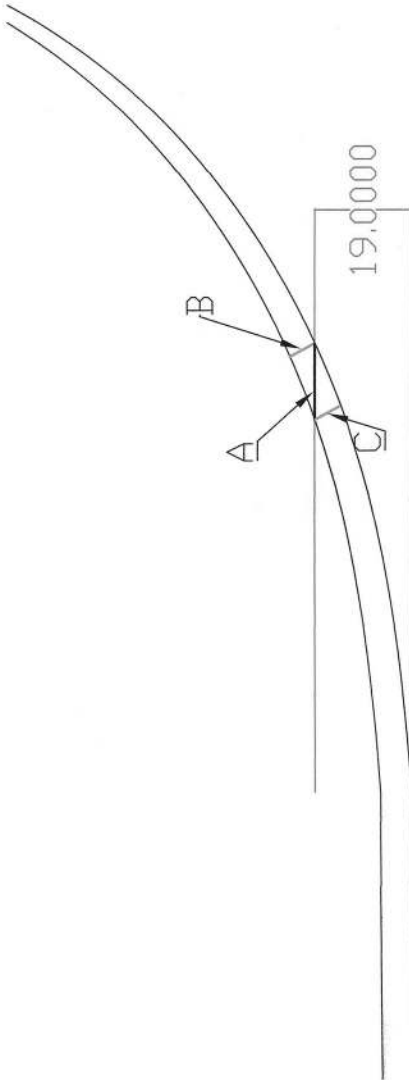


Diagram 19 mm Dimension

In Specification E.9., the $\frac{3}{4}$ " (19 mm) dimension above the ice which establishes the upper limit of the sharpened ice contact edge shall be determined according to diagram "19 mm Dimension".

Line A represents the official measurement line.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

12/01/2012: The interpretation dated 11/14/89 is amended to read as follows: Wood, metal, or carbon stiffeners may be added to wood body runners (as in E.1.e. for plate runners) as long as the minimum wood body meets the dimensions in E.2.a. and the stiffener is outside the wood body. No wood, metal or carbon stiffener may come between the insert plate and the slot in the wood body. No metal or carbon stiffening is allowed inside the main wood body.

12/01/2012: It is allowed to apply a coating to cover the surface of the steel plate, angle, or T used in a runner. The steel, without the coating, shall comply with the dimensions specified in E.1.a., b., c.; E.2.f. and E.2.h.

01/15/2013: In runner specifications and interpretations runner stiffening elements, i.e. "stiffeners", and runner reinforcement have differing allowed uses, allowed materials, and dimensions.

On plate style runners, stiffening elements are controlled by specifications E.1.; E.1.d., e., f.; E.4; E.6. and interpretations E. Runners dated 11/14/89, 7/1/92; four interpretations dated 1/15/2010; and I. Fittings dated 1/15/2010

Stiffening elements and reinforcement of the body of wood body runners are controlled by specifications E.2.a., d., e.; E. 4.; E. 6. and interpretations E. Runners dated 10/17/83; 11/23/87; 7/1/92; 11/30/98; four interpretations dated 1/15/2010; 12/01/2012; and I. Fittings dated 1/15/2010

On wood body runners, allowed material added to the body which is outside the allowed maximum body thickness of 1 1/32 inch (26.1 mm) is considered a runner stiffener or stiffening element. Allowed material added to the outside of the wood body, or in the slot of insert style runners, that does not exceed the allowed maximum body thickness of 1 1/32 inch (26.1 mm) is considered external reinforcement.

05/23/2013: It is not permitted to reduce the thickness of the runner steel below the allowed minimum by rounding, fairing, or tapering except as specified for the leading edge in Specifications E.9 and E.12. Refer to the interpretation E. Runners dated 10/01/2010 and the diagram "19 mm dimension". The 3/4" (19 mm) dimension does not establish a line extending along the runner parallel to the sharpened ice contact edge below which the thickness of the runner steel is allowed to be less than the specified minimum.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

02/17/2014 Runners are not considered ballast and runners of different weights may be used during a regatta, provided each runner complies with the specifications and interpretations under E. Runners.

02/17/2014 The interpretation dated 1977, which reads: "It is legal to change runners from a light set (6lbs) each to a heavy set (17 lbs) each during a regatta and not be in conflict with the change of ballast", is deleted.

F. Runner Base and Cut (Track)

3/12/2007: Measuring F.2: In order to make repeatable measurements of runner cut as intended by this Specification the following methods should be used:

1. Rigging should not be tight during measurement. The skipper must be allowed to loosen the rigging as required to relieve static tension.
2. Sailor should be wearing normal racing attire, including helmet, goggles, etc.
3. Mainsheet must be loose.
4. The runners must be allowed to move. If on the ice, the boat may be pushed to allow the runners to seek a neutral position. If using a measurement fixture, the device must allow free sideways movement of the runners.
5. The skipper should be in the cockpit in normal sailing position (lying in the cockpit with his shoulders against the seatback).

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

G. Sail

1977: It is not legal to use two ply Dacron 6.5 oz cloth for the top panel and bottom panels for reinforcement.

9/1/99: It is legal to use Contender 6.5 oz. Polycote cloth in the construction of the sail.

5/6/04: The specification of weight is a nominal cloth weight. This specification of 6.5 oz/sq.yd. refers to a "sail maker's yard", which is 36" X 28.5". Sails built prior to July 1, 2003, which complied with the specifications at the time of manufacture will be allowed in all IDNIYRA regattas.

11/29/2010: In specification G.9. the batten width is measured at 90 degrees to the length. Batten thickness is the smallest of the three dimensions, i.e. length, width, thickness. There are no restrictions on batten thickness.

11/29/2010: Specification G.12. excludes batten adjustment from the factors that alter sail characteristics. Battens may be fitted only in the pockets allowed in G.7. and may protrude from the pocket at the leach end for the purposes of providing a means to secure the batten in the sail and adjusting the tension. There is no restriction on the tensioning of battens in the pockets and at any time battens may be interchanged in the allowed pockets.

11/20/2011: The batten pockets in all sails made prior to 1/1/2012 shall lie at 90 degrees plus or minus 8 degrees to the leech. These sails may be used in all DN races, including all championships. All other sails must comply with Specification G.8.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

H. Rigging

01/01/78: Boom pulleys may not be attached inside the boom by slotting the bottom of a boom and inserting the pulley into the slot and then picking up the sheave with a bolt or rod.

01/01/82: A track may be mounted on the boom for sliders to which the pulleys may be attached.

09/23/82: Mounting Pulleys on deck or boom by any means is acceptable as long as the rope and sheave are outside the boom or deck. No recessed block will be allowed in which part of the sheave or rope is below the back deck. (see also 11/1/84 interpretation)

10/17/83: It is not allowed to use a shock absorber (spring) between side stay and the fitting on the end of the runner plank.

11/1/84: It is not permissible for any part of the sheet block, or any hardware integral with the mounting of the sheet block, to be below the projected top surface of the deck, with the exception of the screws or bolts used to fasten the hardware to the deck.

11/10/73 & 11/14/88: Cable may not be used in place of mast hound. Hound may be made from strap or plate material. No leaders or bridles allowed.

I. Fittings

9/23/57: An adjustable footrest may be used.

9/23/57: Shockabsorbing steering chock steering chock must be according to plan, but springs or rubber grommet may be added as shock absorber.

11/10/73 & 11/14/88: There is no limit to the number of holes in the tack fitting. Tack pin location may not be changed during a regatta.

11/10/73: It is not allowed to have two holes in the chock.

11/10/73: The number of straps on the mast hound is optional.

11/23/87: It is not allowed to use a long fitting to connect the mast hound with the head stay so that the position of the mast hound can be moved (within the allowed dimensions) without having to make a new stay.

INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS BY THE TECHNICAL COMMITTEE

4/15/90: The degree to which the side runner pivots vertically in the chock may be restricted only by way of the friction on the sides of the chock which is controlled only by the tightening of the pivot bolt. Any other device to restrict the vertical movement of the runner is not allowed, with the exception of devices as allowed in Specification I.12.

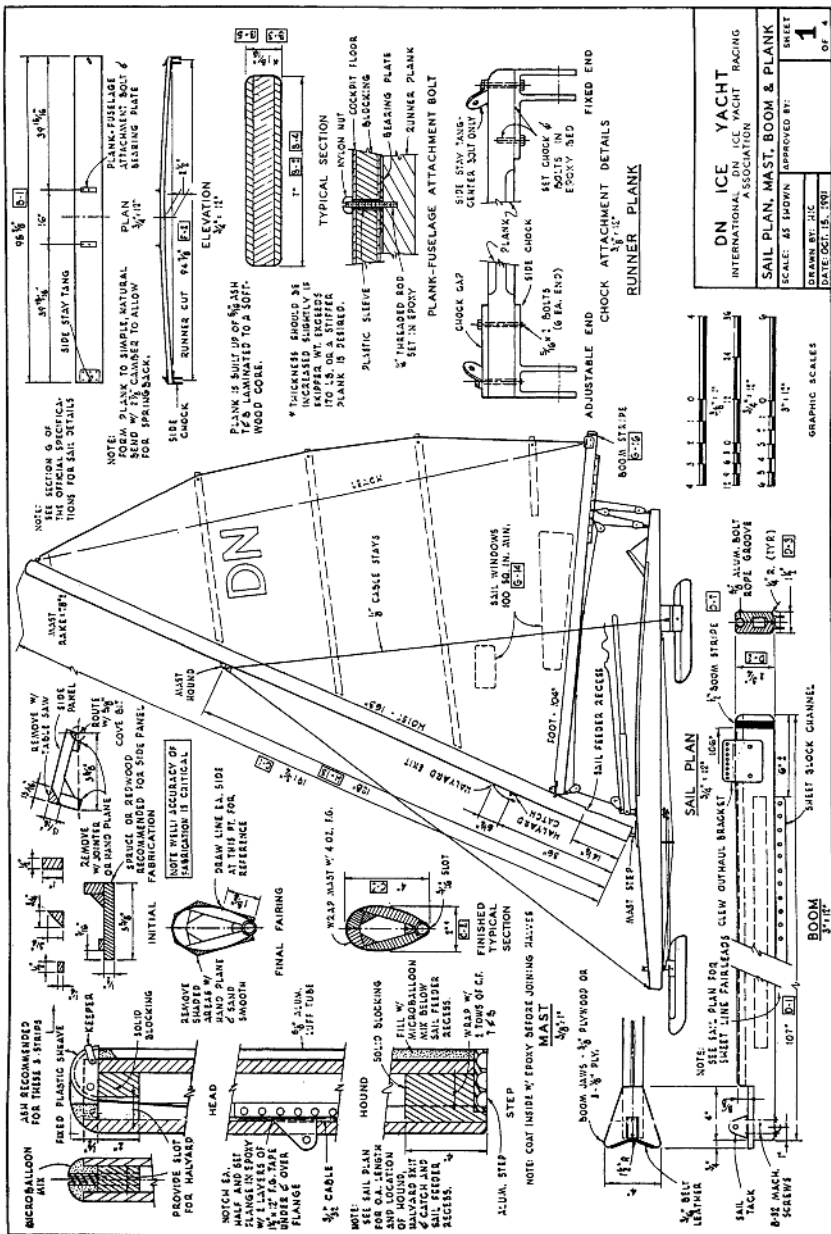
11/12/03: It is allowed to use a fixture providing additional height to mount the rear sheet blocks to the deck. This fixture must be mounted on the surface of the deck and may not be integral with the deck structure. This fixture must provide a fixed position for the block attachment (the blocks may not be moved while underway)

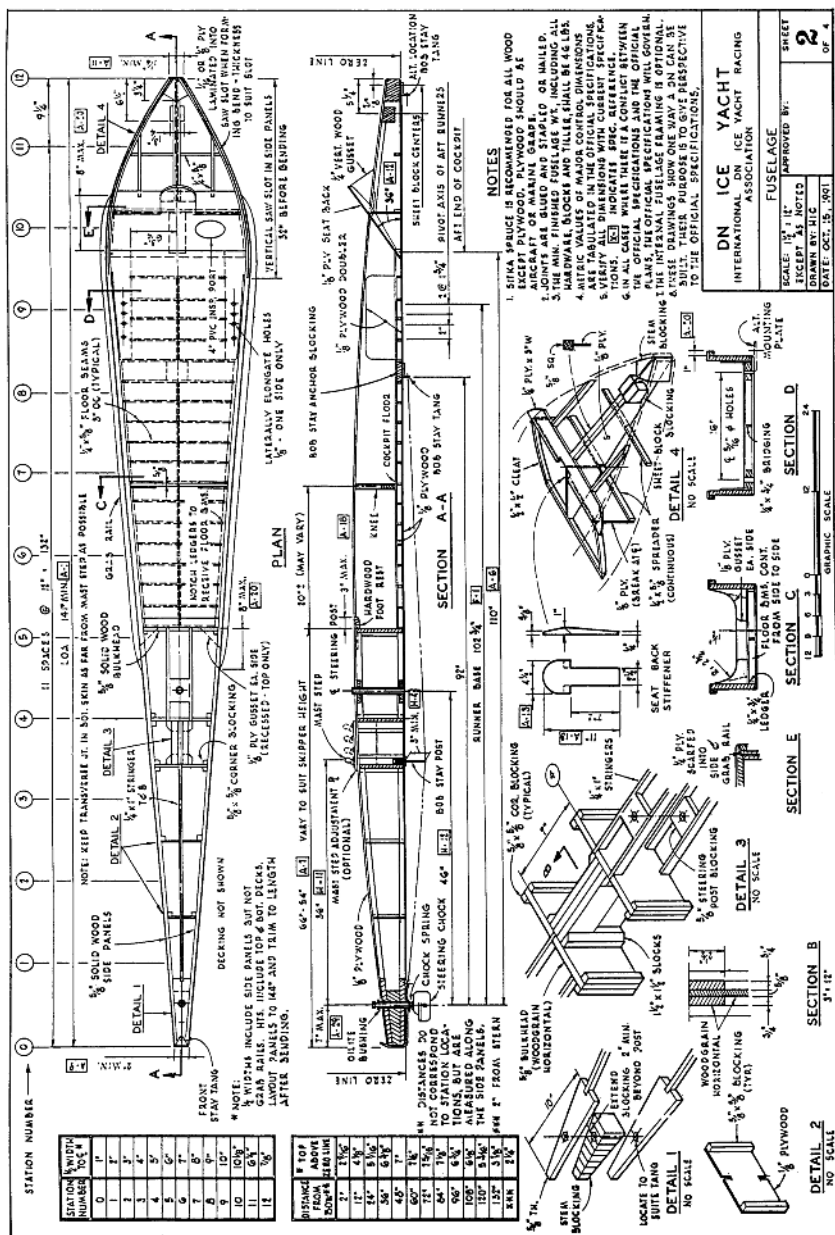
12/12/07: “Kent” style chock – Either the inside or outside flange of the side chock may be reduced in size to allow clearance for runner stiffening elements. When viewed from the side, the profile of the chock must meet all dimensions specified.

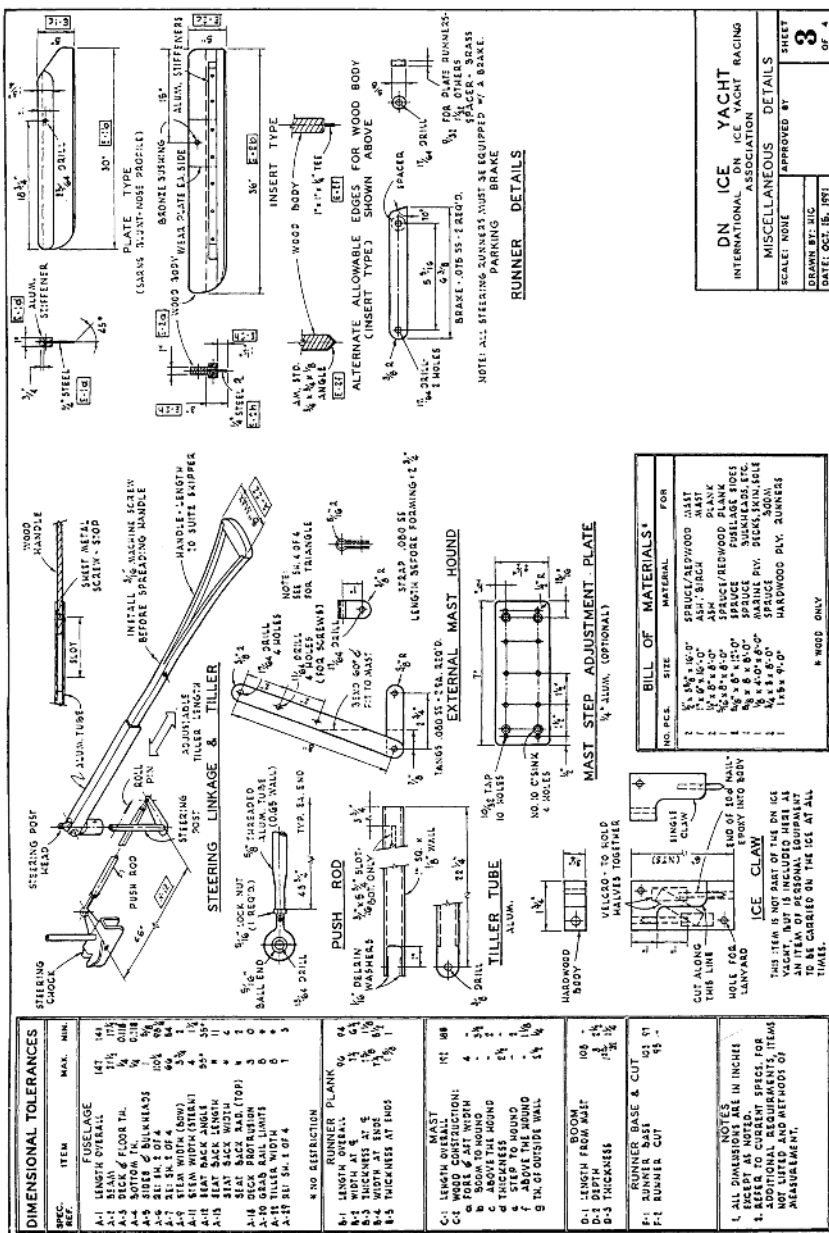
1/15/2010: The reinforcement bar or stiffening element that is associated with the “Kent” style chock is allowed to be attached only to the runner and must meet all requirements of a runner stiffener. Reference Specifications E.1.e; E.1.f.; E.4; and Interpretations E. Runners, 11/14/89; E. Runners 7/1/92; E. Runners 1/15/2010.

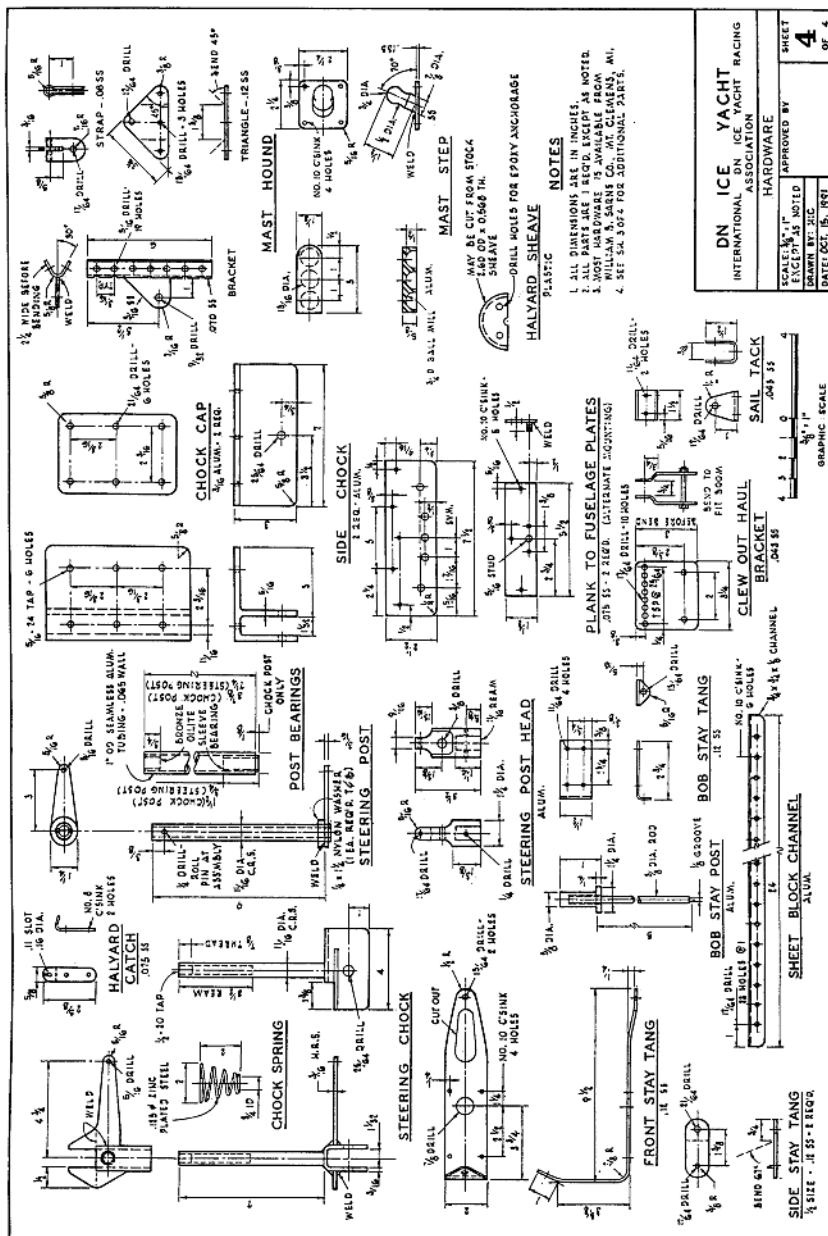
12/01/2012: It is allowed for there to be more than one position for the mast hound or an adjustable fitting with multiple positions, providing the location of the lower mast hound bolt (pivot bolt) is within the range specified in H.13. The position may not be adjusted while the yacht is underway.

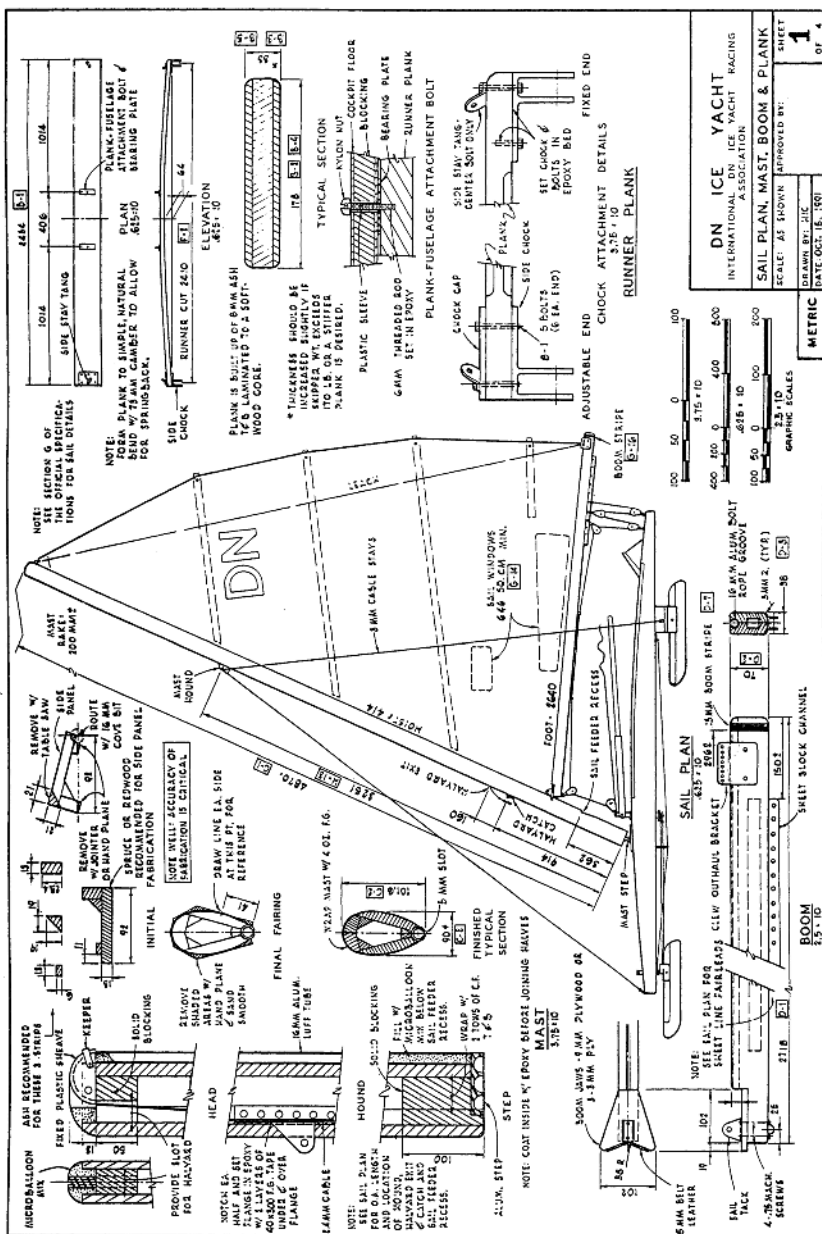
9/20/2013: The bow tang, reference specification I.14 must be mounted at the forward most (bow) end of the fuselage and all parts of the tang, with the exception of the fastenings attaching it to the fuselage, must be outside the fuselage.

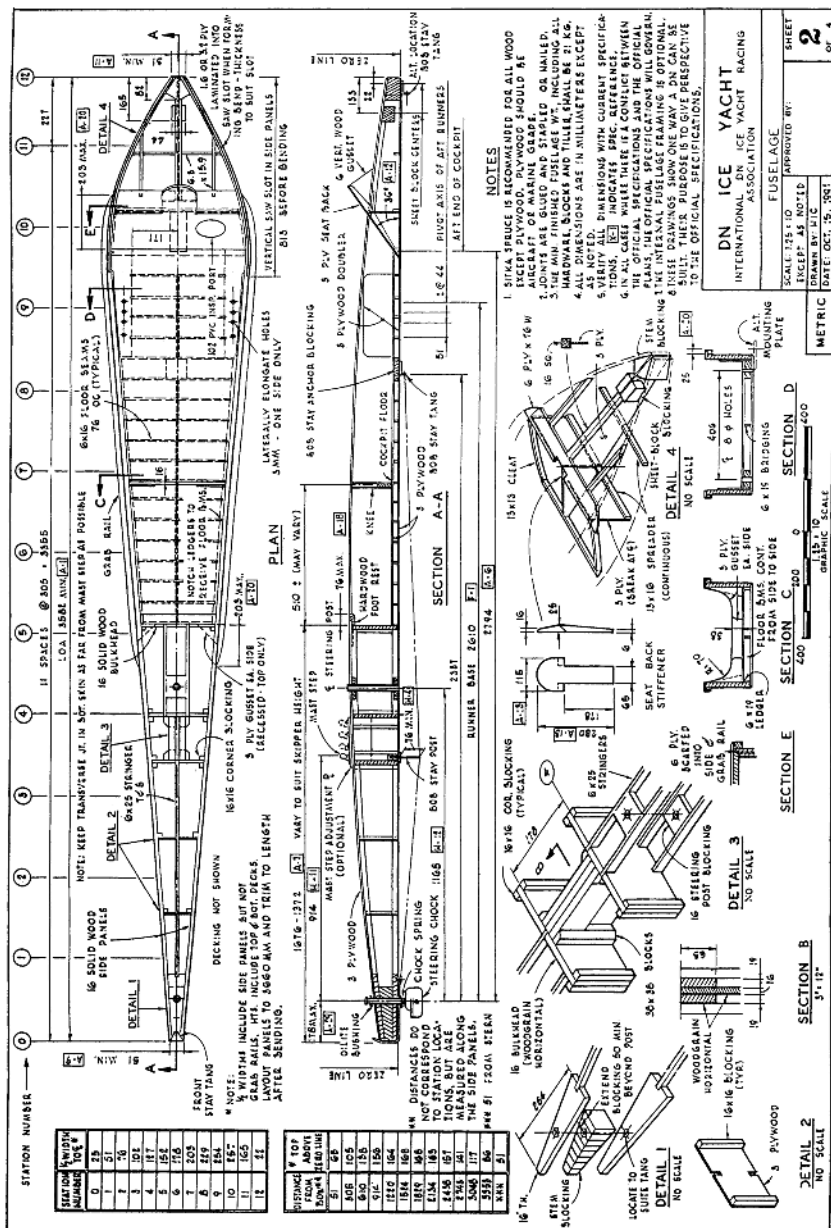


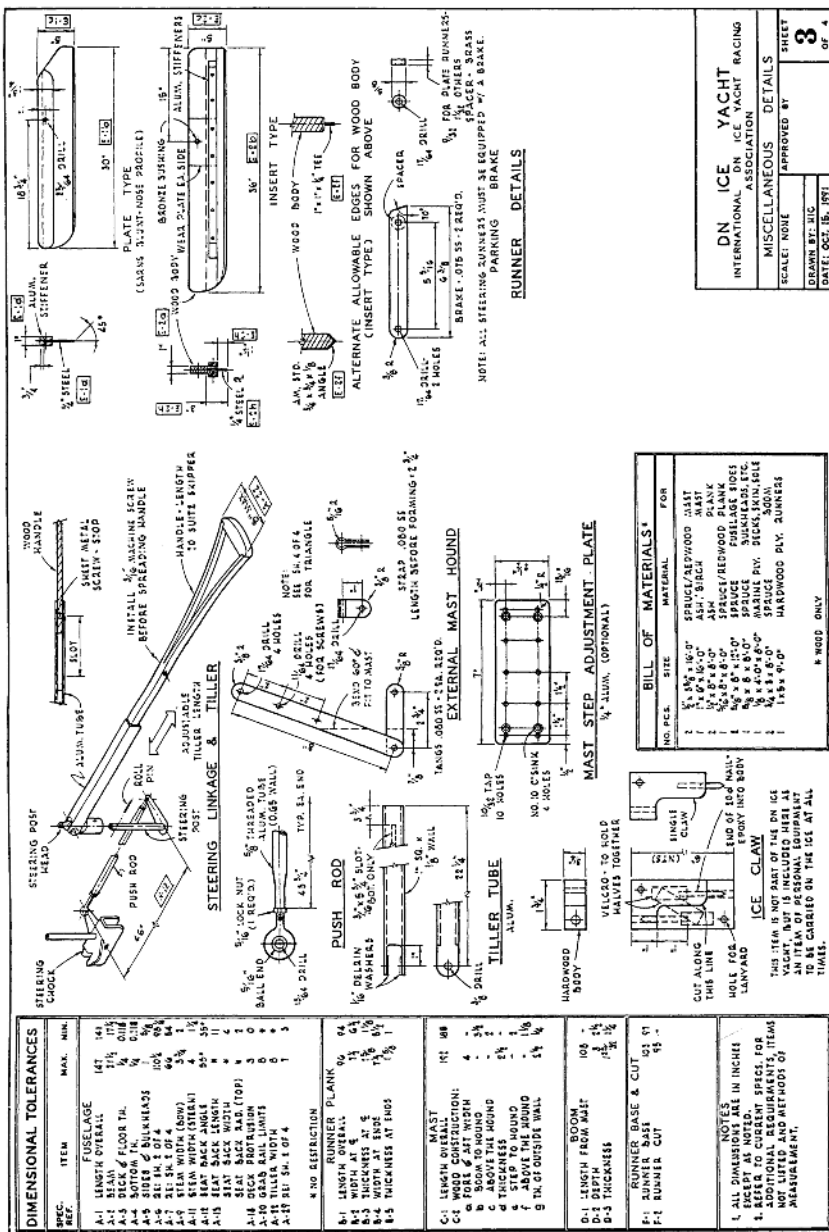


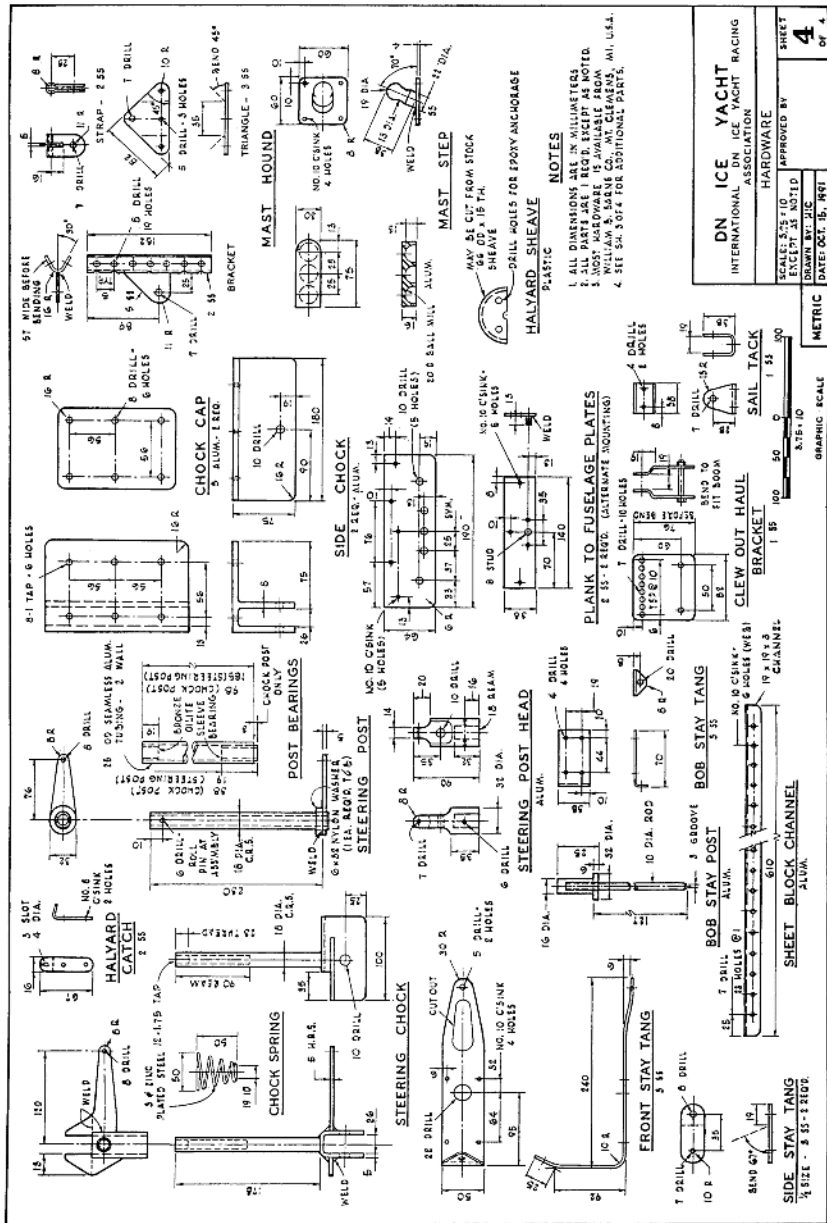




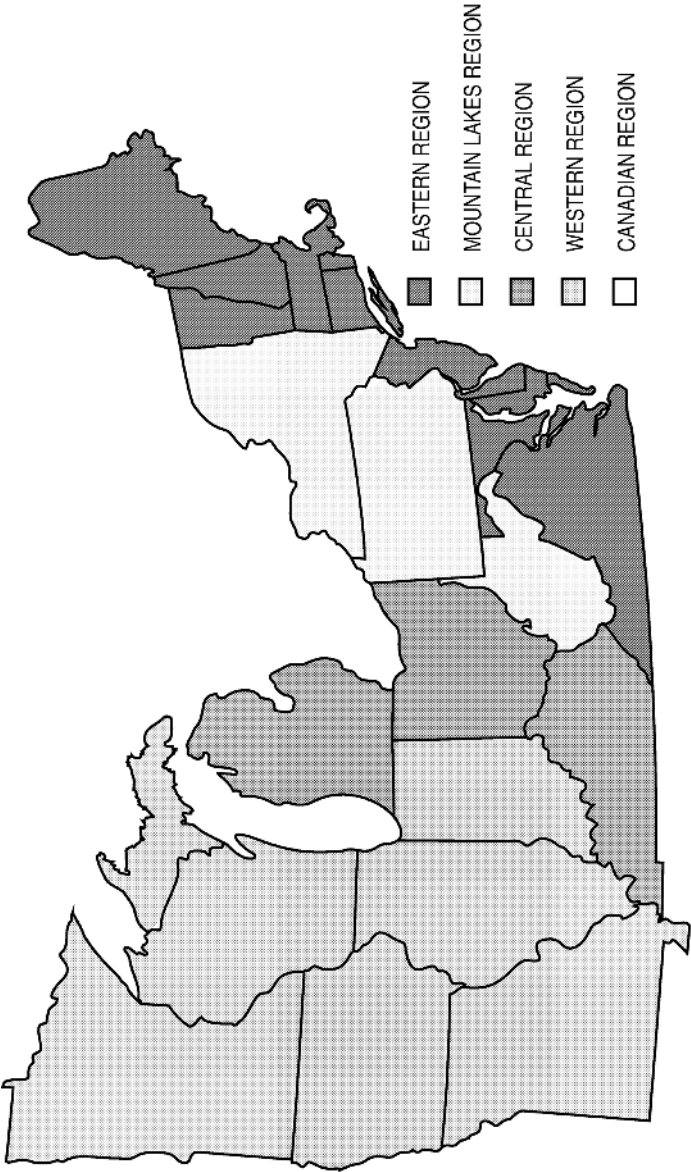








REGIONS OF THE NORTH AMERICAN IDNIYRA



IDNIYRA CONSTITUTION

ARTICLE I - NAME AND EMBLEM

The name of this organization shall be the International DN Ice Yacht Racing Association. Its emblem shall be the letters DN, placed on the sail in ten-inch letters, in red or other contrasting color.

ARTICLE II - PURPOSE

The Association's purpose shall be to promote ice yacht racing in this one design class, in boats built to the Official Specifications, and to sponsor class regattas.

ARTICLE III - ORGANIZATION AND FISCAL YEAR

The Association is one of individual members, acting through general membership meetings, and through the Governing Committee. The fiscal year shall be July 1 through June 30.

ARTICLE IV - DUES AND FEES

Association dues and fees to the North American Regatta shall be fixed by the By-Laws.

ARTICLE V - MEMBERSHIP

There shall be three classes of membership, as follows:

A. Active: Any owner or part owner of a DN, in good standing. Privileges are to sail in the Annual Regatta, vote on the Association affairs, receive all Association communications, and hold office.

B. Associate: Any non-DN owner interested in the affairs of the Association, in good standing. Privileges are to receive Association communications.

C. Inactive: Any Active or Associate member who has not paid dues in the current year by the time of the Annual Regatta will no longer be considered a member in good standing and will be transferred to inactive status. There are no privileges. Members must be Corinthians, and power to accept or reject applications for membership is vested in the Governing Committee.

IDNIYRA CONSTITUTION

ARTICLE VI - ELECTIONS AND DUTIES OF OFFICERS

A Commodore, Vice Commodore, Secretary, Treasurer, and Past Commodore shall be nominated at the annual meeting. They will be voted for by mail vote. Write-in votes will be accepted. The candidates receiving the greatest number of votes wins the election to that office. The term of office shall be one year: from July 1 to June 30 of the following year or until successors are elected. If an Officer's position becomes vacant before the term of office ends, the person to fill the vacancy shall be elected by the remaining Officers.

Duties are as follows:

Commodore: To be chief executive, preside at all meetings, be chairman of the Governing Committee, rule on procedure and jurisdiction, summarize decisions, appoint special committees, authorize payment of bills. The Commodore will counter-sign any checks for expenditures of more than \$1000.00. He will receive a photocopy of all IDNIYRA bank account statements (including IDNIYRA checks and Constitution deposit slips) involving IDNIYRA funds within three weeks of being mailed by the bank.

Vice Commodore: To officiate in the absence of the Commodore. To help the commodore in the approval of N.A. and Gold Cup sites.

To take care of perpetual trophies:

- 1) Make sure all perpetual trophies are returned, repaired, or replaced.
- 2) All perpetual trophies are to be properly engraved with winners names.
- 3) Make sure Perpetual trophies are delivered to the proper race officials two weeks prior to the regatta.
- 4) The keeping of names and addresses of all winners of the perpetual trophies and to pass such information to the new Vice Commodore.

Secretary: To keep minutes and other Association records, publish several newsletters each year and publish an annual directory. He shall publish any results of a membership vote with the full text of the ballot and vote count. He shall bring the records up to date, complete business pending from the Annual Meeting, and shall transmit all Association records to his successor as soon as possible after June 30. The Association shall purchase a \$10,000 surety bond for the Secretary.

Treasurer: He shall maintain the Association membership list, collect dues, assign sail numbers, maintain association funds in a checking account, disburse funds on order of the Commodore, take care of all government or legal documents required of the Association, generate reports and notices associated with financial matters, respond to inquiries about the Association by interested non-members, oversee the sale of mail ordered goods sold by the Association. Complete an annual financial report as of June 30th. Deliver all Association records to his successor as soon as possible after June 30th. The Association shall purchase a \$10,000 surety bond covering the Treasurer of the IDNIYRA. An annual audit shall be made of the Association books by the IDNIYRA Corporation or their designated auditor. Both the positions of Secretary and Treasurer can be held by one person, if necessary.

IDNIYRA CONSTITUTION

Rear Commodores for each of the 5 regions of North America shall be elected by the region they represent. If a vacancy exists as of May 1, the Commodore may appoint a replacement until the next regional meeting. Past Commodore: To serve on the Governing Committee, participate in its deliberations, and vote on decisions. Only members in good standing who have served as an IDNIYRA commodore are eligible. This officer serves as an at-large member.

ARTICLE VII - GOVERNING COMMITTEE

The Governing Committee consists of its Officers, acting as a group. Its powers are to render final decisions on appeal, sanction or prohibit races in the North American and Gold Cup Regattas (when held in North America), accept or reject membership applications, approve the Race Committee for the North American and Gold Cup Regattas (when held in North America) and perform other duties mentioned in the By-Laws. The Governing Committee may initiate changes to the Constitution, By-Laws, Official Specifications, or Racing Rules by proposing such changes to the entire membership at the Annual Meeting, or by mail. Membership vote is required for enactment of all proposed changes. Otherwise, the Governing Committee may not change or modify any of the above documents, although it has the power to interpret them.

ARTICLE VIII - MEETING AND QUORUM

The Annual Meeting shall be held during and in the vicinity of the North American Championships, or the DN World Championship when held in North America if possible. Special meetings may be called on the order of the Governing Committee or upon demand in writing by twenty-five percent of the membership. Exact time and place of all meetings shall be fixed by the Governing Committee. If a special meeting is called, the purpose thereof must be stated in such form as to permit voting by mail, and no other business may be transacted. All proposals to be made at the IDNIYRA Annual Meeting shall be submitted to the Governing Committee in writing ninety (90) days prior to the meeting. All proposals so submitted must be published in the DN newsletter by December 15. A quorum at a meeting is any number present. However, if less than twenty percent of the then-paid membership is present, all action must be ratified (unless rejected) by a mail vote. In addition, any action requiring a majority of two-thirds (such as amendments to the Constitution or By-Laws, or changes in the Official Specifications or Racing Rules) shall be put to a mail vote, regardless of the number present at the Annual Meeting. All meetings shall be conducted according to Roberts Rules of Order.

ARTICLE IX - VOTING

A majority of the votes cast shall be determined on all questions not otherwise stipulated, and the chair (if in a meeting) shall cast the deciding vote in case of a tie. The chair also has the power to fix a time limit on speakers and discussions of motions.

IDNIYRA CONSTITUTION

ARTICLE X - OFFICIAL SPECIFICATIONS

Requirements for the yacht, sail, and attached equipment shall be set forth in the OFFICIAL SPECIFICATIONS OF THE DN ICEYACHT. In all cases where there is a conflict between the Official Specifications and the Official Plans, the Official Specifications shall prevail. Specifications, however complete, cannot anticipate every possible situation that may arise. If a point is not covered, a ruling must be obtained from the Governing Committee through the Technical Committee. The intended meaning and the basic principles of maintaining the DN as a one-design class shall be considered in interpreting any point not covered. If a point is not clearly covered by the Plans and/or Specifications, it must be assumed to be illegal. The OFFICIAL SPECIFICATIONS shall be enforced in two ways. First, any contestant or judge or Race Committee member at the North American and Gold Cup Regattas may file a protest against any competing yacht. Second, the Race Committee shall measure the first five place - winning yachts at the conclusion of each regatta.

ARTICLE XI - AMENDMENTS AND CHANGES

Amendments to the Constitution or By-Laws, and changes to the Official Specifications* or the Racing Rules, may be made only after approval of the membership by a mail vote. A two-thirds majority of the votes cast is required. Changes in the Official Specifications shall become effective only if approved by July 1.* Otherwise they shall become effective the following season.

*For changes to the Specifications and their effective date, please refer to the Specification Management System.

IDNIYRA CONSTITUTION

ARTICLE XII - TECHNICAL COMMITTEE

The Technical Committee shall consist of six members, three elected from North America and three elected from Europe. One new member shall be elected each year at the Annual Meeting for a term of six years. Upon resignation prior to six years, a replacement member shall be elected at the Annual Meeting to serve the unexpired term. Term of office will begin July 1, and expire June 30 of the appropriate year. Each year the Committee shall elect from its membership a Chairman to serve until June 30 of the following year. The Chairman shall report to the Governing Committee on all recommendations of the Technical Committee. The Committee may initiate changes in the Official Specifications by proposing such changes to the Governing Committee, who may submit them to the membership on a mail ballot requiring two-thirds majority for approval. The Committee shall, upon the request of any member or at the direction of the Governing Committee, provide interpretation of the Official Specifications. These findings shall be published in the Association Newsletter. Such Interpretations shall prevail as Supplements to the Official Specifications unless and until voided by a simple majority vote by mail ballot. If a Committee seat becomes vacant for any reason, the Commodore may appoint a replacement until the next Annual Meeting.

ARTICLE XIII - CLASS HISTORIAN

The Class Historian keeps the class historical records, including, but not limited to past Newsletters, important historic documents and other articles of importance to the class. The position is appointed by the Governing Committee.

IDNIYRA BY-LAWS

GENERAL

1) Membership dues are Twenty Five (\$25.00) dollars for individuals annually, payable before November 1 to the Treasurer.

2) The entry fee for all IDNIYRA regattas shall be set by and paid to the Regatta Chairman (Rear Commodore of the host region), who shall submit a budget for approval to the Governing Committee no later than October 15 preceding the regatta, and a financial report following the regatta. Funds in excess of actual costs shall be deposited with the Treasurer. A Receipt and Disbursement Financial Report shall be presented to the membership through the Class Newsletter.

3) The national affiliation of each DN member is identified by National letter designators:

Australia	KA	Italy	I
Austria	OE	Latvia	O
Belarus	B	Lithuania	T
Canada	KC	Netherlands	H
Czech Republic	CZ	Norway	N
Denmark	D	Poland	P
Estonia	C	Russia	R
Finland	L	Sweden	S
France	F	Switzerland	Z
Germany	G	United Kingdom	K
Hungary	M	United States	US

Members of countries not appearing on this list may request the appropriate Governing Committee to assign them letter designators.

4) A North American member of the IDNIYRA shall be assigned a number by the Treasurer upon written request and payment of a ten dollar (\$10.00) fee. A member may request multiple numbers. Numbers may be transferred only with the written permission of the person the number is assigned to. A ten dollar (\$10.00) fee is required for transfer of a number. A European IDNIYRA member shall be assigned a number by the National Secretary of the appropriate country. The number on the sail shall conform to Specification G. 13.

5) An annual membership directory shall be published by December 15 and will include all members in good standing.

IDNIYRA BY-LAWS

IDNIYRA REGATTAS

A) World DN Championship (Gold Cup)

- 1) The Gold Cup is held annually; the site alternating between continents.
- 2) It is the responsibility of the relevant continental class officers to select the host region and with the assistance of that regional authority, to manage the regatta in accordance with these By-Laws.
- 3) The regatta is held during a specific week, beginning on a Sunday and ending on or before the following Saturday. The specific week shall be selected by the relevant continental class officers and the host regional/national authority.
- 4) If no races have been completed, day 1 may be postponed. If the regatta is moved, the continental officers may restart the regatta. If all fleets have completed four races by sunset of the third day, the regatta is over. If the minimum number of races has not been completed by sunset of the fourth day, racing will continue on the remaining days until the minimum number of races is completed. The regatta is cancelled if the minimum number of races has not been completed by sunset on Saturday, or in view of the weather and/or ice conditions the regatta is abandoned by the Race Committee.
- 5) On the final day, every effort should be made to complete all the originally scheduled races.

B) NORTH AMERICAN CHAMPIONSHIP REGATTA

1) Management

The regatta shall be under the control of the Governing Committee and managed by the Rear Commodore of the host region in accordance with these By-Laws.

2) Dates

- a) When held in conjunction with the Gold Cup, the regatta is held on the day or days remaining after the completion of the Gold Cup. If the minimum number of races has not been completed by sunset of the third day, racing will continue on the remaining days, until the minimum number of races has been completed. If the minimum number of races has not been completed by sunset Saturday, or in view of the weather and/or ice conditions the regatta is abandoned by the Race Committee, the regatta is canceled.

On the final day every effort should be made to complete all originally scheduled races.

IDNIYRA BY-LAWS

b) When not held in conjunction with the Gold Cup, the regatta is held during a specific week, beginning on a Monday and ending on Friday, with Saturday as a reserve day. If 3 races in all fleets have not been completed by sundown on Friday, racing shall be continued until sundown on Saturday. The specific week shall be selected by the Governing Committee and the host Rear Commodore. If the regatta is moved, the Governing Committee may restart the regatta. The regatta is cancelled if the minimum number of races has not been completed by sunset on Saturday, or in view of the weather and/or ice conditions the regatta is abandoned by the Race Committee. On the final day, every effort should be made to complete all the originally scheduled races.

3) Site

The regatta rotates each year among three regions - Eastern (which includes the Eastern Seaboard, Mountain Lakes, and Canada), Central Lakes, and Western Lakes (see map). In the Eastern Region, the three area Rear Commodores will decide which region will host the regatta. The site is selected by the host Rear Commodore and approved by the Governing Committee. Should the host region not have suitable ice, the regatta will be moved to the nearest site where suitable ice is available. If the regatta moves to a different region, the Governing Committee will coordinate the move with the original Rear Commodore and the Rear Commodore of the new region. Ice reports will be available during the week prior to the regatta so those sailors who are on their way to the regatta may be advised with regards to the final site location.

4) Entries

a) The regatta is open to any member of the Association who has paid all current dues and who has mailed an entry form with entry fee prior to January 1. Late registration at two times the normal fee will be accepted up to one day before the regatta.

b) Novice entries: Novice DN racers may enter one or both regattas if two years association dues are paid. No other event registration fee will apply. A DN racer shall be considered a novice if that person has never belonged to IDNIYRA, or if their membership has lapsed five or more years. A novice must comply with all other entry requirements.

5) DN Race System

There shall be seven races scheduled for each fleet and sailed alternately. Cancellation of races in one fleet shall not affect the races in another fleet. The regatta, as a whole, will be considered a complete event if the Gold Fleet finishes a minimum of three races, regardless of the number of races completed by the other fleets. If the regatta is completed, prizes will be awarded to all fleets that have completed one or more races. There shall be seven races scheduled for each fleet

IDNIYRA BY-LAWS

and sailed alternately for the Gold Cup and for the North Americans when held in conjunction with the Gold Cup. When not held in conjunction with the Gold Cup, there shall be fourteen races scheduled. Cancellation of races in one fleet shall not affect the races in another fleet.

a) The races are scheduled in the following sequence:
Mini-qualification races; Gold; Silver; Bronze; Aluminum; Gold; Silver; Bronze; Aluminum, etc. until racing is completed. Each fleet starts no sooner than five minutes after the last yacht of the previous fleet finishes. (See section 7, Mini Qualification Races)

Delays caused by weather or ice conditions may necessitate changes in the above schedule. The host region authority in consultation with the continental class officers present at the regatta has the authority to make changes deemed necessary for safety or to complete the regatta in time. This may include postponing to a later day, canceling non-gold fleets to give time for Gold fleet races and other appropriate measures.

b) Scoring will be done by assigning points in the following manner:
first place, 1 point; second place, 2 points; third place, 3 points; fourth place, 4 points; fifth place, 5 points; etc. -lowest score to win. DNS, DSQ, and DNF one worse than the number of yachts in the fleet after the mini-qualification races.

c) Ties will be broken by the following algorithm: If two or more skippers are tied for a position, the skipper who finished ahead of the other in the most races wins the tie. If the tie is not broken, the skipper who finished ahead of the other in the last race wins the tie. If necessary, the next to last race will be used to break the tie. If the tie is still not broken, the next previous race will be compared, and so on in reverse order. If the tie is not broken after all races are compared, it will be declared a tie. Note: All races will be considered in breaking ties (including any throwouts).

d) Throwouts: After 5 races are completed, the points for each yacht's poorest race (including DNS, DNF, and DSQ) will be eliminated from the scoring with the exception that, following a hearing, the protest committee may, at its discretion, award a score of DNE to a disqualified yacht that has been found to have dangerously violated NIA Rules Part IV, Sailing Rules, and this race shall not be eliminated from the scoring. The next-worse score shall be eliminated. After 12 races are completed, the points for each yacht's second poorest race (including DNS, DNF and DSQ) will be eliminated from the scoring, with the exception that, following a hearing, the protest committee may, at its discretion, award a score of DNE to a disqualified yacht that has been found to have dangerously violated NIA Rules Part IV, Sailing Rules, and this race shall not be eliminated from the scoring. Then next worse score shall be eliminated.

IDNIYRA BY-LAWS

6) DN Split Fleet Ranking

The IDNIYRA Secretary shall maintain a fleet ranking. This ranking shall be updated yearly and shall be effective October 15 using regatta results from the previous two WCs (World Championships), NACs (North American Championships), and ECs (European Championships). This ranking shall be the lessor of the following:

1. Best finish in the Gold fleet in the previous two WCs, NACs, and ECs.
2. 22 plus your best finish in the Silver fleet in the previous two WCs, NACs, and ECs.
3. 40 plus your best finish in the Bronze fleet in the previous two WCs, NACs, and ECs.
4. 58 plus your best finish in the Aluminum fleet in the previous two WCs, NACs, and ECs. In the event of equal ranking for more than one sailor, the ranking which is based on the most recent race result will be considered higher.

7) Mini Qualification Races

A race will be held for all non-ranked and Aluminum fleet sailors. This race will be three laps with the first 12 places qualifying for Bronze fleet. The remainder of the finishers will be scored as follows: 13th place will receive the 1st place points, 14th place will receive the 2nd place points, 15th place will receive the 3rd place points, and so on to the end of the finishers. The race will be counted as the first Aluminum fleet race.

The race committee must be notified of any protest involving the first 12 finishers within 10 minutes of the last boat finishing. The protested sailor will be notified of the protest. He will be given the choice of a protest hearing at the close of racing for the day, or acknowledgment of guilt and withdrawing. If the protest goes to a hearing at the end of the day and the protested sailor loses, he will be disqualified from the regatta.

A second race will be held for all Bronze fleet sailors including those who have just qualified for Bronze fleet. This race will start no sooner than 20 minutes after the 12th boat finishes the previous race. This race will be three laps with the first 12 places qualifying for the Silver fleet.

The remainder of the finishers will be scored as follows: 13th place will receive the 1st place points, 14th place will receive the 2nd place points, 15th place will receive the 3rd place points, and so on to the end of the finishers. The race will be counted as the first Bronze Fleet race. Protests involving the first 12 places will be dealt with as above.

IDNIYRA BY-LAWS

A third race will be held for all Silver fleet sailors including those who have just qualified for Silver fleet. This race will start no sooner than 20 minutes after the 12th boat finishes the previous race. This race will be three laps with the first 12 places qualifying for the Gold fleet. The remainder of the finishers will be scored as follows: 13th place will receive the 1st place points, 14th place will receive the 2nd place points, 15th place will receive the 3rd place points, and so on to the end of the finishers. This race will be counted as the first Silver fleet race. Protests involving the first 12 places will be dealt with as above.

The first race of the Gold fleet will start no sooner than 20 minutes after the 12th boat finishes the previous race. The Race Committee, with the approval of the Governing Committee members present at the race, may cancel the Mini-qualification Races if time or conditions warrant.

Fleet Splitting

All registered sailors will be divided into fleets as necessary. The number of sailors in each fleet will be determined by the race committee, with approval of a majority of those members of the Governing Committee present, after the close of registration. The recommended maximum fleet size is fifty sailors per fleet. Fleets of approximately equal size are desirable but not mandated. The safety of all participants is the primary consideration.

Fleet assignments will be based on the following criteria:

Gold Fleet:

1. Skippers who finished in the first 3 places in the Silver Fleet of the preceding continental or higher regatta.
2. Skippers who finished in the first 12 places of the Silver Fleet mini-qualification race.
3. The balance of this fleet will be comprised of the top ranked skippers who have registered prior to the deadline.

Silver Fleet:

1. Skippers who finished in the first three places in the Bronze Fleet of the preceding continental or higher regatta.
2. Skippers who finished in the first 12 places of the Bronze Fleet mini-qualification race.
3. The balance of this fleet will be comprised of the top ranked skippers who have registered prior to the deadline that are not in a higher fleet.

Bronze Fleet:

1. Skippers who finished in the first three places in the Aluminum Fleet in the preceding continental or higher regatta.

IDNIYRA BY-LAWS

2. Skippers who finished in the first 12 places of the Aluminum Fleet mini-qualification race.

3. The balance of this fleet will be comprised of the top ranked skippers who have registered prior to the deadline that are not in a higher fleet.

Aluminum Fleet:

1. All remaining skippers.

8) Racing Procedure

1. Every skipper must check-in at the registration desk, no later than 15 hours before the first scheduled race. Skippers checking in after that deadline will be assigned arbitrarily to the Silver or Bronze Fleets by the regatta officials, with no regard for the past racing record of the skipper and positioned at the end of the line in the first race. Check-in by phone prior to the deadline may be accepted by the officials, if they so choose.

2. Following registration, after fleet assignments are determined, drawing for starting positions for the first race will be done by the race committee. In subsequent races, yachts are positioned on the starting line according to their finishing position in the previous race.

9) All skippers are required to show Proof of Liability Insurance to the extent of \$300,000.00. The Regatta Committee may require that Proof of Insurance at the time of registration, if they so choose.

10) Trophies and Prizes

The Perpetual Trophies are described in the Yearbook. They must be returned to the Association at or before the next regatta. Keeper Trophies will be awarded to the first ten skippers in the final standings in a Single Fleet regatta, and in a Split Fleet regatta to the first ten in each of the Fleets. A Keeper Trophy will also be awarded to the highest scoring senior (over 50) in each fleet, and highest scoring junior (under 18) in each fleet.

11) No commercial information will appear on any DN parts other than standard sailmaker's or builder's marks. Those marks must comply with the following criterion: One sailmaker's mark may appear on each side of the sail. The entire mark must be within 16" of the tack. One or two builder's marks may be displayed on a part made by that builder. All builder's marks must be smaller than 6" by 6". This requirement will apply from 7:00 AM of the first day of any official activity for a regatta until after the awards ceremony for that regatta. Sailmaker's or builder's marks and commercial decals and stickers that are in place before May 15, 1989 are

IDNIYRA BY-LAWS

not covered by this requirement.

12) The IDNIYRA is an amateur racing association. It is managed by volunteers and is financially self-sufficient. To preserve this character, the IDNIYRA and its members will not participate in commercial promotion at IDNIYRA events. The IDNIYRA recognizes the importance of suppliers of DN sailing products. The relationship between the IDNIYRA and DN suppliers may include: support of raffles, advertising of DN related products and supplier listings in IDNIYRA publications, and other similar activities in keeping with the character of the IDNIYRA. The Board may allow sponsorship at IDNIYRA events, so long as said sponsorship shall benefit the organization as a whole, and not any individual competitor or group of sailors.

C) THE EUROPEAN CHAMPIONSHIP WILL BE THE SOLE RESPONSIBILITY OF THE EUROPEAN COMMODORE

D) REGIONAL REGATTAS WILL BE THE SOLE RESPONSIBILITY OF THE REAR COMMODORE FROM THAT REGION AND ARE EXEMPT FROM FINANCIAL ACCOUNTABILITY TO THE IDNIYRA

RACING RULES

The Racing Rules of the National iceboat Authority (available at <https://ice.idniyra.org/yearbook>, select National Iceboat Authority Resources) shall prevail, supplemented by the following Association rules:

A. Failure to use the parking brake at any time that the yacht is left unattended with sail up, may result in disqualification for the entire regatta if damage or injury results and the Race Committee deems it appropriate.

B. While racing, each sailor must wear a helmet that they determine adequately protects them from the hazards of the sport. As a minimum, the helmet will be hard-surfaced, energy-absorbing crushable foam-lined, 3/4 head and cover the ears. This minimum helmet standard has not been shown to provide adequate protection from the hazards of the sport in all circumstances. Interpretations of this rule may be made by the Governing Committee.

C. All races shall be a minimum of four (4) miles in length. (6 1/2 km)

D. Lap time limit shall be 6 1/2 minutes per mile (1 mile between marks = 13 minute lap time limit). (see N.I.A. Rules, Part II, par. G.)

E. No race may be started after 1/2 hour before official sunset. This time may be increased at the discretion of the Regatta Chairman and his advisors.

F. The same equipment (except runners and sails) must be used throughout the entire regatta, except articles found illegal in one race or heat.

IDNIYRA BY-LAWS

G. Official sunset time shall be posted at the skippers registration area at the time of registration.

H. No yacht shall be expected to start any race (championship, gold, silver or color section) earlier than fifteen minutes after the last boat finishes the race (or section) in which the yacht competed.

I. No electronic equipment is allowed, except timing device.

J. The first yacht finishes her race after completing the scheduled number of laps. Any following yacht finishes the race when she crosses the Finish Line after the finish of the first yacht. In each race, a yacht will be scored ahead of all yachts with fewer laps. Any yacht not finishing within 20 minutes of the first yacht will be scored according to her finish in the previous lap (if no laps have been completed she will be scored DNF).

K) The IDNIYRA governing committee may further modify the racing rules. Any modifications must be published in the Sailing Instructions and course diagram if applicable.

SPECIFICATION MANAGEMENT SYSTEM

MANAGEMENT OF THE IDNIYRA OFFICIAL SPECIFICATIONS AND PLANS

Article I - OFFICIAL SPECIFICATIONS

Requirements for the yacht, sail, and attached equipment shall be set forth in the OFFICIAL SPECIFICATIONS OF THE DN ICE YACHT. The DN is an inexpensive, home buildable, one-design ice yacht. It is intended that changes in the Official Specifications be limited to the following purposes: To make the yacht safer, to minimize differences in sailing performance associated with the design and construction of the yacht, to make it easier or less expensive to build, to make the yacht more durable, and/or to clarify existing specifications.

Article II - OFFICIAL PLANS

The Official Plans are the one example of how a DN Ice Yacht can be built. Their purpose is to demonstrate the function of the parts and to provide perspective for the specifications. In all cases where there is a conflict between the Official Specifications and the Official Plans, the Official Specifications shall prevail. Changes in the Official Plans will be managed in the same way as the Official Specifications.

Article III - ORGANIZATIONAL STRUCTURE

The IDNIYRA is made up of two continental organizations: the IDNIYRA North America and IDNIYRA Europe. Changes in the official IDNIYRA Specifications and Plans will be made as outlined in this document. Other affairs will be managed at the continental level.

Article IV - MEMBERSHIP

A member who has paid his dues for the current year to either continental organization will be a member of the IDNIYRA. Membership in the IDNIYRA will entitle all members to vote on all proposed changes in the Official Specifications or Official Plans, to sail in the IDNIYRA Gold Cup and other Continental Championship Regattas and to receive a copy of the IDNIYRA Annual Directory. Other benefits and obligations of membership are afforded by each continental organization.

Article V - TECHNICAL COMMITTEE

The IDNIYRA Technical Committee shall consist of six members, three elected from North America and three elected from Europe. A new member will be elected every year. In North America a new member will be nominated at the annual meeting on even numbered years. The term of office is six years. Upon resignation prior to six years, a replacement member will be elected at the continental level. Term of office will begin July 1, and expire June 30 of the appropriate year. Each year the Committee shall elect from its membership a chairperson to serve until June 30 of the following year. The Chairperson shall report to the Governing Group Commodore on all recommendations of the Technical Committee. The Committee may initiate changes in the Official Specifications by proposing a change to one of the Governing Groups, who may submit a proposal to the membership as described in this document.

SPECIFICATION MANAGEMENT SYSTEM

Article VI - WORLD COMMODORE

There will be an honorary office of World Commodore. The World Commodore will be the Commodore of the continent that is hosting the Gold Cup that year or his designated representative. The World Commodore will be the official representative of the IDNIYRA at the Gold Cup and associated regattas.

Article VII - PROPOSAL SUBMISSION

Proposals to change the Official Specifications or Plans can be made by either continental Governing Group. All proposals submissions should be written in specification language as it would appear in the specifications. In North America, proposals may be made in the manner described in Article VIII of the IDNIYRA (North America) Constitution.

Article VIII PROPOSAL APPROVAL WORDING

Wording of proposals will be reviewed by the Technical Committee. They will suggest revised wording, if appropriate, to make the proposal more clear or reasonable to interpret. They will work directly with the author of the proposal on suggested revisions. All proposals will be submitted to the Technical Committee and the Governing Groups by May 31. Final wording on proposals will be submitted to the North American Secretary by the Technical Committee by September 1. Voting: A ballot will be sent by air mail to all IDNIYRA members for all Specification or Plan proposals during September. This ballot will be sent by the Secretary of the IDNIYRA North America. The completed ballots will be sent to and counted by the Secretary of the IDNIYRA North America. All ballots must be received by the 15th of December. IDNIYRA members that are members of both the North American and European organizations will receive only one ballot. Passage: To pass, there must be at least twice as many 'yes' votes as 'no' votes received by the end of the voting period. Any proposal which does not pass may not be resubmitted for one year. Effective Date: Changes in the Official Specifications or Official Plans shall become effective March 31 unless the Governing Group that submits a specific proposal stipulates that it have a January 1 effective date.

Article IX - INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS OR PLANS

Specifications, however complete, cannot anticipate every possible situation that may arise. If a point is not covered, a ruling must be obtained from the Continental Governing Group through the Technical Committee. The intended meaning and the basic principles of maintaining the DN as a one-design class shall be considered in interpreting any point not covered. If a point is not clearly covered by the Plans and/or Specifications, it must be assumed to be illegal. The Technical Committee shall, upon the request of any member, or at the direction of either Governing Group, provide interpretation of the Official Specifications. These findings shall be published in the Association Newsletter. Such interpretations shall prevail as Supplements to the Official Specifications unless and until voided by a simple majority vote by mail ballot. Such a vote will be initiated in the same way a Specification Change Proposal is made.

SPECIFICATION MANAGEMENT SYSTEM

Article X - ENFORCEMENT OF THE SPECIFICATIONS:

The OFFICIAL SPECIFICATIONS shall be enforced in two ways. First, any contestant or judge or Race Committee member at the Gold Cup, European Championship or North American Championship Regattas, may file a protest against any competing yacht. Second, the Race Committee shall measure yachts during a regatta as described in the Continental Governing Documents or the regatta Sailing Instructions.

Article XI - ADMINISTRATION OF THIS DOCUMENT

This document will be administered by the North American Governing Committee. Administration will consist of publishing and mailing the IDNIYRA Specification Proposal Ballots, counting the completed ballots, communicating the results, and associated duties. Each year, a directory of all paid up members of the IDNIYRA will be published in the fall by the North American Secretary. IDNIYRA Europe will provide a computerized membership list in a format that can be used with the North American Computer to generate a membership list for the annual directory and mailing labels to mail a ballot. This list will include only members of IDNIYRA Europe who have paid their dues during the current season. It will be provided to the North American Secretary by August 1. The costs of administering this document will be shared by the two continental organizations. Costs attributable to either organization will be paid by that organization. Shared costs will be allocated in proportion to the number of members in each organization. An account will be established and maintained with enough money to administer this agreement for one year. An annual financial report will be published in the North American Newsletter in the Fall. English will be the language for all official communications between North America and Europe. Copies of official newsletters and yearbooks will be sent to all National Secretaries, Continental Officers and Technical Committee members.

Article XII - CHANGES IN THIS DOCUMENT

Any changes in this document will be made by the method described for Specification Changes with the exception that the Technical Committee does not need to review wording of proposals to change this document.

Article XIV - JURISDICTION

On matters relating to the Official Specifications and Official Plans, this document will override the governing documents of the Continental Organizations if there is a conflict.

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ORGANISATION OF THE IDNIYRA EUROPE

I. General

a. The European DN Class Organisation is part of the International DN Ice Yacht Racing Association (IDNIYRA). Its purposes are the same as those of the IDNIYRA.

b. The European organisation IDNIYRA Europe Society e.V. (constitution on the web site) is made up of individual members or National Organisations represented by their National secretaries acting through its first and second chairmen, the Executive Board, the general meeting in the form of the European Governing Committee (National secretaries Meeting)

2. Dues, Membership and Fiscal Year

a. National dues are set by the respective National organisations

b. The IDNIYRA-Europe dues are set by the National Secretaries' Meeting and based upon a subscription per country or per start place in the European Championships. The starters in the EC should pay those dues to IDNIYRA-Europe each year before the EC entry closing date.

c. A DN sailor who has paid his/her dues for the current year to either IDNIYRA-NA or the appropriate IDNIYRA-Europe National organisation shall be a member of IDNIYRA. Such memberships will entitle all members to vote on proposed changes to official specifications, plans, Constitution and By Laws; to sail in the IDNIYRA Gold Cup and other major regattas (NA and EC); hold office; and to receive information published by IDNIYRA.

3. Duties of IDNIYRA-Europe Officers

a. There will be an honorary office of World Commodore. The World Commodore will be the Commodore of the continent hosting the Gold Cup that year or his designated representative.

b. European Commodore:

i. To preside at Board and National Secretaries Annual Meetings.

ii. To represent IDNIYRA-Europe interests.

iii. To call Board meetings. When a meeting is requested, in writing, by at least two Board Members a Board meeting will be organised within four weeks.

iv. Ultimate authority over the choice of the regatta site for European

ORGANISATION OF THE IDNIYRA EUROPE

Championships, European Cup, and World Championships when held in Europe.

c. European Vice Commodore:

i. To assist the European commodore in his/her duties and represent him/her during his/her absence.

ii. To oversee the care of the perpetual trophies. These must be engraved each season and available for presentation at the appropriate prize giving (WC, EC & E Cup) and a record kept of winners' names and to arrange for replacement/repair of trophies if damaged or lost.

d. European Treasurer: (May be combined with Commodore or Vice Commodore)

i. Collection of membership dues.

ii. Financial administration of IDNIYRA-Europe.

iii. To provide a current Financial Report to the National Secretaries Meeting and a Budget for the next year in advance of the meeting.

iv. Payment of reasonable expenses incurred on IDNIYRA-Europe business by Executive Board and Technical Committee members.

v. Obtain approval of the European Commodore for other expenditures.

e. European Secretary:

i. Keeping and directing all IDNIYRA-Europe correspondence.

ii. Proposing the Agenda for the National Secretaries meeting

iii. Taking the Minutes of the National Secretaries Meetings and, after approval by the Commodore, conveying them to all participants and indicated parties.

iv. To administer lists of members provided by the National Secretaries.

v. To liaise regularly with the IDNIYRA-NA Secretary.

f. Junior Programme Manager:

i. To act as the link between Junior DN and Ice Optimist sailors and the Executive Board. See 13 for relationship IDNIYRA Europe and Junior Programme.

ORGANISATION OF THE IDNIYRA EUROPE

- ii. To attend National Secretaries and Board meetings.
 - iii. Organise Junior Programme planning meetings.
 - iv. Attend and supervise Junior DN and Ice Optimist major Regattas (WC and EC)
 - v. Encourage participation in IDNIYRA-Europe regattas by junior DN sailors.
- g. Insurance Manager:
- i. Keeping contact with the Insurance company which provides the TPL- Insurance for IDNIYRA- Europe.
 - ii. Checking Confirmation of Cover (CoC) and assigning an “Insurance ID” for the online-registration to EC and WC – when held in Europe.
 - iii. Preparing the entry lists after registration is closed and providing them for the following purposes:
 - iv. Printing stickers for each sailor
 - v. Checking the payment for each sailor
 - vi. Setting the fleets after registration in the Race office at the site.
- h. National Secretary:
- i. Represent their National organisation in IDNIYRA-Europe.
 - ii. Attend National Secretaries Meeting. If unable to attend, a representative with written authorisation may attend as National Secretary or a proxy vote may be given to another attending National Secretary.
 - iii. Assignment of National sail numbers.
 - iv. To provide a National membership list annually by 1st September to the IDNIYRA-Europe Secretary and the IDNIYRA-NA secretary
 - v. Remit National annual dues to IDNIYRA-Europe Treasurer before the EC entry closing date.
 - vi. Select and enter National participants in World Championship, European Championship and Europa Cup regattas.

ORGANISATION OF THE IDNIYRA EUROPE

i. Webmaster

- i. A responsible webmaster will be appointed by the National Secretaries Meeting for every two years.
- ii. The portal website www.idniyra.eu is the main information source for IDNIYRA-Europe.
- iii. The website shall provide official IDNIYRA information and links to member Nations and be administered by the webmaster.
- iv. Special services for ice reports, regatta information, membership administration, voting procedures, online publishing etc. shall be developed.
- v. A yearly budget will be allocated and revised by the National Secretaries Meeting.

j. Technical Committee:

- i. Full details are contained in the IDNIYRA Constitution. The three European members of this Committee will be elected at the European National Secretaries' meeting. The term of office is six years and for purposes of continuity, one new member shall be elected every two years.
- ii. Respond within a reasonable time to all technical questions addressed to them by National Secretaries or individual members.
- iii. Keep measurement rules up to date and distribute them to all members via the webmaster and the web.
- iv. Maintain close contact with the NA members of the Technical Committee.
- v. Act in all respects in accordance with Art XII of the IDNIYRA Constitution
- vi. Provide a summary of the past year's Technical Committee activities via a report at the National Secretaries meeting or an email report in advance of the Meeting.

4. Governing Committee

The European Governing Committee consists of its officers acting as a group on 2 levels.

- a. The Executive Board consisting of the European Commodore, Vice Commodore, Treasurer, Secretary, Junior Programme Manager, Insurance Manager and Web Master. All members are elected by majority vote at the National Secretaries Meeting and serve for two years. In order to ensure continuity on the Board, no more than three members should change in any one year. National Secretaries are appointed by nations.

ORGANISATION OF THE IDNIYRA EUROPE

b. The National Secretaries Meeting consists of the Executive Board, National Secretaries of all European Member countries and one member of the European Technical Committee.

Their individual duties are specified in para. 4. Their corporate responsibilities are:

Executive Board: The day to day working of IDNIYRA-Europe between the Annual National Secretaries Meeting and in consultation where appropriate with National Secretaries.

The National Secretaries Meeting: The running of IDNIYRA – Europe primarily through attendance at the Annual National Secretaries Meeting either in person or via proxy. No nation may carry more than two proxy votes at this meeting. Initiation of changes to Constitution, By Laws, Official Specifications and Racing Rules by proposing such changes to the IDNIYRA membership according to the procedure described in article XI of the IDNIYRA Constitution. The approval of new member countries. The setting of dues and insurance requirements of the IDNIYRA Constitution.

5. Annual Meeting

The annual European National Secretaries Meeting shall be held on completing each season if possible before 1st May and hosted by a European member nation in rotation. It will be chaired by the European Commodore and the minutes taken are published by the European Secretary. A majority of votes cast shall determine the decision on all questions and the chair shall cast the deciding vote in case of a tie. The chair also has the power to fix a time limit on speakers, discussions and agenda items.

The entire European Governing Committee (see para. 5) will be invited together with an IDNIYRA-NA representative. Member countries unable to attend may nominate proxy votes by agreement to specific agenda items. Proxy votes are restricted to 2 absent member nations per attending nation. A quorum shall be a minimum of five National Secretaries or their appointed representatives (not including proxies).

The Commodore and Secretary will issue the meeting agenda at least one month ahead of the meeting date. Issues for the agenda must be communicated to both the Commodore and Secretary at least 6 weeks before the meeting date. The meeting will normally be scheduled to run for 2 days.

6. Protest Procedure

a. The names of jury members and Chairman must be clearly published on the race board on the ice and at the race office before the regatta starts. Protest forms and NIA rulebook should be at hand on the ice. All this is a question of good preparation.

b. Jury members should be on the ice to have their own impressions of what is going on and what ice and weather conditions there are.

c. Jury members must be from a different nation and fleet than those involved.

d. RC shall score the Top Mark roundings by writing them down.

ORGANISATION OF THE IDNIYRA EUROPE

- e. RC shall write down every rule infringement at the Top Mark noting sail number and lap.
- f. Rule books and office supplies should be provided to the jury by the RC.
- g. A quiet room that can be closed off for privacy is a prerequisite for a smooth protest procedure.
- h. There should be no pressure on the protest committee for time.
- i. A written decision should be on file with the results.
- j. The written protocol (including Facts found, Drawing and Decision) of the Protest Hearing signed by the judges shall be stored by the PRO and/or a member of the IDNIYRA-Europe Board. This prevents the loss of papers. The chairman of the protest hearing is responsible for this.
- k. The course marks and signs shall clearly differ in colour and shape.
- l. If Sailing instructions are made by word of mouth before the race they should also be published at the score board on the ice.

7. European DN Iceboat Authority (EDNIA)

- a. The EDNIA is established to hear appeals against any Regatta Protest Committee decisions in Europe which are disputed. The five EDNIA members are elected, and reviewed every five years, by the European National Secretaries at the National Secretaries' Meeting. They must not be members of the PRC. Their names and contact details shall be posted on the IDNIYRA-Europe website and in the Year Book. They shall name a Chairman. Three out of the five should be chosen for the appeal hearing by the Chairman.
- b. The Appeals procedure must follow the same route as Appeals to the National Iceboat Authority (NIA Racing Rules Part V, D). All Appeals shall be made in writing and filed with the EDNIA within 30 days of the rendering of the decision appealed. It is particularly important to have written consent to the Appeal, signed by the Judges rendering the original decision.
- c. The appeal decision shall be made within 30 days of the date the Appeal was properly filed with the EDNIA and shall be sent to all parties to the infringement and appeal.
- d. The EDNIA shall also maintain regular contact with the Directors of the NIA.

ORGANISATION OF THE IDNIYRA EUROPE

8. Insurance

All participants in regattas held in Europe are required to have 3rd Party Liability Insurance. The Regatta committee and Insurance Manager will require proof of insurance from all skippers at the time of registration. Insurance claims should include the following documents:

1. The Protest Committee findings (in a Regatta)
2. A realistic damage assessment and cost by a Technical Committee member (in a Regatta)
3. In non Regatta incidents a competent equivalent of the above.

Maximum Claim Values:

Hull	2800€
Mast	1500€
Plank	500€
Runner	450€ each
Sail	700€
Chock	100€
Boom	150€

9. Anti Doping Policy

In the interests of FAIR SAILING IDNIYRA Europe has an Anti Doping Policy in force and is a signatory to the World Anti Doping Agency (WADA) Code.

(www.wada-ams.org) This web site contains full details of the "Prohibited List" together with the procedure required for those sailors who have a medical need to take drugs which are on the Prohibited List. – A Therapeutic Use Certificate (TUE). Misuse of alcohol will not be tolerated.

10. IDNIYRA-Europe Regattas

a. The European Professional Race Committee (PRC) is responsible for the running of major regattas (EC and WC when in Europe). It must work in close cooperation with the Commodore, the Organising country and the Host nation. The full scope of the PRC responsibilities are listed on the European website.

b. An Organising country will be agreed at the national Secretaries meeting each year. When the major regatta takes place in the planned country that country is also the Host nation. If the regatta moves to another country that new country becomes the Host nation and the original planned country retains the role of Organising country. Responsibilities of Organising country and Host nation are listed on the European website.

c. These regattas are conducted in accordance with the By-Laws and the NIA rules as supplemented by the IDNIYRA Racing Rules with the following additions:

- i. In the interests of safety the two racing marks will be extended by the use

ORGANISATION OF THE IDNIYRA EUROPE

of the “Darling” mark which will be secured to the ice at least 100 meters directly up wind from the Leeward Mark and at least 100 meters directly down wind from the Windward Mark. During any race no yacht may pass between the Darling mark and the main mark at any time unless forced to do so by another yacht. A DSQ will be the penalty for the offending yacht.

ii. The maximum number of participants is set at 200 divided into four fleets of 50.

iii. Each IDNIYRA Europe Nation may have 4 participants by right regardless of their International Ranking. Any additional participants will normally need to have a ranking place. This arrangement ensures representation from all Nations is possible.

iv. In the event of a move of the regatta to another country the DN members from that new country who have not already registered may do so at normal fee (not double) if there are still places available before the regatta check in closes

11. Sailing Instructions

These will be produced by the PRC in coordination with the Commodore and the Host nation. Sample Race Instructions can be found on the IDNIYRA Europe web site under “Download”. This also contains many other helpful samples for Regatta organizers.

12. Trophies

The Vice Commodore will maintain a record of the current Trophy holders and this will be placed on the DN Forum – Bulletin Board. Perpetual trophies are to be properly engraved with the winners’ names and delivered to the proper race official two weeks prior to the regatta. They must be repaired or replaced if damaged or lost.

13. Junior Programme

The European Junior Programme operates under the guidance of IDNIYRA-Europe from its Junior Programme Manager. The two Classes are the Junior DN and the Ice Optimist.

The Age limits for these two Classes are:

DN Juniors: in 2016 birth year 1993 and younger

ICE-OPTIMIST: in 2016 birth year 2000 and younger

All trainers and junior leaders are welcome to discuss younger sailors participating in both classes with the Race Committee.

IDNIYRA-Europe recognizes that the future of the DN class relies to some extent on this programme. The Junior DN is a standard DN Ice Yacht. The Ice Optimist is an ice sailing version of the Optimist dinghy using an adapted version of the Optimist sail rig. An important IODA condition for the use of this is that all sails in major regattas (WC and EC) must carry the Optimist Logo and the button (disc) in the tack corner of the sail. See www.idniyra.eu (Juniors) for Ice Optimist building plans.



2017 Gold Cup. Photo: Gretchen Dorian

STANDARD SAILING INSTRUCTIONS

INTERNATIONAL DN ICEYACHT RACING ASSOCIATION STANDARD SAILING INSTRUCTIONS

I. RULES

All races will be sailed in compliance with the Specifications, Constitution and By-Laws of the International DN Ice Yacht Racing Association and the rules of the National Iceboat Authority as modified herein.

II. COURSE LOCATION

The racing area will be indicated on a map posted on the official bulletin board.

III. MARKS

Marks will be displayed at the Skippers meeting.

IV. STARTING TIMES

Skippers Meeting:

First race day one:

First race day two:

First race day three:

Starting times of subsequent races each day will be announced by the Race Committee Chairman, reference IDNIYRA By-Laws

V. STARTING AND FINISHING SIGNALS

The starting signal will be the swift lowering of the starters arms or flag after he raises his arms or flag to signify the preparatory signal. If the start is to be delayed after the preparatory signal the starter will slowly withdraw his arms or flag. The finish of the first yacht shall be signaled by the raising of a checkered flag, which will remain raised for 20 minutes or until all yachts have finished, whichever is earlier.***

VI. TIME LIMIT

The lap time limit is 6-1/2 minutes per mile
(1 mile between marks = 13 minute lap time limit).

VII. PROTESTS

Intention of protest must be reported to the Race Committee immediately after the race. A written protest must be filed with the Race Committee Chairman within one hour of the race.

VIII. TALKING TO THE SCORERS

A skipper who attempts to speak to the scorers while a race is in progress may be liable to disqualification from the regatta.

STANDARD SAILING INSTRUCTIONS

IX. STARTING LINE

The Race Management Team may adopt the “Wire” starting line system if approved by the Governing Committee members present.

Note: This is the suggested format for IDNIYRA regatta sailing instructions. A complete knowledge and understanding of the following racing rules is critical to safe racing. Be sure you know and understand them before you race.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

The Racing Rules of the National Iceboat Authority¹

November 15, 2016

Part I Definitions

When one of the terms defined in Part I is used in its defined sense in the definitions or rules, it is printed in CAPITAL letters. All definitions rank as rules.

ACTUAL WIND – The natural wind.

WINDWARD-LEEWARD COURSE – A course sailed around two MARKS, an imaginary straight line drawn between the two MARKS is parallel to the ACTUAL WIND.

STANDARD COURSE - A WINDWARD-LEEWARD COURSE with a finish line placed 90 degrees from the ACTUAL WIND and a safety zone.
(Reference: Appendix I)

DARLING COURSE – A STANDARD COURSE modified with the addition of two DARLING MARKS. (Reference: Appendix I)

INLINE COURSE – A DARLING COURSE modified with the finish line being placed in line with the windward and leeward MARKS. (Reference: Appendix I)

ON-THE-WIND – A yacht heading less than 90° from the direction from which the ACTUAL WIND is blowing is ON-THE-WIND.

OFF-THE-WIND - A yacht heading more than 90° from the direction from which the ACTUAL WIND is blowing is OFF-THE-WIND.

STARBOARD TACK – A yacht is on a STARBOARD TACK when the ACTUAL WIND is approaching her from her right side.

PORT TACK - A yacht is on a PORT TACK when the ACTUAL WIND is approaching her from her left side.

WINDWARD YACHT and LEEWARD YACHT – When two yachts are on the same tack, the one on the side from which the ACTUAL WIND is blowing is the WINDWARD YACHT, the other is the LEEWARD YACHT.

TACKING – A yacht is TACKING from the moment she is beyond head-to-ACTUAL WIND until her mainsail has filled on the other side.

JIBING – A yacht is JIBING when, with the ACTUAL WIND aft, the foot of her mainsail crosses her centerline until it has filled on the other side.

¹ Rules interpretations are shown as footnotes

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

OBSTRUCTION – Any object a yacht cannot safely sail over.

MARK – Any object, which a yacht must round or pass on a required side to properly round the course.

DARLING MARK – A MARK that is positioned in line with and between the windward MARK and the leeward MARK. The imaginary line between the MARK and its accompanying DARLING MARK is considered part of the MARK.
(ref:Appendix I)

OUTSIDE – In rule 8 of the Right-of-Way Rules, any yacht to the right of another yacht is the OUTSIDE yacht.

CANCELLATION – A CANCELLED race is one which cannot thereafter be sailed.

POSTPONEMENT – A POSTPONED race is one which is not started at its scheduled time and which can be sailed at any time the Race Committee may direct.

ABANDONMENT – An ABANDONED race is one which is stopped while it is in progress and which can be re-sailed at the discretion of the Race Committee.

Part II Management of Races

Part II is written in a manner that can be applied to club races and large regattas. Any specifics such as wind velocity restrictions, course length, scoring systems, temperature minimums, ice thickness and condition, and number of boats on one course must necessarily be adapted to the type of boat being sailed.

A. Notice of races shall contain the following information (Ref:Appendix III, A):

1. That the races will be sailed under the rules of the National Iceboat

Authority and those of the association or class concerned.²

2. Date, place, of regatta and starting time of first race.

3. Class or classes for which races will be held.

4. Amount of entrance fee.

5. Time and place of registration.

6. Number of prizes.

7. Time and place for receiving sailing instructions.

8. Number of races scheduled and number required for a complete series.

B. Sailing Instructions (Ref:Appendix III, B):

1. May be written or verbal.

2. Contents:

a. Course location.

2 It is not the intent of the NIA to address the rules and regulations that govern the equipment used on iceboats. The use of electronics, radios, electrical, or hydraulic devices should be governed by the relevant sailing association or class rules.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

- b. Course description, including MARK description.
- c. Starting time.
- d. Starting and finishing signals.
- e. Time limit of race (over-all time limit and time per mile for each class).
- f. Time limit within which and address at which protests shall be lodged and heard.
- g. Description of scoring system.

3. Distribution - shall be available to each yacht.

4. Changes- written or verbal changes to the sailing instructions can be made not later than two minutes prior to the starting signal.

C. Officials - duties and responsibilities

1. Race Committee

a. Personnel (course personnel, starters, timers, scorers)

1. Course personnel:

- a. Determine if conditions (wind, ice, temperature, visibility) are safe; mark dangerous sections of ice.
- b. Supply MARKS and components for starting line. DARLING MARKS should be distinguishable from other MARKS.
- c. Set course (MARKS, starting line, and if applicable, safety zone).

2. Starters:

- a. Supply flags, loud speakers, and audible signals.
- b. Start race.

3. Timers: Time laps and the race.

4. Scorers:

- a. Supply score sheets, pencils, master scoreboard at headquarters, tallies for drawing for starting positions.
- b. Are positioned in teams of two persons each, to windward of the windward MARK and at the end of the finish line opposite to the leeward MARK.
- c. Record the race as follows: Each team has a score sheet. The score sheet is divided into as many columns as there are number of laps in the race. As the yachts pass the MARK, one member of the team calls the sail numbers to the other member, who writes them down on the score sheet. A new lap column is started each time the leading yacht completes a lap. After starting a new lap column, continue to mark all yachts in that column regardless of lap position until starting another lap column with the leading yacht. In the case of yachts which have been lapped by the leaders, the number of laps completed may be determined by the number of times such yacht's sail number has been tallied. After the last yacht finishes, the scorers must then work back through their tally sheet to determine which yachts, and in what order, finished the race properly.
- d. Post starting positions for succeeding race.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

- e. Transfer race results to master scoreboard and add up points.
 - f. Determine final finishing positions in the series.
- b. Safety: The Race Committee should have on hand:
 - 1. First aid equipment.
 - 2. Phone number of first aid squad.
 - 3. Equipment for getting boats and persons out of the water.
- 2. Judges
 - a. Authority - have the ultimate authority over all other officials in all incidents involving interpretation and enforcement of the rules governing all aspects of the race.
 - b. Must enforce all rules.
 - c. Oversee the race and must protest all violations of the rules.
 - d. Hear and decide protests.
- D. Courses- all courses shall be WINDWARD-LEEWARD courses, MARKS left to PORT.
 - 1. The course includes all MARKS, the starting line, the finish line, and, if applicable, the safety zone. See Appendix I for course diagrams.
 - 2. Typical spacing of MARKS
 - a. The MARKS of a WINDWARD-LEEWARD course are typically spaced 1 mile apart.
 - b. When using a DARLING COURSE or INLINE COURSE, the spacing between the MARK and its accompanying DARLING MARK is typically 100 yards when the windward and leeward MARKS are spaced 1 mile apart.
- E. Starting
 - 1. It is recommended that the starting line be a line perpendicular to the ACTUAL WIND and set 60-100 yards to leeward of the leeward MARK of the course and consist of marked positions. The center of the starting line shall lie on an extension of an imaginary line through the MARKS of the course. Numbered starting positions shall be placed along the starting line at intervals at least four times the length of the runner plank, odd numbers running from the center rightward and even numbers running from the center leftward (standing facing the course).
 - a. The components of the starting line do not rank as MARKS of the course.
 - 2. Starting positions for the first race shall be drawn by lot (or shall be determined by another means if so announced in the sailing instructions) before the first race. Entries arriving after the drawing shall be assigned positions on the ends of the line. For succeeding races a yacht will start at the position corresponding to her finishing place in the immediate preceding race and DNF's, DSQ's, and DNS's shall be assigned positions at the ends of the line by the race committee.
 - 3. Starting Procedure
 - a. At the starting line the Race Committee shall announce the course, number of laps, time limit for the race and for each lap, and if applicable, instruct all yachts not starting to clear the safety zone.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

b. When blocks are used to mark the starting positions, each yacht's windward runner is placed at the starting position, odd numbers on PORT TACK, even numbers on STARBOARD TACK if the PORT and STARBOARD TACK start system is used. When the starting positions are connected with a rope or wire, the leeward runner is placed at the starting position.

c. The Race Committee shall check to make sure all yachts are laid off (headed) similarly and the Committee has the authority to require a yacht to alter her heading.

d. There shall be a preparatory signal approximately 1 minute before the start. This shall be a visual signal made by the starter standing near the leeward MARK. He shall raise a flag or raise his arms. This may be accompanied by an audible signal (megaphone or gun).

e. The starting signal shall be the lowering of the starter's flag or arms. It may be accompanied by an audible signal. The visual signal governs the start; the audible signal is only a supplement.

f. After the starting signal, skippers may begin to move their yachts away from the starting line, either by pushing or sailing.

1. No yacht may be in forward motion at the starting signal.

2. A yacht that arrives at the starting line after the starting signal must come to a stop on the starting line before beginning the race.

3. When using a safety zone, it is recommended that when one class is lined up on the starting line when another class is racing, the starting positions to windward of the safety zone be vacated to leave room for yachts finishing to clear the finish line.

F. Finishing

1. STANDARD COURSE or DARLING COURSE

a. The finish line is 60 to 100 yards long and perpendicular to the ACTUAL WIND. It lies between a stake and the leeward MARK. The stake is placed so that a yacht crossing the finish line from the direction of the windward MARK shall leave the leeward MARK on her PORT side. The Race Committee will be positioned on an extension of the finish line at the stake end of the line.

b. The finish line shall be shifted at any time so as to remain perpendicular to the ACTUAL WIND.

2. INLINE COURSE

a. The finish line is 60 to 100 yards long and is parallel to the ACTUAL WIND. It lies between the leeward MARK and the 1-2 starting positions. A finishing yacht shall leave the leeward MARK on her PORT side.

b. After finishing, yachts should make every attempt to approach the parking area by sailing around the odd end of the starting line.

3. A yacht finishes when any part of her hull or equipment crosses the finish line while leaving the leeward MARK to PORT.

a. If provided for in the Sailing Instructions, the finish of a yacht may be recorded when the forward part of the mast crosses the finish line while leaving the leeward MARK to PORT.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

4.The finish of the first yacht shall be indicated by a flag displayed by the Race Committee as described in the sailing instructions; this may be supplemented by an audible signal.

G. Time Limit

1. Lap Time Limit - Time allowed for sailing any lap of the course.
 - a. The Race Committee establishes and announces the lap time limit. Individual clubs, classes, or associations may have a predetermined time per mile in their rules.
 - b. If at any time during a race no yacht completes any lap within the lap time limit, the race must be **CANCELLED** or **ABANDONED**.
 - c. Valid Race - A race is valid if any yacht makes each lap within the lap time limit.
 - d. Example case: If the lap time limit is 9 minutes, the leading yacht must complete the first lap before the starting time (ST) plus 9 minutes. If the leader completes the first lap at ST plus 7 minutes, her time at the completion of the lap (L1) is noted and the yacht leading (not necessarily the leader at L1) at the completion of the second lap must complete that second lap before L1 plus 9 minutes. (In this case, within 16 minutes of starting time.) Her time is noted (L2). L2 plus 9 minutes is the time for any yacht to complete the third lap, and so on for each lap.
2. Over All Time Limit - Any yacht not finishing within 30 minutes of the first yacht shall be designated Did Not Finish (DNF). This time limit may be altered by class, club, or association rules.

H. CANCELING, POSTPONING, ABANDONING, Changing Course

1. The Race Committee may **CANCEL**, **POSTPONE**, or change the course before the start provided it informs all yachts verbally or in writing.
2. The Race Committee may **CANCEL** or **ABANDON** a race after the start if a **MARK** has shifted or the course for any reason becomes unsafe.
 - a. When a **MARK** has shifted:
 1. The Race Committee should make every attempt, without endangering the safety of racing yachts or Race Committee personnel, to reset or replace the **MARK** while the race is in progress, without abandoning the race.
 2. Yachts should round any portion of the **MARK** that remains in the original position.
 3. If no portion of the **MARK** remains in the original position, yachts should round the portion in the shifted position until the **MARK** is reset, replaced, or the race is abandoned.
3. The Race Committee must **CANCEL** or **ABANDON** a race after the start if no yacht makes any lap time limit.
4. The course may not be altered after the start.
5. All yachts concerned shall be notified as to the date, time, and place of the

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

resail of a POSTPONED or ABANDONED race.

6.The signal for CANCELING or ABANDONING a race shall be the placement of a visual signal (e.g. flag, flare) at the leeward MARK.

I. Dead Heat- In the case of a dead heat in any one race, add the points for the place for which the yachts are tied and the place (or places, if more than two yachts are involved in the tie) immediately below the tied yachts and divide equally. Any yacht finishing immediately after the tied yachts shall be awarded the position corresponding to one worse than the number of yachts finishing ahead of her.

J. Accidents

1. In the event of an accident, the Race Committee may ABANDON or CANCEL the race. Each yacht must render every possible assistance to any yacht or person in peril, even if the person in peril is not racing. A yacht that renders assistance may file a written request for compensatory points and the Judges may award her points equal to her worst race in the series, disregarding the race in question, did not start, did not finish, and disqualifications.

2. A skipper of a yacht in distress shall make every attempt to signal all passing yachts and, if possible the Judges whether he is seriously injured or not. If he is not, he should get out of his cockpit and stand up and walk around the yacht.

K. Resailed Races; When a race is to be resailed:

1. All yachts entered in the original race shall be eligible to sail the resailed race.
2. Subject to the entry requirements of the original race, and at the discretion of the Judges, new entries may be accepted.
3. Rule infringements in the original race shall be disregarded.
4. The Race Committee shall advise the yachts concerned of the date, time, and place of the resailed race.

L. Award of Prizes

1. Before awarding the prizes, the Race Committee shall be satisfied that all prize-winning yachts have complied with the racing rules, sailing instructions, and class rules.

2. If, within a 60 day period from the original date of award, the Judges shall find that a yacht did not comply with the racing rules, sailing instructions, or class rules, they may demand return of the award.

Part III General Requirements

A yacht intending to race shall, to avoid subsequent disqualification, meet these general requirements:

1. Pay all required membership and entrance fees.
2. Meet the measurement requirements of the association sponsoring the race.
3. Shall not be sailed by a professional(s) ice yachtsman. A professional being defined as anyone who accepts money for sailing on an ice yacht. If the

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

status of an ice yachtsman (as to amateur or professional) is questioned, the National Iceboat Authority shall be the ultimate authority in determining his status.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

Part IV Sailing Rules

The purpose of the following rules is to prevent collisions.³ Any infraction of these rules is cause for disqualification.

A. Fair Sailing. In all situations, the Judges, Race Committee, and contestants must act in terms of common sense, safety, and good sportsmanship.

B. Right-of-Way Rules.⁴

1. A yacht in motion shall keep clear of a yacht stopped.

2. A yacht sailing OFF-THE-WIND shall keep clear of a yacht sailing ON-THE WIND.

3. When two yachts are sailing ON-THE-WIND, the yacht on the PORT TACK shall keep clear of the yacht on the STARBOARD TACK. When two yachts are sailing OFF-THE-WIND, the yacht on the PORT TACK shall keep clear of the yacht on the STARBOARD TACK.

4. When two yachts sailing ON-THE-WIND are on the same tack, the WINDWARD YACHT shall keep clear. When two yachts sailing OFF-THE-WIND are on the same tack, the LEEWARD YACHT shall keep clear.⁵

5. A right-of-way yacht shall not alter her course so as to mislead or prevent a nonright-of-way yacht from keeping clear. When a faster moving yacht approaches another yacht on the same tack from the rear, the faster yacht must not sail so close that the slower yacht cannot keep clear.

6. A yacht may not TACK or JIBE so as to involve the probability of collision with another yacht which, owing to her position or speed, cannot keep clear.

7. A yacht approaching and unable to clear an OBSTUCTION without fouling or endangering another yacht may signal the other yacht for room to clear. The signaled yacht shall at once give room and if it is necessary for her to TACK or JIBE, the signaling yacht shall also TACK or JIBE immediately thereafter.

8. When approaching or rounding a MARK, an OUTSIDE yacht shall keep clear and a faster moving yacht approaching another yacht from the rear shall stay clear of a yacht that has started her rounding maneuver.

a. When yachts sailing ON-THE-WIND on opposite tacks are approaching a MARK, the PORT TACK yacht shall keep clear of the STARBOARD TACK yacht.

b. Each yacht shall be entitled to room to cross the finish line.⁶

3 The Sailing Rules and Race Management Rules are designed to provide a safe and fair race. The rules should not be used with tactical advantage a prime concern, but rather should be used to avoid collisions.

4 Sailors should realize that iceboat racing rules differ from sailboat racing rules in a number of fundamental ways, most prominent of which are: starboard tack does not always have right-of-way; windward yacht has right-of-way off the wind; there is no three length rule at an iceboat course mark.

5 Once the forward most part of the yacht catching up from behind has come past the after most part of the yacht ahead, one yacht is windward, the other leeward. The yachts must then be governed by their responsibilities as windward and leeward yachts.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

9. After finishing a race, a yacht shall keep clear of the course and yachts still racing.

C. Sailing the Course ⁷

1. At the start a yacht must be laid off (headed) similarly to other yachts on the line. A skipper may demand that the race committee require another yacht to change the degree to which she is laid off.
2. A yacht shall be disqualified without protest if she starts prematurely.
3. After a yacht starts and until she rounds the windward MARK for the first time, a yacht shall not pass between the center of the starting line (positions 1 and 2) and the leeward MARK.
4. A yacht not leaving a MARK on the required side or not rounding all MARKS in proper sequence shall be disqualified.
5. Except when avoiding an accident, a yacht shall be disqualified if she fouls a MARK, fouls a DARLING MARK, or passes over the imaginary line connecting the MARK and the accompanying DARLING MARK.
6. Yachts are required to pass over the finish line only at the finish. On previous laps of the course, they need not cross the finish line.

D. Propulsion – A yacht may not employ any means of propulsion other than the action of the wind on the sails. However, the crew (unassisted by anyone except for reasons of physical disability as authorized by the Judges) may push the yacht to achieve windpropulsion. Other pushing shall be cause for disqualification.

E. Safety Zone – When using the STANDARD COURSE or the DARLING COURSE, the Race Committee shall designate a safety zone (ref: Appendix I) to leeward of the starting line. A yacht not competing in the race about to be started or the race in progress shall not enter the safety zone. After a warning by the Race Committee, a yacht found to be in the Safety Zone without just cause may be disqualified or otherwise penalized according to the provisions of Part V of the rules.

F. Ballast – A yacht must start and finish a race with the same ballast and crew.

6 This rule is for the ends of the line and is designed to keep the race committee from getting killed and to avoid general havoc in the finishing area. Port tack yachts shall keep clear of starboard tack yachts except at the committee end of the line where port tack must be given room to finish. And at the ends of the line a windward yacht must allow a leeward yacht room to finish.

7 The rules do not allow boats to collide at the start or shortly thereafter and all sailing rules apply here as they do elsewhere on the course.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

Part V Protests, Disqualifications, Appeals

A. Protests

1. Parties to protests.

a. Who may protest:

1. Any competing yacht.

2. The Race Committee, or any member of the Committee.

3. A Judge.

b. Who may be protested:

1. Any competing yacht.

2. The Race Committee.

2. It is mandatory for all the parties in 1a, above to protest any infringement of the Racing Rules, Parts I, II, III, IV. A protest may not be withdrawn.

3. A party entering a protest shall:

a. Make his intent to protest known to the Race Committee immediately after the race in which the rule infringement occurred or as soon as an infringement of other than the Sailing Rules is noted. A protest may be entered at a later time if the protesting party is unable to finish the race, but must be made within two hours of the finish of the day's racing unless the protesting party can prove, to the satisfaction of the Race Committee, that he was unable to meet the deadline.

b. Present the protest in writing at the time and place indicated in the sailing instructions, stating the rule violated, a statement of the facts, and a diagram to illustrate same (when relevant).

4. The Race Committee shall do its best to notify the protested party as soon as possible.

5. The Judges must call a hearing as soon as possible, allowing a reasonable time for preparation of defense. Protests from one day's racing should be heard before the next day's racing begins and, on the last day of racing, before prizes are awarded. Failure on the part of any interested party to make an effort to attend the hearing may justify the Judges in dismissing the case or in deciding the protest as they see fit.

B. Decisions and Penalties

1. The Judges shall make a prompt decision and notify, verbally or in writing, the parties involved.

2. If, during the hearing, any yacht is found to have infringed any of the Racing Rules she shall be disqualified or otherwise penalized according to any special sailing instructions.

3. If the Race Committee is found to have infringed a Racing Rule and a yacht's chances to win a prize were prejudiced as a result of the infringement, the Judges may order the race resailed or award the prejudiced yacht in accordance with the alternative provided in any special sailing instructions. (Ref. Appendix V.B.5)

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

C. Interested persons must not take part in decisions. No Judge may hear a protest involving a yacht he owned or sailed on at the time in question or involving a Race Committee of which he was a member.

D. Appeals to the National Iceboat Authority

1. Appeals involving solely the interpretation of the Racing Rules may be taken to the National Iceboat Authority for final determination by any individual or group that is a member of the Authority.

2. Preparation of Appeal Papers- All appeals shall be in writing and shall set forth the grounds of the appeal and be signed by the appellant. They shall be filed with the Secretary of the Authority within thirty days after the rendering of the decision appealed from together with:

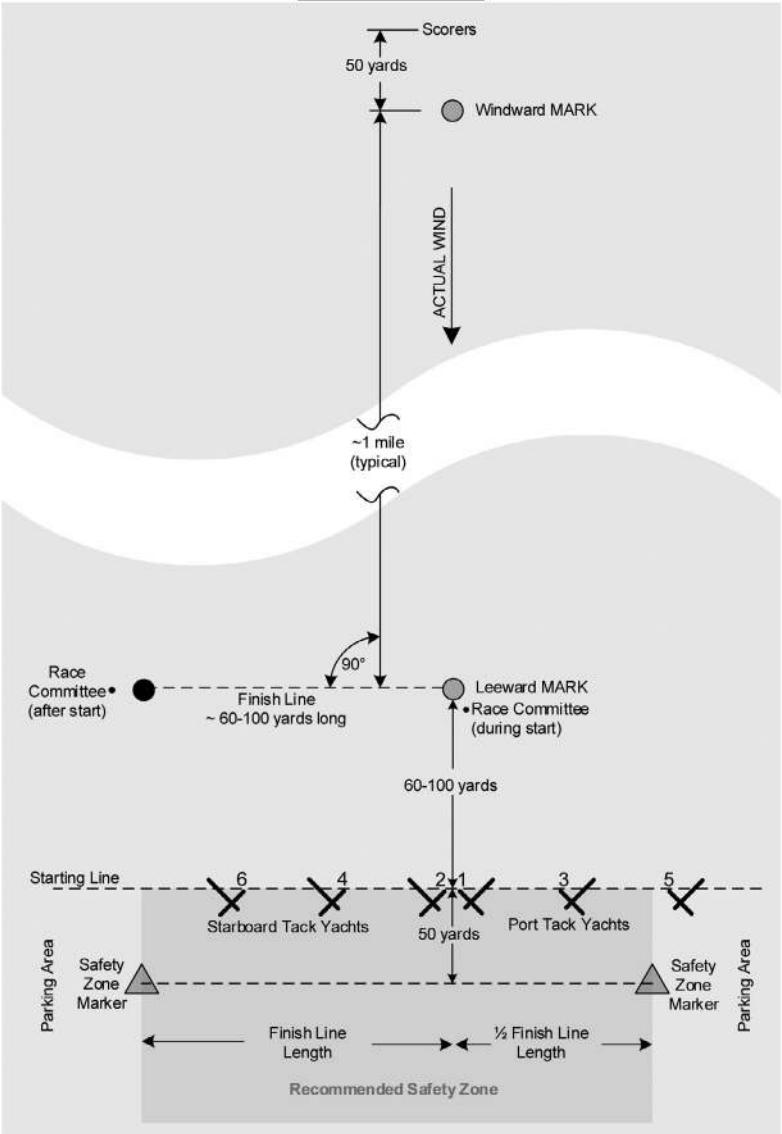
- a. The written consent to the appeal signed by the Judges rendering the original decision.
- b. A copy of the sailing instructions.
- c. A copy of the protest.
- d. The names of the parties represented at the hearing, and of any party duly notified of the hearing, but not represented.
- e. A copy of the decision of the Judges containing a full statement of the facts found by them.
- f. An official diagram prepared by the Judges in accordance with the facts found by it and signed by it showing:
 1. The direction and velocity of the wind.
 2. Temperature and ice conditions.
 3. Visibility.
 4. Positions and tracks of all yachts involved and their position in relation to the course and the MARKS of the course.

3. Decisions of Directors of the National Iceboat Authority shall be in writing and the grounds of each decision shall be specified therein. The decision shall be made within 30 days of the date the appeal was properly filed with the Secretary and shall be sent to all parties to the infringement and appeal. The annual report of the Directors shall contain all the decisions.

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

APPENDIX I: RACE COURSE CONFIGURATIONS

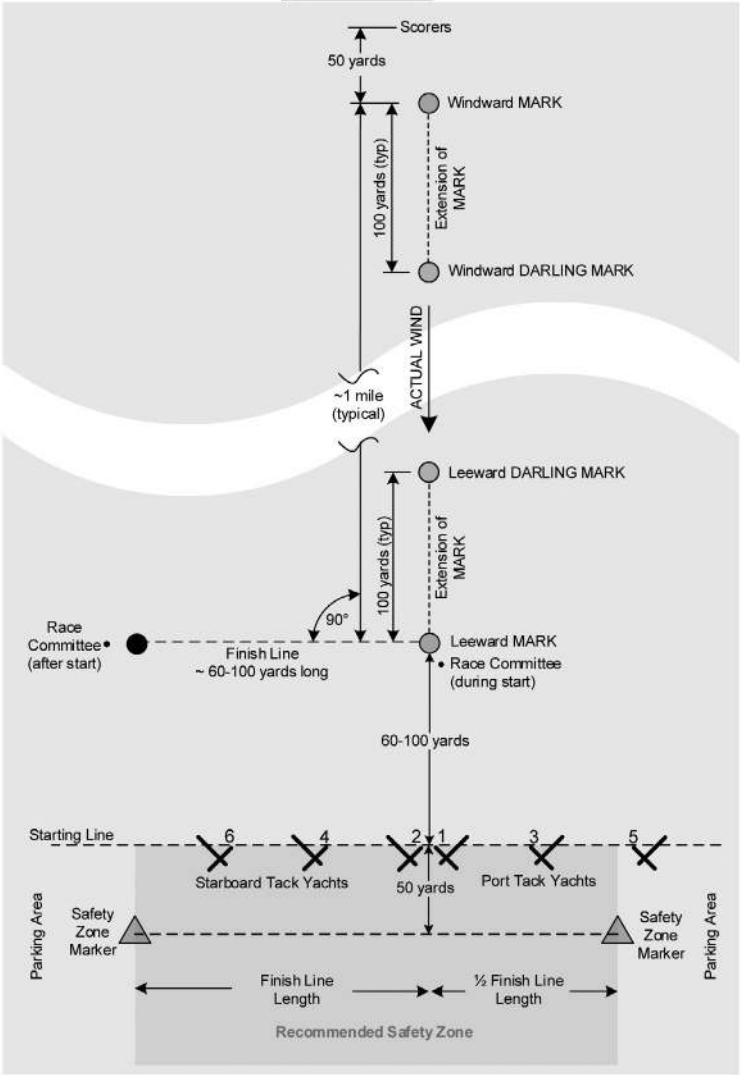
STANDARD COURSE



RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

DARLING COURSE

DARLING COURSE





RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

APPENDIX III

Sample Forms for Race Notices and Sailing Instructions (Part II, A & B)

A. Notice

(Name of Regatta)

1. All races will be sailed under the rules of the National Iceboat Authority and those of the (class or sponsoring association).
2. The regatta will be held (date) at (location). The first race will be held at (time, date).
3. Races will be held for classes: (class)
4. The entrance fee is (amount) per yacht.
5. Registration will be at (headquarters) on (date) from (time) to (closing time).
6. Trophies will be awarded to (how many places).
7. Skippers may obtain sailing instructions (where, when).
8. (Number of) races are scheduled for each class. (Number of races) are necessary for a complete regatta.

B. Sailing Instructions (distribute at registration desk or at skippers' meeting)

1. The course will be the (where the racing ice is in respect to place of registration).
2. The race course will be (description of race course used for each class) and racing MARKS will be (description of MARKS used for each class).
[Note to Race Committee: It is recommended that the race course configuration not be changed for the duration of a regatta.]
3. The starting time for each race:
4. The starting signal shall be the lowering of the starter's (arms or description of flag) approximately 1 minute after he raises his (arms or description of flag) to signify the preparatory signal. The finish of the first boat will be indicated by (description of flag and how it will be displayed at the finish) by the race committee.
5. The time limit per mile for each class will be: _____ Any yacht not finishing within 30 minutes of the first yacht shall be scored DNF.
6. The scoring system will be: _____
7. Intention to protest must be reported to the race committee immediately after the race. Written protest must be filed with the Judges at (where they should be filed) within two hours of the finish of the last race of each day.

APPENDIX IV
NATIONAL ICEBOAT AUTHORITY PROTEST FORM

Regatta or series _____

Protesting yacht: Class _____ Sail Number _____

Skipper _____

Protested yacht: Class _____ Sail Number _____

Skipper _____

Date and race number of incident: _____

Witnesses who saw the incident: _____

Statement of the Facts

Rule(s) the protestor believes were violated: _____

Time protest was reported to Race Committee: _____

Temperature: _____ Wind Velocity: _____ Visibility: _____

Ice condition: _____

Diagram

Indicate wind direction position and tracks of all yachts involved. Also, show their positions in relation to the course and the MARKS of the course.

Signature, skipper of protesting yacht: _____

FOR RACE AND PROTEST COMMITTEE (JUDGES) USE

Protest received (date, time): _____ By: _____
(official)

DECISION OF PROTEST COMMITTEE (JUDGES)

Facts Found

Decision and grounds, including rule(s) infringed and yacht(s) infringing.

Diagram:

yes or no _____'s diagram is endorsed.

yes or no Diagram constructed by the Judges is attached.

Chief Judge: _____

Names of other Judges hearing protest: _____

Date of hearing: _____ Date of Decision: _____

RACING RULES OF THE NATIONAL ICEBOAT AUTHORITY

Rules Interpretations

December 1, 1973

1. The Sailing Rules and Race Management Rules are designed to provide a safe and fair race. The rules should not be used with tactical advantage a prime concern, but rather should be used to avoid collisions.

2. Sailors should realize that iceboat racing rules differ from sailboat racing rules in a number of fundamental ways, most prominent of which are: starboard tack does not always have right-of-way; windward yacht has right-of-way off the wind; there is NO two length rule at an iceboat course mark.

3. The rules do not allow boats to collide at the start or shortly thereafter and all sailing rules apply here as they do elsewhere on the course.

4. Right-of-way rule #8 giving each yacht room to finish is for the ends of the line and is designed to keep the race committee from getting killed and to avoid general havoc in the finishing area. Port tack yachts shall keep clear of starboard tack yachts except at the committee end of the line where port tack must be given room to finish. And at the ends of the line a windward yacht must allow a leeward yacht room to finish.

5. Once the forward most part of the yacht catching up from behind has come past the after most part of the yacht ahead, one yacht is windward, the other leeward. The yachts must then be governed by their responsibilities as windward and leeward yachts.

North American Ice Yacht Clubs

CONNECTICUT

Bantam Lake Yacht Club
Bill Cady 203-573-1560

Belly Up Yacht Club
(Lake Pocotopaung)
Bob Kilpatrick 203-267-8173 (eve.)

Connecticut Ice Yacht Club
(Bantam Lake)
Bob Johnson 203-274-3652

Westchester Ice Sailing Club
Warren Dartess 516-883-9004 (eves.)

Eastern Connecticut
Paul O'Niell 203-599-3607

ILLINOIS

Decatur Ice Sailing Club
Peter Johns 217-429-3401

Lake Springfield Ice Yacht Club
Tim Dixon 217-529-7417

Illiana Yacht Club
John Whelan
219-989-0452

INDIANA

Indianapolis Sailing Club
Mike Rian 317-549-1817 (days)

Wawasee Ice Boat Squadron
Rick Lemberg
260-856-4563

IOWA

MAINE

Chickawauke Ice Boaters Club
Lloyd Roberts
207-596-2095

MARYLAND

Maryland Ice Yacht Club
Jim Holechek 310-433-7187

MASSACHUSETTS

Steve Madden 508-954-9243

MICHIGAN

DN Ice Yacht Club of Detroit
Chris Clark 248-988-0851

Grand Traverse Ice Yacht Club
John L. Russell 231-947-2737

Gull Lake Ice Yacht Club
Doug Bottomly 734-552-2258

Jackson Michigan Ice Boat Fleet
Mel Nichols 517-522-8107

Saginaw Bay Sailors
Bob Struble 989-686-9899

West Michigan Ice Yacht Club
Stan Jones 231-893-3319

MINNESOTA

Minnesota Ice Sailing Association
John Dennis 612-840-9855
<http://iceboating.net>

MISSOURI

St. Louis Ice Yacht Club
Bill Cornell 314-639-8825

MONTANA

Canyon Ferry
Dale Livezey 406-442-8922

Teton Ice Boaters
Phil Vorhees 404-466-2477

North American Ice Yacht Clubs

NEW ENGLAND ICE YACHT ASSOCIATION AREA

Managers

Maine-1

Lloyd Roberts 207-596-2095

Maine-2

Dave Fortier 207-400-9709

Vermont

Paul Gervais 802 343 7899

Connecticut

John Stanton john@neiya.org

Rhode Island

Jim "T" Theiler 401-258-6230

Massachusetts

Steve Madden 508-954-9243

NEW HAMPSHIRE

Kate Marrone k8innh@yahoo.com

Randy Rice sailfast1@myfairpoint.net

NEW JERSEY

Barnegat Bay Ice Yacht Club

Raymond Albert

Lake Hapatcong Ice Yacht Club

Peter Block 201-663-3051

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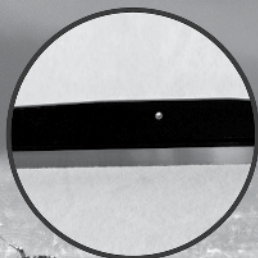
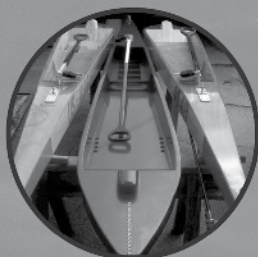
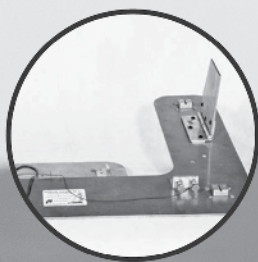
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Composite Concepts - Ron Sherry	Page 237
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Hale Performance Coating - Mike Deye	Page 217
Icewise- Peter Hamrek	Page 17
North Sails - Chad Atkins	Page 71
North Wind Iceboats/ID Sails - Steve Duhamel	Page 79
RKR Composites - Bob Rast	Page 224
Toledo Ice Yacht Club	Page 6
Ullman Sails - Mike Boston	Page 154

