

MAGAZINE OF THE INTERNATIONAL DN ICE YACHT RACING ASSOCIATION

# RUNNER TRACKS

SEPTEMBER 2022



**PROPOSALS FOR SPECIFICATIONS A & B FROM THE IDNIYRA TECHNICAL COMMITTEE**  
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Cover Photo: Peter Van Rossem KC2766 and JR Francis US807 at the 2022 DN Nationals on Lake Senachawine in Illinois. Photo: Gretchen Dorian



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RUNNER TRACKS is edited by IDNIYRA Secretary Deb Whitehorse

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# COMMODORE'S REPORT

FROM THE COMMODORE

JODY KJOLLER US5435, TEMPERANCE, MICHIGAN, USA

Summer is wrapping up. Time to make sure you are ready for the ice. It will be here before you know it.

The IDNIYRA Technical Committee has been working very hard at getting a few proposals ready for the membership. The big news is past Commodore Warren Nethercote's complete rewrite of the class specifications. The rewrite has taken a lot of work. It should make things much easier to read and interpret.

We've also got a couple of specification and rule changes coming from last April's European Secretar-

ies meeting, as well as a change to our rules here in North America. Look for these in this issue of Runner Tracks.

The Toledo Ice Yacht Club is hosting a golf outing on October 16, 2022. Let me know if anyone wants to come to town and get a foursome together. I can get you the details.

Think Ice

*IDNIYRA Commodore Jody Kjoller US5435*



ggd

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# PROPOSAL FROM THE TECHNICAL COMMITTEE: REVISE SPECIFICATIONS, SECTION A: FUSELAGE; SECTION B: RUNNER PLANK

Over the summer of 2022, the IDNIYRA Technical Committee held six online meetings to revise the wording of the Official IDNIYRA Specifications, specifically Section A (fuselage) and Section B (runner plank).

Their main goals were to

- Incorporate the Interpretations into Official Specifications.
- Rewrite for more user-friendly wording.
- Enrich each section with a measurement guide and measurement drawings.
- Consolidate fragmented sections.
- Eliminate loopholes and grey areas by giving more precise definitions.
- Maintain specifications

Following is the result of their work. IDNIYRA class members can comment on the proposed revisions at the 2023 Annual Governing Meeting, held during the January 2023 World & North American Championship. IDNIYRA and IDNIYRA European class members will vote on the proposed changes via electronic ballot in the spring of 2023.

## IDNIYRA TECHNICAL COMMITTEE

**News from TC:**

**Total of 6 online meetings completed.**

**Submitted Section A Fuselage and B Runner Plank**



Jeff Kent US3535	David Croner S1	Paul Goodwin US46	Tomek Zackrzewski P55	Steve Orlebeke US4926	Argo Vooremaa C36
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## IDNIYRA TECHNICAL COMMITTEE MEMBERS



## A Fuselage

### 1 Materials

- 1.a Fuselage shall be constructed of wood only except as provided in specifications A.1.b, A.1.d, and A.1.e.
- 1.b Only fiberglass may be added to wood components for reinforcement only. Other fiber reinforcement materials, such as but not limited to carbon fiber or para-aramid fibers are prohibited.
- 1.c Engineered or composite materials manufactured by binding particles or loose fibers of wood and/or fiberglass are not allowed in the side panels.
- 1.d Core materials are allowed for the internal construction of the fuselage.
- 1.e Adhesives and fasteners are allowed.

### 2 Construction Methods and Details

- 2.a Design of the internal structure of the fuselage including cockpit floor is optional.
- 2.b Fuselage sides (side panels) and the bulkhead at the front of the cockpit must be solid or laminated wood and shall not have any internal cavities. Adhesives and fiberglass reinforcement may be used between individual wood layers of laminated side panels, but the total thickness of wood laminations must satisfy Specification 4f.
- 2.c Where minimum dimensions are specified for wood components, fiberglass reinforcement may be added to the components only after minimum required dimensions of wood are satisfied.
- 2.d The decks, fuselage bottom skin and cockpit floor skin shall be wood (typically plywood). Fiberglass reinforcement may be added after minimum required dimensions of wood are satisfied.
- 2.e A full bulkhead must be installed at the front of the cockpit.
- 2.f Structural members such as longerons, stringers, knees, listings, bulkheads, etc. may be added provided the addition does not infringe or exceed the specifications and allowed materials.
- 2.g Knees or other means of side panel support in the cockpit shall be made of wood and must be installed as provided in specifications A.1.b, A.1.d, and A.1.e.
- 2.h The deck may not protrude more than 3" (76.2mm) into the cockpit from the bulkhead at the front of the cockpit. For the purposes of this specification the deck is the covering on the top of the fuselage in the areas forward and aft of the cockpit. The deck extends uninterrupted from the outside of the side panel on one side to the outside of the side panel on the other side.
- 2.i The seatback shall be flat, measuring 11" (279.4 mm) in length at the center line, no maximum height. The crown on top of the seat must be a minimum of 2" (50.8 mm) radius with minimum seat width 4" (101.6mm). Top edge of the seatback is determined by use of template. See 5.a.
- 2.j The seatback shall be raked aft at an angle of 45 degrees plus or minus 10 degrees, measured from the cockpit floor skin. The seatback may allow access to a stowage compartment.
- 2.k Grab-rails may be installed on the inside and/or outside vertical surface of the side panels. They may not extend beyond 8" (203.2 mm) from either end of the cockpit. Rails shall not exceed 1" (25.4 mm) in depth or width. Rails are exempted from fuselage measurements
- 2.l Ballast, if used, shall be permanently installed.

### 3 Fuselage Fittings

- 3.a Rigging and fittings are not part of the integral structure or reinforcement of the fuselage and may be attached by mechanical fastenings and/or adhesives. The materials allowed in the rigging and fittings attached to the fuselage are optional but must comply with the specifications in sections H and I, and interpretations in sections General, H and I. The dimension and location of the rigging and fittings must comply with the specifications in sections A, H, I, and the interpretations in sections General, H, and I.
- 3.b Steering
  - 3.b.i Steering shall be accomplished by means of a tiller. Tiller may be of any length or shape but shall not be more than 8 inches (203.2 mm) wide. Material is optional and may include para-aramid fabric (Kevlar).
  - 3.b.ii
    - a. The tiller post shall be located forward of the bulkhead at the front of the cockpit. The tiller shall be attached to the post at a point above the level of the deck. The sheet block that is installed on the tiller post shall be attached to the tiller post at a point higher than the point of attachment of the tiller.
    - b. For the purposes of Specification A.3.b.ii.a, bails, straps, lashings, shackles or strops used to attach the sheet block to the tiller post are neither part of the block nor the tiller post; they are fittings.
  - 3.b.iii The steering post and chock may be inclined in the vertical plane of symmetry.
  - 3.b.iv Steering rods, cables, or a single push-pull link shall be used and shall be positioned beneath the fuselage as shown on the Official Plans.
- 3.c National letter and sail number affixed to each side of the fuselage below the mast stepping point is optional. If a national sail letter and sail number are affixed to the fuselage, the color shall contrast with the fuselage and the recommended size of each letter and number is 5.5 inches (139.7 mm) high and 0.79 inches (20.1 mm) wide. National letters and sail numbers affixed to the fuselage shall match those on the sails, except for boats loaned or chartered for a regatta.

### 4 Dimensions and Weights

- 4.a Hull outer surface cross section must be rectangular  $\pm 2$  degrees from a point 6" (153. mm) from the bow to a point 6" (153. mm) from the stern. Exclusions: Cockpit surface, seat back, knees, rails, fittings, and hardware. A maximum 1/4" (6.4 mm) radius is allowed on the corners of the fuselage where the deck / bottom and side panels intersect. Concavities in the deck are not permitted.

	English (in)		Metric (mm)	
	Max	Min	Max	Min
b. Length Overall (not including bow tang or bobstay fittings)	147	141	3733.8	3581.4
c. Beam (measured to the inside of any external grab-rails)	21.5	17.5	546.1	444.5
d. Thickness of wood in deck, cockpit floor and bottom skins	0.25	0.118	6.4	3.0
e. Minimum thickness of wood in side panels and bulkhead at front of cockpit		0.625		15.9
f. Maximum thickness of wood and any fiberglass reinforcement in side panels and bulkhead at front of cockpit	1		25.4	
g. Total thickness of cockpit floor (cockpit floor skin plus internal structure plus bottom skin)		0.625		15.9
h. Intersection of seat back and cockpit floor skin to pivot point of steering runner. See 5.c.	110.25	98.25	2800.4	2495.6
i. Distance from bow (not including bow tang) to front of cockpit	66	54	1676.4	1371.6
j. Width of forward most point of bow	3.75	2	95.3	50.8
k. Width of aft most point of stern	4	1.25	101.6	31.8
l. Horizontal distance from the bow (not including bow tang) to the pivot point of the front runner	7	3	177.9	76.2

- 4.m.i The total height of side panel at each fuselage station shall not be less than heights in Table A1 (The minimum side panel height does not include the deck and bottom).

- 4.m.ii The bottom surface of the fuselage (side panels including bottom skin), shall not exceed a maximum of 1" (25.4 mm) above zero line and/or a maximum of 1/2" (12.7 mm) below zero line. Zero line shall be established by a straight line from bow to stern on bottom surface (not including bow tang or bobstay fitting).
- 4.m.iii Maximum height of the fuselage side (side panel including deck) above zero line may be 8-1/2" (215.9 mm). All heights of fuselage sides shall be proportional to height as shown in "Layout of Side Panel". The top edge of the side panel may not be concave at any point along its length. When describing the fuselage side profile: the term "proportional" will be defined as a "smooth curve" without reversing the line abruptly.
- 4.m.iv A side panel profile is acceptable if there are no concavities on or along its upper edge, the maximum height of fuselage sides is not exceeded, and the side panel height at each station is not less than the specified minimum.

Note: Table A.1 applies to the side panel without deck and bottom skin to assist the builder. Specifications 4.m.ii and 4.m.iii apply to the side panel plus deck and bottom skin to assist the measurer.

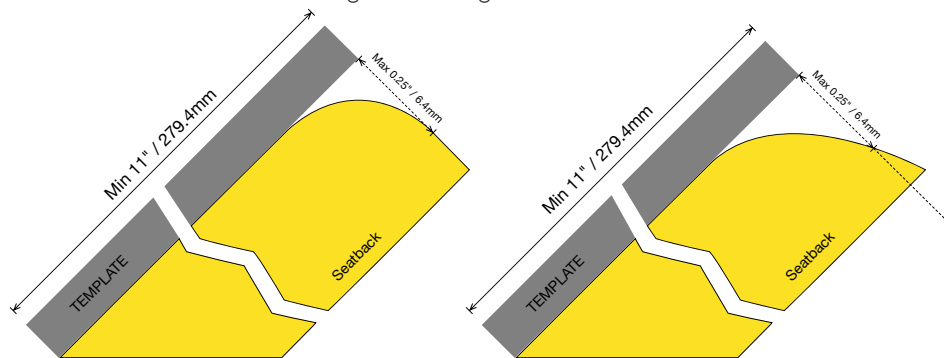
Table A.1: Layout of Side Panel/Minimum Side Heights

Position along Side Panel Starting at Bow		Minimum Side Panel Height (Excluding deck and bottom skin)	
English (in)	Metric (mm)	English (in)	Metric (mm)
2	50.8	2.563	65.1
12	304.8	3.875	98.4
24	609.6	5.063	128.6
36	914.4	6.063	154.0
48	1219.2	6.438	163.5
60	1524.0	6.625	168.3
72	1828.8	6.625	168.3
84	2133.6	6.500	165.1
96	2438.4	6.188	157.2
108	2743.2	5.563	141.3
120	3048.0	4.625	117.5
132	3352.8	3.375	85.7
Last station: 2 in.	(50.8mm) from stern	2.000	50.8

- 4.n Minimum weight of fuselage complete with all hardware, blocks, tiller, and any ballast required to achieve minimum weight is 46 lbs (20.9kg).

**5 Measurement**

- 5.a Top edge of seatback is determined according to following method:

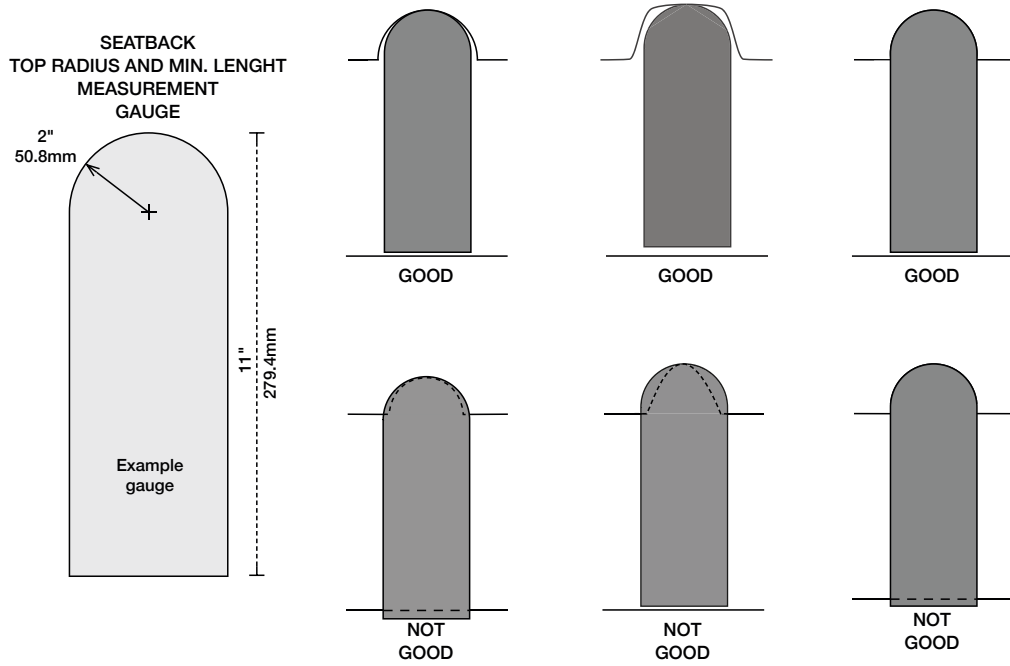


Dotted line represents top edge of the seatback

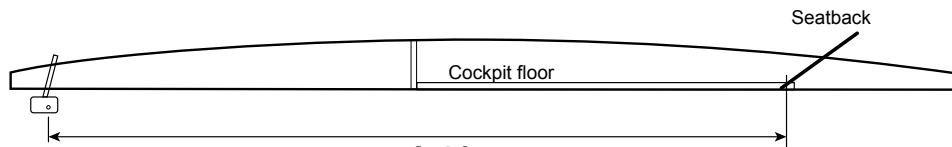
Top of seat shall not have a gap more than .25" /6.4mm measured at 90 degrees from template / seat back to actual seat back surface.

5.b Seatback minimum length at centerline and crown measurement.

**SEATBACK MEASUREMENT EXAMPLES**



5.c Measuring intersection of seat back and cockpit floor skin to pivot point of steering runner.



**A.4.h**  
Max 110.25" / 2800.4mm  
Min 98.25" / 2495.6mm

## B Runner plank

### 1 Materials

1.a The allowed materials in runner plank construction are wood, fiberglass (as external reinforcement), adhesives (for gluing or lamination), and non structural cosmetic coatings.

### 2 Construction

2.a The number of wood laminations is optional.

2.b The cross Section is optional. Runner plank may be of hollow construction, but only wood or adhesives for lamination purposes may be used to comply with the minimum length, width, and thickness requirements in specifications B.3.a-f.

2.c Fiberglass and coatings may be added to the exterior of the wood plank after minimum length, widths and thicknesses are satisfied (specifications B.3.a-f). Added fiberglass and coatings shall not cause the runner plank to exceed the maximum length, width, and thickness (specifications B.3.a-f).

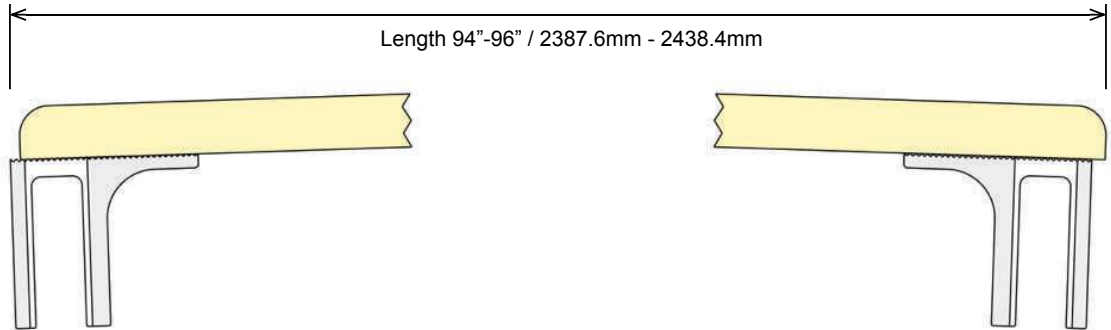
2.d The underside of the runner plank shall be higher at the centerline than the underside of the outside ends ("crown"). The profile of this crown shall form a fair continuous curve. Reversals of the curve are prohibited, and the curvature cannot be concave. Cutouts or depressions are prohibited.

### 3 Dimensions and weight

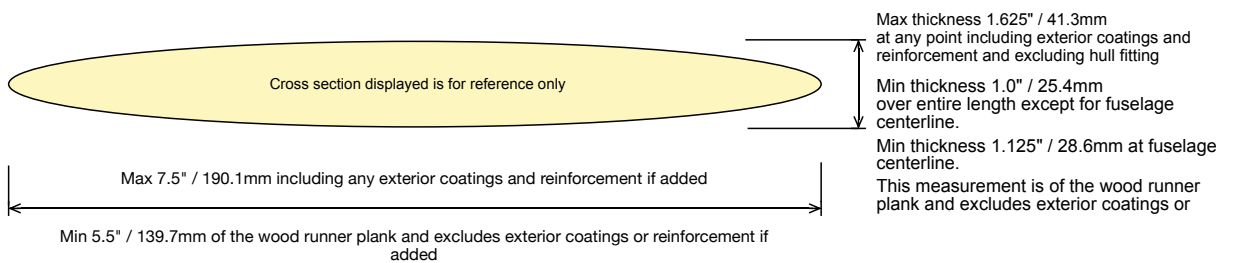
		English		Metric	
		Max	Min	Max	Min
a.	Length overall including hardware except pivot bolt. Max dimension refers to finished plank, min dimension refers to wood before adding reinforcements and external coatings.	96	94	2438	2388
b.	Minimum width along entire length of the plank. This measurement is of the wood runner plank and excludes exterior coatings or reinforcement if added.		5.5		139.7
c.	Maximum width along entire length of the plank including any exterior coatings and reinforcement if added.	7.5		190.5	
d.	Minimum thickness over entire length except for fuselage centerline. This measurement is of the wood runner plank and excludes exterior coatings or reinforcement if added.		1.0		25.4
e.	Minimum thickness at fuselage centerline. This measurement is of the wood runner plank and excludes exterior coatings or reinforcement if added.		1.125		28.6
f.	Maximum thickness at any point including exterior coatings and reinforcement and excluding hull fitting pads.	1.625		41.3	
g.	The runner plank, including all hardware and pivot bolts, shall not weigh less than 20 lbs. (9.1 kg).				

**4 Measurement**

4.a Specification B.3.a Length overall shall be measured in a straight line while not mounted on the fuselage and relaxed (under no load). The measurement shall be taken from the end of the runner plank or the chock, whichever gives the longer length; the measurement point may be different on different ends of the plank to determine the longest length. Overall length includes hardware but not pivot bolt.

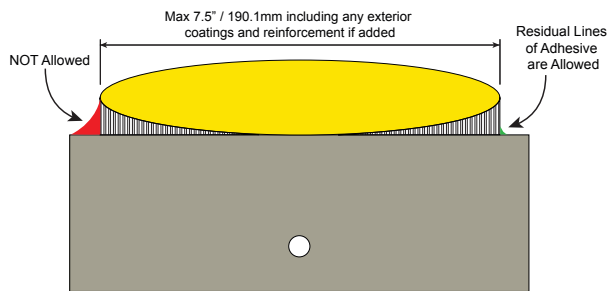


4.b Specifications B.3.b-f. Width and thickness requirements.

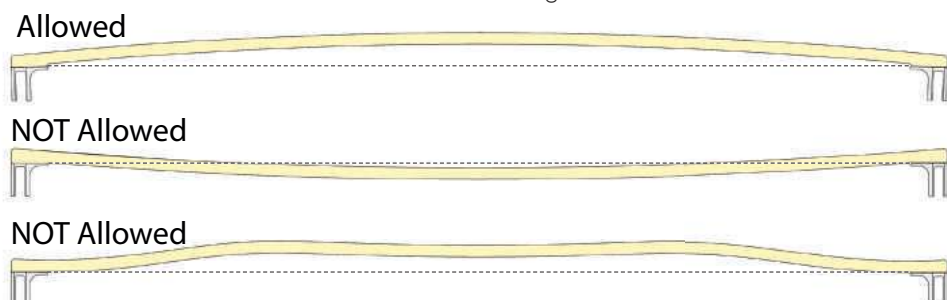


4.c Specifications B.3.a-f. Runner plank with exterior coatings and reinforcement where any exterior dimension is the minimum allowed (B.3.a-f) shall fail the minimum wood requirement

4.d Specifications B.3.c. Areas of plank over the chock: The same fore and aft dimensions apply in this location. Fairings of any type in this area are not allowed if they extend outside the maximum width of the runner plank unless they are part of the chock. Fairings may be considered part of the chock only if the height (Specifications I.5.b) and materials (Specification I.13) requirements for the chock are satisfied. Small residual lines of adhesive between the plank and chock are acceptable.



4.e Specification B.2.d, Crown: A 'fair continuous curve' may be one whose radius varies along its length. When not mounted on the fuselage and relaxed (under no load) the radius of the crown shall nowhere be concave, and no part of the curve shall be lower than the ends. The radius of curvature of the crown may vary along the length of the plank. Examples shown below. Localized buildups for chock mounting are excluded from the measurement.



## PROPOSAL TO AMEND SPECIFICATION D7 **BOOM**

Frederik Lonegren S8 submitted a proposal to amend the OFFICIAL SPECIFICATION D7 Boom to the ID-NIYRA European board. The European Secretaries and Board approved the proposal for ballot during the 2022 European Secretaries meeting. IDNIYRA and IDNIYRA European members in good standing will vote on the proposals by electronic ballot in the Spring of 2023.

### ORIGINAL TEXT

D. Boom

7. A 1/2" (12. mm) wide stripe shall be painted around the boom in contrasting color.

Stripe shall be perpendicular to sail tunnel. Forward edge of the stripe shall be 8' 10" (2692 mm) or less from forward inner surface of mast sail slot projected fairly.

### CHANGE WORDING TO

Delete D.7.

### REASONING

- We don't need it and it makes no sense to keep it.
- Will make the specifications clearer and simpler.
- We should measure the foot of sail instead.
- It's easier to measure the foot of sail.

## 3 PROPOSALS TO AMEND THE ESTABLISHED PROCEDURES OF ICEBOAT COMPETITION (EPIC AGREEMENT)

During the 2022 European Secretaries meeting, the board and Secretaries approved the following three proposals submitted by Attila Patakay M100 to amend the EPIC Agreement. IDNIYRA and IDNIYRA European members in good standing will vote on the proposals by electronic ballot in the Spring of 2023.

### PROPOSAL ONE: ARTICLE VI TECHNICAL COMMITTEE

#### ORIGINAL TEXT Article VI - TECHNICAL COMMITTEE

The DN Class Technical Committee (TC) shall consist of six members, three elected from North America and three elected from Europe. TC members shall be elected according to the respective Continental Governing Documents (CGD). One member will be elected every year, alternating between continents. North American members will be elected in even years and European members in odd years. The term of office is six years. Term of office will begin July 1, and expire June 30 of the appropriate year. Each year the TC shall elect from its membership a chairperson to serve until June 30 of the following year. All decisions of the TC require 2/3 majority vote of all members. Upon resignation prior to six years, a replacement member will be elected by the respective CGG to complete the unexpired term. The Chairperson shall report to the CGGs on all recommendations of the TC. The TC may initiate changes in the OFFICIAL SPECIFICATIONS or OFFICIAL PLANS by proposing a change to both CGGs.

#### PROPOSED CHANGES

##### Article VI - TECHNICAL COMMITTEE

The DN Class Technical Committee (TC) shall consist of six members, three elected from North America and three elected from Europe. TC members shall be elected according to the respective Continental Governing Documents (CGD). One member will be elected every year, alternating between continents. North American

Continued next page

members will be elected in even years and European members in odd years. The term of office is six years. Term of office will begin July 1, and expire June 30 of the appropriate year. Each **second year** the TC shall elect from its membership a chairperson to serve until June 30 of the **following appropriate** year. **The chairperson must be elected alternately from American and European members. If the chairperson resigns or cannot fill out his/her term, a new chairperson from the appropriate continent is to be elected by the TC to fill out the remainder of the term.** Upon resignation of a member prior to six years, a replacement member will be elected by the respective CGG to complete the unexpired term. All decisions of the TC require 2/3 majority vote of all members.

The Chairperson shall report to the CGGs on all recommendations of the TC. The TC may initiate changes in the OFFICIAL SPECIFICATIONS or OFFICIAL PLANS by proposing a change to both CGGs.

### REASONING

We believe that serving one year as chairperson is not enough to carry out long-term plans. At least a two-years term must be given to the chairperson.

The European Community would like to have more even representation of their interests in the TC, therefore requires the election of the TC chairperson alternately from the American and the European members. Without examining the reason why in the past there were only NA chairpersons elected in the TC, we would like to enshrine this rotating principle in the EPIC Agreement.

## PROPOSAL TWO: ARTICLE X VOTING

### ORIGINAL TEXT

#### Article X VOTING

The DN Class will submit a ballot to the membership, no later than Oct 1. All members having valid membership at Sept. 1 and a valid e-mail address on file will have the right to vote. The method of voting will be an electronic ballot or any other safe and reliable method. The voting will be closed 21 days after the submission. The chosen electronic voting system should be able to certify and to verify the voting results. DN Class members that are members of both the North American and European organizations may only cast one ballot.

Passage: To pass a proposal must get at least 2/3 yes votes. Any proposal which does not pass may not be resubmitted for one year.

Effective Date: Changes in the Official Specifications or Official Plans shall become effective May 31 unless both Governing Groups agree an earlier effective date, and that the effective date is on the ballot proposal.

### PROPOSED CHANGES

#### Article X VOTING

The DN Class will submit a ballot to the membership, no later than Oct 1. All members having valid membership at Sept. 1 and a valid e-mail address on file will have the right to vote. The method of voting will be an electronic ballot or any other safe and reliable method. The voting will be closed 21 days after the submission. The chosen electronic voting system should be able to certify and to verify the voting results. DN Class members that are members of both the North American and European organizations may only cast one ballot.

Passage: To pass a proposal must get at least 2/3 yes votes. **Only valid, casted votes must be counted. Abstentions count as votes that have not been cast and are therefore invalid.** Any proposal which does not pass may not be resubmitted for one year.



Effective Date: Changes in the Official Specifications or Official Plans shall become effective May 31 unless both Governing Groups agree an earlier effective date, and that the effective date is on the ballot proposal.

## REASONING

The previous wording was not clear and could have given rise to misunderstandings and controversy. The current wording is clearer and fully in line with Robert's Rules of Order.

## PROPOSAL THREE: ARTICLE XI ENFORCEMENT OF THE SPECIFICATIONS

### ORIGINAL TEXT

Article XI - ENFORCEMENT OF THE SPECIFICATIONS

The OFFICIAL SPECIFICATIONS shall be enforced in two ways;

1. Through a protest filed by any contestant, Judge or Race Committee member at the Gold Cup, European Championship or North American Championship Regattas against any competing yacht; or,
2. By the Race Committee measuring yachts during a regatta described in the CGDs, NIA rules, or the regatta Sailing Instructions.

### PROPOSED CHANGES

Article XI - ENFORCEMENT OF THE SPECIFICATIONS

Each and every sailor has the full responsibility to ensure that his/her iceboat is maintained to comply with her class rules and is in full compliance with the OFFICIAL SPECIFICATIONS.

The OFFICIAL SPECIFICATIONS shall be enforced in two ways;

1. Through a protest filed by any contestant, Judge or Race Committee member at the Gold Cup, European Championship or North American Championship Regattas against any competing yacht; or,
2. By the Race Committee measuring yachts during a regatta described in the CGDs, NIA rules, or the regatta Sailing Instructions.

## REASONING

There was no proper responsibility defined earlier in the Governing Documents.



Roland Huber OE250 Photo: Anna Pataki

# PROPOSAL TO AMEND THE IDNIYRA CONSTITUTION ARTICLE VI & ARTICLE XII

WARREN NETHERCOTE KC3786

IDNIYRA class members can comment on the proposed revisions at the 2023 Annual Governing Meeting, held during the January 2023 World & North American Championship. The proposal will be put to an electronic vote in the spring of 2023.

*"Any action requiring a majority of two-thirds (such as amendments to the Constitution or By-Laws, or changes in the Official Specifications or Racing Rules) shall be put to an electronic vote using an email address provided by the member."*

During the 2021/22 season a situation arose leading to desire to 'recall' a member of the DN Class' Technical Committee, to remove the incumbent from office. Neither the IDNIYRA nor the IDNIYRA EU had any such capability in their governing documents at the time. The IDNIYRA EU added such a provision to its governing documents for Technical Committee members at the 2022 National Secretaries Meeting and acted upon it. This proposal includes an equivalent measure for IDNIYRA and also clarifies the process for election of members of the Technical Committee.

Both the Officers of the IDNIYRA and the North American members of the Technical Committee are elected by the IDNIYRA membership so the proposal requires that a ballot of the membership is required for recall. The proposal allows any voting member to propose recall, but to avoid vexatious actions, proposals must be supported by either a 2/3rds majority of the Governing Committee or 10 percent of the IDNIYRA membership (typically 20 to 30 members) before being referred to electronic ballot. An electronic ballot of the membership would require a 2/3rds majority in favour of recall, to be consistent with the measure adopted by the IDNIYRA EU.

In 2021 we amended the IDNIYRA Constitution and By-laws to eliminate duplication with the EPIC and Regatta Management Agreements, but inadvertently eliminated the definition of election processes for members of the Technical Committee. This proposal reinstates those processes and additionally specifies electronic voting when an Annual Meeting is not held.

Make the following changes to the IDNIYRA Constitution:

## **ARTICLE VI – ELECTION AND DUTIES OF OFFICERS**

Change title to **"ELECTION, RECALL AND DUTIES OF OFFICERS"**

Add a new paragraph, as follows, after the first paragraph of ARTICLE VI:

The members may recall (remove from office) an Officer by means of an electronic ballot with a two-thirds majority required for recall. Proposals for recall shall be submitted in writing to the Governing Committee and either a motion supported by two-thirds of the Governing Committee or written support of 10 percent of the voting membership of IDNIYRA is required for the proposal to be submitted to the membership for an electronic ballot. If approved by the membership, recall is effective immediately upon publication of ballot results.

**ARTICLE XII – TECHNICAL COMMITTEE**

Change title to **“ELECTION AND RECALL OF MEMBERS OF THE TECHNICAL COMMITTEE”**

Delete “(See EPIC Agreement Article VI)” and replace by:

Article VI – TECHNICAL COMMITTEE of the EPIC AGREEMENT defines the structure, responsibilities and authority of the Technical Committee

North American members of the Technical Committee shall be nominated and elected at the Annual Meeting for a term of six years. Should an Annual Meeting not be held, the election of a Technical Committee member may be appended to the annual electronic election of IDNIYRA Officers. A North American member or members of the Technical Committee may be recalled (removed from office) using the recall processes for Officers in ARTICLE VI of the IDNIYRA Constitution.



Photo: Oscar Lindell



dn-parts.com

Designed by Zakrzewski P-55



Continued on page 20

# 1988

DEFENDING A DN WORLD CHAMPIONSHIP TITLE  
DURING THE LAST DAYS OF THE SOVIETS



Mike O'Brien waits during the last seconds before the start of the final race of the 1988 DN World Championship in Leningrad (St. Petersburg), Russia.  
Photo: I. Federov

# WORLDS

BY MIKE O'BRIEN US3456



**T**raveling from Finland to the USSR wasn't like traveling from Illinois to Wisconsin. I was required to disclose all currencies and precious metals. As defending champion, I carried the DN Gold Cup World Championship into Russia, and the border guards were interested. "We must see this Gold Cup." They realized the trophy was gold in name only, and I heard them read off the Estonian winners.

## **Day One: March 21, 1988**

The Leningrad facilities were undoubtedly more than the athletes had anticipated, and we checked into predetermined rooms broken into areas based upon nationality.

The first day began gray and humid. A solid 3" of dense, unsailable snow and 8" drifts covered the ice. Typically, there would have been four or five alternative lakes for a world championship regatta, but the Russians weren't flexible. There was ice in nearby Estonia, but it was impossible to move the regatta. The money spent on our facilities was enough to keep us in Leningrad for the next two and a half weeks, regardless of ice conditions. Our visas further complicated the problem because they only allowed traveling up to 60 km from Leningrad. Short of an act by Mr. Gorbachev, we were not able to cross that invisible line.

We spent the first day working on our equipment. "Making do with what you have" was well known in Russia. There was no corner hardware store to buy odds and ends. If you did not bring it or the well-prepared Germans or Dutch did not have it, you would have to do without it.

## **Day 3**

Today was to be the beginning of the Russian championships. Overnight rain reduced the snow to 3" thick, slushy drifts. To our amazement, sailing took place but no racing because rain caused poor visibility, making it impossible to determine speed.

## **Day 4**

Light rain postponed racing for several hours, giving me time to align my new angle runners and slot runners. After repairing my halyard, I was ready to sail. To my amazement, they were racing in the light rain and 3" of watery slush on the ice. Even though I wore plastic bags over my socks in my track shoes, I knew I was in trouble upon seeing the seasoned Estonians wearing boots with spikes. My front runner was throwing slush into my face like a fire

Continued next page



*"The Russian Race Committee measured sails as if they were sailmakers." Photo from DN Europe 88/89 Newsletter*

hose dousing a fire. My insert runners with bulky stiffeners created much drag and made it difficult to sail. I was in trouble. My feet and gloves were soaked, and I couldn't see through my goggles. The mast I had built was too stiff and runners non compatible for such conditions.

After 2 hours on the ice, it was best to use my time working on equipment that night in the provided workshop. I sanded the mast to remove the carbon fiber to make it more flexible. I had brought a sanding block and sandpaper but no dust mask, which was not available in Russia. Many spectators watched as I began my project by fashioning a dust mask out of an old rag. The dust started to fly, and curiosity grew. In no time, there was a good mess of black carbon covering my clothes and work area. I figured the Russians were interested in this black fiber on my mast, so I swept up the carbon fiber and put it in a tin can. The can was gone the next morning, and they liked that black fiber.

Sports were big in Russia, and all national TV networks featured iceboating every night. Iceboating had prestige above Gorbachev.

We were honored with IDNIYRA European Commodore Wim van Acker's presence and long-winded adventures. Since we all recently crossed the border, it was an excellent topic for Wim. He talked about going into East Germany, where the guard asked for his occupation. Wim replied, "I'm a welder," which puzzled the guard who asked, "You have this beautiful truck, a large trailer filled with equipment, and you are a welder?" Wim replied yes and pointed to his passenger, saying, "you should see what he

has. He's a chief welder!"

Another time, Wim had too much equipment and wanted to prevent the border guards from checking their semi-open trailer. He covered the tarp with grease, which kept the border guards from inspecting because they didn't want to get dirty.

We talked about new places to go ice boating. There was talk of a trip to Alaska next fall and discussion of the fourth annual Santa Claus regatta in northern Finland, just within the Arctic circle.

### **Day 5**

The dismal weather continued with fog and slightly above freezing temperatures and would last the entire regatta. Plastic bags became a high-demand commodity. I had to use three bags to keep my feet dry.

Slush was a major concern, and the Polish competitors seemed well seasoned in this condition. They fabricated plywood splash guards for their boat bows. I made a maximum stiffener for my front runner to perform the same function using the foam and duct tape packing materials I used for my boat's journey.

After pushing out of the launch through 6" of water and slush, I could sail by the wind alone. Water flew on my face mask and down my suit from the stiffener of my side-angle-iron runners. I would probably have pneumonia if not for my Henry Lloyd one-piece summer sailing suit.



Opening Ceremonies. Photo: Mike O'Brien

I avoided the spray, sliding slightly forward but couldn't sail very well. I changed to plate runners to alleviate the water problem but at a sacrificed speed. I sailed a qualifier and a Silver fleet race and finished in the high teens both times.

Polish sailors dominated, and Stan Macur P111 had strong finishes giving him an easy victory. Piotr Burczynski P154 followed along with a host of other Polish team members. Germany's Andreas Bock G624 made a strong appearance in one race and elected to spend the day in Leningrad instead of sailing through the slush all day.

I had my share of problems just getting around the course without pushing, but it was good practice starting with the European reindeer track stars and sailing the Figure Eight course.

The Figure Eight course requires you to round the leeward mark to starboard, which changes traffic flow. Instead of sailing a diamond, the boats follow two legs of a triangle causing many head-on situations between upwind and downwind boats. The

Figure Eight course is exciting but results in hairy situations.

The Russian Race Committee (RC) thoroughly measured boats and components for the World Championship. Sails were measured as if they were sail makers. The runner cut-track, masts, and booms were measured. My two sets of plate runners passed scrutiny, but my angle-runners and slot-runners became the center of controversy with the Russian measurers. The general opinion of other contestants, the Technical Committee, and team coaches was that my insert runner stiffeners were legal. They had a surface 1 1/2" off the ice, and the stiffener was all within the cut track. I tried explaining this to the Russian measurers, but suddenly, two days before the Worlds, my best runners had been sidelined.

The opening ceremonies began during this time, and I missed the honor of raising the American flag to open the regattas. I could walk in the country parade, but my feelings were very uneasy.

I knew angles would work well in softer ice conditions; the slush was down to less than 3" and decreasing quickly. I could not be without the angles.

I gathered some hand tools and headed to the workshop with the angles. It had taken me 16 hours to fabricate the wood and carbon fiber stiffeners for these runners. With the limited tools available, it seemed I would need another 16 hours to remove them. A group of Russians again gathered around, and they knew exactly what I was doing. They appeared with hand tools to make the task easier.

One Russian began working with me; he was a big help and instant friend. At times he would disappear with one of the runners only to reappear with the work nearly completed. I had a pair of vice grips to remove the bolts that fastened the stiffeners to the

Continued next page



Opening Ceremonies. Photo: Bernd Zieger G107



3 Mikes in Leningrad. From left, Americans Mike Boston US9, Mike O'Brien, and Mike Rian US467.

runners. The Russian knew one English word, which was "good." That was enough to know he approved of the vice grips.

My mast needed more softening, but the runner modifications left no time to work on the mast. I decided to sail with my Kenyon mast in the European Cup races.

That evening I approached Harold Stuertz G7, a German Technical Committee member, for advice about the insert runner situation. Harold went to Endel Vooremaa of Estonia and asked that he help me talk to the measurers about the runners.

### Day 6: European Cup Day 1

That morning Endel Vooremaa and I met with the stubborn Russian measurers. After an hour of discussions with themselves and with Endel reading the rules, the measurers agreed to allow me to use the runners for the World Championship, provided they could find no further evidence that proved they were right. Their decision would not make sleeping any easier that night.

The European Cup served to divide the 154 boats into Gold (A), Silver (B), and Bronze (C) fleets for the World Championship. Late morning, the ice was relatively hard, with 1 ½" deep slush in the worst places; the water had been absorbed into the deteriorating granular ice.

The first A fleet race was fast. I started in position eight with Andreas Bock and Vaiko Vooremaa C6 (Endel's son) directly to leeward. Andreas was like a deer at the start of this race. Andreas' good start did not affect my clear air while Vaiko tacked away, never to be seen again.

I had freedom and speed, rounding tenth at the top mark and eighth at leeward. Piotr Burczynski P154 had a commanding lead, and by the next leeward mark, I was fifth and closing on Piotr's lead. I passed Andreas, Stan Macur, and another Polish boat to finish a respectable second.

By the time of the second A fleet race, the ice had softened, and I lost all speed. I again awakened to the fact I had work to do to be competitive. My angle runners, set up for harder ice conditions, were digging into the soft ice and not riding up onto it. It would be a long evening correcting the problem.

The wind dropped during the third A fleet race, and instead of waiting for more wind, I opted to push back to the harbor and get a jump on the work at hand. Upon reaching the harbor, the media was waiting to interview me for the first time. An Estonian radio interviewer asked about conditions and how the day's sailing went. I kept my replies positive and stressed that I was glad to have had the opportunity to practice and learn what work was necessary to improve my performance.

At 4 PM, I began working, pleased to have the workshop to myself and that Germans were around to borrow the needed power tools. The lead-in on the runners needed refining; a gradual entry was necessary to allow the runner ability to climb up onto the slushy soft ice. My runner lead-ins were blunt and abrupt to maximize the runner length on the ice. This setup was suitable for the conditions on Barnegat Bay in New Jersey, but we never raced when this ice was as soft as it was here. Ignorance is no excuse, and I believe that Americans demand too good of conditions to consider racing, something to be said about our type of iceboating.

The angle runners were mainly wood with thin steel covering the contact edge. With a grinder and drill, I removed the wood from under the steel so I could change the profile. I then cut a slit into each angle side, allowing the steel to bend and conform to the new wood profile. I was fortunate to get a Gougeon [West System] epoxy kit from an American and a heat gun from a German. By the time I was ready to reattach the steel blades to the wood, I had a big audience. The Russians were awed by the Gougeon epoxy kit, so essential to me yet so unobtainable to them. I mixed and applied the epoxy and cured the improved runner with a heat gun in less than 30 minutes, amazing the Russian bystanders.

I filed the steel to fare in the hard spots created by the cuts that facilitated bending. By now, it was 7 PM, and the runners





*Race Committee. Photo from DN Europe 88/89 Newsletter*

had to be aligned because this work could have changed the alignment. Harold Stuertz was aligning his runners and helped me do the same. After dinner, I filed and polished the work area with wet sandpaper. Though it was necessary work, I could do it while drinking a beer and talking about ice boating in our TV room.

My first encounter with our interpreter was less than eight hours after our arrival. I did not trust him at first, but he was very helpful. Vadim could not understand why I arrived alone without the other members of the American team. My answer was simple, "Because ." It was hard for Vadim to grasp that Americans were individuals used to doing as we saw fit. I came before the other Americans because I was there to be best prepared to compete, and the others were there mainly for the experience.

### **Day 7**

There was less than seven mph of wind and no possibility to race. The RC kept us at the site all day. The cancellation of the European Cup caused a very sticky situation because it was impossible to seed the World Championship fleet. It was necessary to split the fleet into five equal divisions: red, blue, green, yellow, and white. Top sailors were divided into each fleet based on past performance. The remaining 154 positions were more difficult to decide and were based on the results of the Russian Championships, and 100 Russian DNs were eliminated from further competition. The random divisions were well picked, and competition in all ten split fleet races was evenly matched.

### **Day 8**

#### **World Championship Day 1**

Racing began at 11 AM because daylight savings meant it was possible to race until 8 PM. The RC did an excellent job running ten quality split fleet races for a regatta with this many competitors. We arrived back at the club at 6:30 PM after a hard day of sailing.

The RC maximized our time by implementing a special rule. Boats had ten minutes to finish after the lead boat to receive a score. Boats not finishing within ten minutes of the lead boat received a DNF, which made the C fleet results look strange with mostly DNFs.

One hundred fifty-four boats in five fleets had an advantage using the split-fleet system. With fewer boats in each fleet, no group had to sail against the next. Therefore, we didn't have to wait 15 minutes between races to give time for the qualifiers to prepare for the next race. As one group finished, the next group was on the line, ready to go with no wasted time.

We raced on the Gulf of Finland which was covered in over two feet of ice. Over fifty people staffed the RC; they always had things ready to go when we sailed to the course. The RC was more extensive and efficient than any I had seen anywhere else in the world. Russia's "full employment" policy paid the RC to run the races. The RC consisted mainly of Estonians who were well rehearsed in the task.

Like in North America, the starting line was behind the leeward mark. A special rule required competitors to clear the windward side of the leeward mark before tacking, eliminating the running and tacking to the favored side.

The RC positioned the starting blocks closer to each other than in North America. Europeans line up their DNs very close to straight into the wind. The lighter the wind and slower the ice condition, the longer they run. At the start, Europeans run like a deer for a long distance. A couple of times, I felt like I was going to pass out when I finally jumped into my boat.

The RC's base camp was at the leeward mark, and there were another twenty RC stationed just above the windward mark, plus assorted spectators, ice boats, and ice board sailors. The big crowd of people made it easy to spot the weather mark.

The sun shone for only two days during the regatta. The atmosphere was hazy because of water evaporation off the ice. Clear goggles and glasses were crucial for visibility. The RC raised a colored flag when it was time for your group to line up for a start. The RC held a skipper's meeting before each race, where they reported the distance between marks, time limit per lap, and ice hazards.

The RC starting committee stood just below the leeward mark and blew a whistle thirty seconds before raising the starting flag. The head judge would ask the unforgettable words, "right side -OK, left side-OK." A computer system from the west helped the RC tabulate the race results. Race results were printed and displayed every night by 6 PM.

I drew number twenty-seven for my start position. I told inquisitive reporters that my game plan was to get into clear air at the tack over to port one

quarter the way up the leg and then come to the windward mark from the port lay line.

I got the start as planned, and only Tiit Hagma C10 was able to equal my speed and pointing ability. Tiit tacked onto starboard, and I did the same 100 yards in front of him. We crossed over to the port side and tacked in line with those on the lay line. I rounded first with Tiit three hundred yards behind and Andreas Bock behind Tiit. I could sail the best possible course downwind, although my main concern was keeping the boat "hot" (always on the verge of hiking with good speed.) The top three positions remained the same until the last leeward leg when Andreas overtook Tiit to earn second place. The ice was harder on the port side of the course, which superseded any difference in wind conditions.

### Race 2 (Split Fleet)

The wind decreased slightly, and the ice remained relatively hard. Piotr Burczynski had a good start and a strong lead around the course. At one point, he had a three-minute lead over the second-place finisher Dariusz Kardas P13. Ain Vilde C34 (winner of the first-ever World Championship) captured third, followed closely by Rene Kulmann C22.

### Race 3 (Split Fleet)

The ice was softening, and the winds were nine mph. Andreas Bock took a commanding lead, which he held for two of the three laps but then sailed through some soft ice, costing him the lead. Tarmo Tuisk C18 finished first, Wladislaw Stenfanowic P69 second and Andreas Bock third.

### Race 4 (Split Fleet)

Wind and ice conditions remained the same as in race three. I lined up in the twenty-seventh position and followed the same game plan as the first race. Short of tacking on the lay line Roland Berdash C64



Photo: Mike O'Brien

crossed with a good lead and speed. I rounded one hundred yards behind Roland and a good distance ahead of the third-place boat. I closed the distance to one hundred fifty feet, separating me from Roland, and he kept his lead until we rounded the top mark. I saw Roland look over his shoulder twice as we approached the lay line for the leeward mark. Just after he looked back, I jibed, which gave me the advantage at the leeward mark. My speed was better, allowing me to sail lower and have about one hundred yards on Roland at the leeward mark. I held on to the lead, Roland finished second, and Wieslav Marcinczyk P110 third.

### **Race 5 (Split Fleet)**

We sailed the fifth race on similar ice as the fourth, but the wind had begun to freshen as high as twelve mph. Piotr found problems in the stronger breeze and could not get to the front of the pack. Mati Kuulmann C2, though light, took a close lead over Tarmo Tuisk and had a commanding lead over the rest of the fleet. Mati held his lead from start to finish and was shaping up as a major factor in this World Championship. Tarmo finished second, Andrey Dalecki P180 third, and Piotr fourth.

### **Race 6 (Split Fleet)**

In the sixth race, winds had subsided to around ten mph. Andreas Bock took an early lead and went on to an easy victory. Roland Berdash had a tough race, fighting from a deep position to fourth. Darisusz Kardas finished second and Wieslav Marcinczyk third.

### **Race 7 (Split Fleet)**

In race seven, the winds had leveled off to around ten mph. I drew the number one starting position while Piotr and Mati were both starting on starboard tack. I wanted to be sailing against those two as soon as possible. I tacked onto starboard as soon as I could clear the leeward mark. Vaiko Vooremaa (who started to leeward of me) had the same game plan. Vaiko had run longer and crossed slightly ahead on his starboard tack. I tacked soon after Vaiko cleared me. I had to get out and push the boat up to speed. Mati crossed me about 90% of the way to the lay line, and he was indeed a factor in the regatta.

I carried a little farther, then tacked. We rounded the mark a good distance ahead of Stan Macur. I jibed to fetch the leeward mark. When we rounded the leeward mark, I had taken away about one-quarter of Rene's lead. He was now only three hundred feet in front of me. I expected to close distance with in line speed, but that was not the case because Rene was light, and I only made ground by pointing inside him. Once again, he tacked inside of me. I carried a little longer though short of the lay line. We converged near the mark, and I tacked

short of Rene's course (Rene was on starboard.) Rene had a little momentum on me. I rounded first, but Rene rolled me. Again he was in the lead and had to decide when to jibe for the leeward mark. I was keeping him on his toes by sailing in his blind spot. I jibed away without him knowing. We both had gone too far and were overstanding the leeward mark. This gave me an advantage, we rounded, and Rene was about one hundred feet behind and could not point higher; thus, he had to settle for a slight amount of bad air. Rene tacked early for the mark, and I tacked to cover. I had two hundred fifty feet on Rene by the top mark. Rene could not gain any distance off the wind. Stan Macur finished a distant third, and Piotr a disappointing sixth.

### **Race 8 (Split Fleet)**

Race 8 was strange in that it took place about 3:30 PM as the temperature dropped, and the winds built to as high as fourteen mph. The Polish team demonstrated that they have a lot of good sailors, not just one or two super good people. This race was close for four boats, Andrey Dalecki, Wieslav Marcinczyk, Roland Berdash, and Ain Vilde.

The wind shifted during the first downwind leg. Andrey Dalecki, who had jibed for the lay line the earliest, did the least amount of reaching to the mark and kept his lead throughout the race. Team member Wieslav finished second, Roland third, and Ain Vilde fourth.

### **Race 9 (Split Fleet)**

The wind had peaked and was settling down to about twelve mph. The ice had softened in critical locations, especially the windward mark, because there had been so many races.

The top sailors found that the best and longest way to stay on the better ice was by keeping close to the windward mark on port and tacking quickly onto starboard.

Piotr showed that he still had it in him and had a good lead from start to finish. Mati Kuulmann hung on to second, and Tiit was third. Tiit proved he was a factor when the wind was just a little stronger. Andreas, being light, was happy to hang onto fourth position.

### **Race 10 (Split Fleet)**

The day's final race did not get underway until after 5 PM, with winds at twelve mph at the start down to eleven mph for the last lap. I started in left-field in position of fifty-four. Vaiko Vooremaa was the only aggressive starter near me. I could start more like what I was used to in North America, where we ran a shorter distance and sailed the boat sooner. Vaiko did a better start but soon tacked away, never to be seen again. I had plenty of clean air but couldn't

Continued next page

tack to port because Tarmo Tuisk and Stan Macur sailed out to the lay line. I was overstood and unable to take advantage of my pointing ability. I ducked a starboard tacker coming into the mark to round in fifth place. I picked up two boats downwind to round close behind Tarmo.

I tacked first of the three boats when close to the upwind lay line. Stan rolled me with speed but no pointing ability. I was now in second behind Stan around the mark. Downwind I closed the gap, and upwind, I got by him by tacking sooner: this time, he could not reach over me. He was stuck in my bad air. I could keep ahead downwind to a comfortable win—Tarmo Tuisk in third and Andrey Dalecki in fourth.

Between races, I made myself scarce to the media because I had already been interviewed once that day and, more importantly, needed to keep my intensity for the racing. The brake on my front runner didn't function well, so I dropped the rig between races to prevent the boat from sailing away. My sail on the ice helped to keep reporters from finding my boat, while other competitors wondered if something was wrong with my equipment. Still, others, who thought my sail was the key to my success, were frustrated when they could not get a look at it. Dropping the rig worked as a psychological advantage.

A photographer snapped a picture of my boat after my second race but didn't attract the attention of the masses. My Henry Lloyd suit was a good disguise and helped me blend in quite well. After the third race, I walked around for thirty minutes, searching for my sail, runners, and lunch. I began to suspect foul play when someone reminded me that the RC had moved the starting area to the right. My equipment was right where I had left it.

The Germans invited me to dinner in Leningrad that evening. I became very familiar with the back seat of Christoph Schmidt's van. The roads were terrible, and Christoph's custom back seat went air born and crashed to the floor after hitting a pothole. On another trek into Leningrad, Christoph's van had a flat tire, and the price for the repair was a metric socket set; rubles were not desired.

## RACE 5

### The first race of the combined Gold Fleet

Before the fifth race, I took some practice laps and found the ice harder than the last three days. I stood the rig higher to allow for tighter trim and more mast bend. Soft ice sections remained on the course. My angle runners were not sliding on the hard ice; thus, there was no reason to change to insert type runners. The angles would go better in the soft ice areas.

Because I had done well the day before in the split fleet races, I was in the first starting block. Andreas Bock was in three, and Andreas Dalecki was in five – both were quick runners who could push a long distance. The RC signaled the start, and I ran until I thought Andreas got in his boat. I jumped in and noticed they were still running. Now I had to get back out and run again. At this point, I was directly behind Andreas, and my only option was to tack away. Thirty seconds after my tack, Piotr just crossed ahead of me. I was 80% to the lay line before I tacked. A good number of boats crossed and tacked above or ahead of me. My speed was good, but there was too much traffic to make any gains. Harold Stuertz rounded first; he started on the far-right side of the course. Piotr rounded about fifth but could pick off some of the slower boats. I rounded about eighteenth and elected to sail high above a group of boats. It was difficult to make gains downwind because if you got too high, you would be off in soft, slow ice. There were boats to leeward, which made jibing early impossible. I caught three or four boats downwind to round around fifteenth. Wladyislaw Stefanowic was first, Harold Stuertz second, Andreas Bock third, and Piotr Burczynski fourth. I had an excellent upwind leg that put me into tenth when I rounded the weather mark. Staying high before jibing helped set me up to pick up two more boats downwind. Piotr was now in solid third behind Andreas and Wladislaw. I went deep left and sailed through soft ice, while two Poles and three Estonians went to the other side of the course on good hard ice. I could not catch any boats downwind and ended up a disappointing thirteenth. Andreas Bock lost a few boats in the same situation to end up fifth.

The B fleet started a race, but the wind lightened, and they didn't make the lap time limit. After the A fleet race, I tuned by boat, sailing off to the course side, but could not come back down because of the soft ice in that area. The Russian that helped me work on runners came up by the top marked and violently waved to me. I understood that meant the A fleet was lined up and ready to go.

I had to sail across the course to get to the start area, where all other competitors were lined up, ready to go. Our team interpreter signaled my starting position. I thought to myself, oh great lucky thirteen. It seemed like they were waiting for me before they would start. I gave my goggles to Bob Cummins US3433 because there was a potential for running with less wind. My goggles had been fogging up, and I could live with a few ice chips in my face if my glasses protected my eyes.

Rene "deer" Kuulmann started just to leeward of me. I was determined to run as fast as I could and as far as Rene. We ran together for a long time,

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twice the distance of the first race. We ran to the beginning of some good ice, a big plus. I felt like I was going to pass out once in the boat. I was very pleased when I looked back at the boats that had started above us because they had jumped in their boats while still on soft ice. Rene and I sailed in tandem. We carried out to the lay line on port tack. Rene tacked to fetch the mark while I carried farther to ensure power at the mark. We were sailing on soft ice the last five hundred feet to the mark and lost some ground. Rene rounded third, and I was fifth. Three boats from the left side sailed a longer distance on the harder ice and tacked below us on the lay line. They were able to point higher and make the mark. Rene jibed early for the leeward mark while I continued a good distance more. Rene and I were fourth and fifth at the leeward mark.

We sailed about one-quarter of the way to the left, tacked, and were on the lay line. This time we sailed the long distance on the good ice. Our positions hadn't changed, and it was time for another downwind leg. Rene jibed away early, never to be seen again. I jibed on the lay line and was well overstood. I reached up and gave away a lot of distance. By the time I was near the leeward mark, I had wished I would have sailed as low as possible. Instead, I made a slow jibe toward the mark, coming in low and slow in about eighth. I had to get out and push before rounding. My rounding was poor, and Wladislaw Stefanowicz came in with speed. As we were rounding up to our next windward course, Wladislaw came up underneath and clipped my runner. We exchanged verbal greetings and were on our way. The next time I would see Wladislaw would be in the protest room.

I had an excellent windward leg and rounded the weather mark in fifth. I was on good ice, similar to the path I took on the last weather leg. I had speed and a low path on my first jibe. I saw Tarmo Tuisk pushing on the opposite jibe. He must have had less air when coming around the weather mark and got high into the bad ice. Mati Kuulmann and Heino Iyesaar C25 were to leeward and ahead. They jibed early, as did Ain Vilde, who was close behind. I jibed one hundred yards later. It looked like I gave up all that distance towards the mark. As with the last leeward leg, I seemed low this time. I kept the boat going fast but low as it would go. I looked up at Ain Vilde, Mati Kuulmann, and Heino Iyesaar. They were on the slow ice and closer to me. They jibed away, and the closest to me, Mati Kuulmann, crossed 20 feet behind me. It took me two days to remember this race, and it's now very memorable.

Mati Kuulmann, a previous world champion, finished a close second. Ain Vilde, another previous world champion was third; Heino Iyesaar and Wieslaw Marcinczyk were fourth and fifth.

I was greeted by the press when coming into the starting area. I was still a long way from being a world champion. There was a possibility of another race, and I had to deal with a protest hearing.

I went to the RC to get a protest form and denied all interviews until the racing was complete. Andreas Bock drove his VW microbus out to the starting area, and it was an excellent place for me to hide and fill out the form. Reporters soon surrounded me to record the moment.

Continued next page

Once the mass of reporters left, the teenagers approached me for autographs. Ten boys individually came with the same pad of paper and pen. Thank God for their friend who supplied the paper.

I got out of the van to find out what was going on. The C fleet was lined up, but there seemed not to be enough wind to race. The reporters noted that I was on the move, and it was picture time. The Moscow station wanted an interview which I declined until the races were over for the day. There was a chance of another race, and I had to stay alert and ready to race.

People began asking me to tune their boats. I explained that I knew what worked for me, but differences in weight, mast bend, and sail shape would make it difficult to improve their performance. A cannon was signaled at 5:15 PM to signify the end of the World Championship races. With the yacht harbor two miles away, some competitors drove their support vehicles out to help tow boats to shore. I sailed halfway to the dock and stopped on the soft ice. The Finnish team stopped and let me tie on behind six DNs in tow. The Estonian team went by with twenty boats and the Germans with ten. What a sight! It looked like a typical setting at a soft water regatta.

Back at the landing, the RC scheduled the protest for 6 PM, leaving just enough time to unrig the boat. I arrived in the protest room after the first international juror, who was Finnish. The protest was to be held in English, a big plus for me. Wladyislaw Stefanowic arrived with Stan Macur as his interpreter. Stan is a longtime friend of mine. The other jurors were Doug Harvey USA, Endel Vooremaa Estonia, and a German. A chairman was elected because there was an even number of jurors. Thus, the jury could decide without a stalemate. The judges told us the facts they



The author signing autographs. "Never in the U.S. would anyone ask for it."



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had found. P69 came on the outside of US3456 with four times as much speed. P69 infringed on US3456 under Rule Five, "A faster moving yacht should stay clear of yacht in its rounding maneuver." The protest was over, and the press conference began. An interpreter sat with me as I took questions from the floor, mostly from newspaper and yachting publications. They asked Andreas Bock a similar battery of questions. Meanwhile, a camera crew set up their gear in the back of the room.

During the interview, I was congratulated and presented with the gold medal I would later receive at the awards banquet. They asked questions about my job, and it was hard for them to grasp the function of a salesman. They asked where I was from, how long I had been iceboating etc. They liked that I was from Neptune (New Jersey), "King of Seas," then asked about other sports interests. I talked about summer sailing and the Tornado class. They asked if I would be in Seoul, Korea, for the upcoming Olympics. I explained that I last sailed a Tornado in 1984. I told them about bike riding, and they asked if I would be in Los Angeles for some major bike race. I explained I was a recreation biker, not a professional.

Sports appeared nightly on the Soviet TV network, and I was more important than world news. The following interview was for an Estonian radio reporter. This interview was the most interesting. Estonia was a free country from 1919 to 1940; they didn't like being considered Russian and wanted to be a free nation someday. I felt he was using this interview as a kind of statement to tell his audience about the free world.

His questions began about iceboating and became more specific. "Do you own a house or rent an apartment? Do you own a car? How much money do you make?" Up to the last question, I was very open. My reply, in this case, was evasive, "I make enough to live on a subsistence wage." His questions become more related to culture. "What do you think of Russian girls?" I replied that I had seen many beautiful Russian girls. "What country has the most beautiful girls in the world?" I said, "I have seen beautiful girls from the US, Canada, Sweden, Finland, Germany, and others. I think every country has beautiful girls."

After showering, I went to the dining room. Most people had left. My German friends decided to stay and brought out some German champagne. The Russian waitresses brought me all the food I could eat. The dinner was delicious that evening, or the circumstances made it seem so. Two bottles of champagne went very fast. After dinner, I was content to sit in our TV lounge and drink a few beers, talking about Russia and ice

boating. Tomas Bergner, a German who brought my boat to Russia, came looking for me. He had been with a few Leningrad iceboaters, and they insisted I drink a toast with them. Harold Stuertz warned me to decline because he thought they would give me grain alcohol which could have terrible consequences.

I thought they would be insulted if I declined and agreed to go to them for only one toast. Besides, I had accomplished my goal of winning the Worlds. Thomas and I went to the sailing compound and met with several other Germans. At the yacht center, we met the Russians on the first floor, which seemed to be the working foreman's office. My Russian friend, who helped me work on my runners, was there with four new faces. They were so happy that I joined them. A bottle of vodka came, two shot glasses, and a can of fish (like tuna but tastier.)



Soon the phone rang, and the Russian who answered it had a surprised look. After a discussion among the Russians, they motioned us to follow them upstairs to what turned out to be "the boss's office." The room was amazing, the same if not fancier than a corporate head office filled with beautiful wood cabinetry, an elegant hardwood table, and a color TV with a clear picture, things we hadn't seen before in Russia. American music was playing on the stereo system. We were introduced to the boss, and he brought out a case of Czechoslovakian beer. The Germans said this was the best pilsner

Continued next page

beer in the world. For the first time, I heard the expression "There are some Russians more equal than others" from the Germans. I understood for the first time how the system functions in Russia.



Novosibirsk DN fleet. Photo Mike O'Brien

We autographed their club burgees and asked for their signatures on ours. Then we posed for a German who took our photograph beneath Lenin's picture.

The following day there was no wind, and I had coffee in Andreas Bock's camper. Christoph Schmidt, Gutta Johansen, and Gutta's video assistant came to the van. They invited us to interview with the ice boaters from Novosibirsk, six guys who drove four days straight through on Russian roads to participate in the regatta. There is a fleet of one hundred DNs in Novosibirsk. They pooled their finances to send sailors to the Worlds. Novosibirsk, Siberia, has a sailing season from September through May. Not

bad practice! However, January and February are too cold and snowy to sail. Their faces told the story of their existence. Five of them had chipped in to buy a birthday present for the sixth person. He was so moved; this must have been quite a big deal. They treated us to a toast with some of their best bourbon and invited us to a cookout that evening.

We returned to Andreas' vehicle, and I was approached by the junior iceboating fleet, who sailed a small boat with an Optimist sail on it. There were about twelve of them who wanted me to sign their helmets. Someone from Riga, Latvia, gave me a book on his country in exchange for an autograph. I was given many burgees and pins in exchange for my autograph.

I gave an interview I had promised to Moscow television, like our UPI Associated Press. The one question that stuck out in my mind was his last. "Would you donate your iceboat to a museum if asked?" My answer was, "I would be honored to if asked." The RC kept us on the ice all day, but there was no sailing. I gave out autographs during much of the day and was approached by many Estonians to sell my sail. When I told the Estonians that Christoph Schmidt had bought it the first day, they offered him runners and German marks for the sail.

### Trophy Ceremony

I had lost track of time and quickly dressed for the awards. No sooner did I sit down than the master of ceremonies called me to the podium to receive the Gold Cup. The head of Kirov Corporation awarded me the gold medal, and three costumed Russian girls each brought me an award. The first was a leaded glass vase, the second three carnations, and the third a burgee. Then with full hands, the Gold Cup.



Trophy ceremony. From left, Mike O'Brien 1st, Piotr Burczyński 2nd, Andreas Bock 3rd.





Silver Fleet

1988 DN World Championship results  
Gold Fleet

Pos	Sailnbr	Name	1	2	3	4	5	6	Total
1	US 3456	Mike O'Brien	1	1	1	1	(13)	1	0
2	P 154	Piotr Burczynski	1	4	6	1	2	(22)	11,6
3	G 624	Andreas Bock	2	3	1	4	5	(13)	13,5
4	P 110	Wieslaw Marcinczyk	(A)	3	3	2	4	5	16,4
5	P 111	Stanislaw Macur	4	5	3	2	3	(29)	16,4
6	P 69	Wladislaw Stefanowicz	5	2	6	5	1	(DNF)	17,6
7	SR 64	Roland Berdash	5	2	4	3	7	(7)	20,5
8	SR 22	Rene Koulimann	4	1	2	2	15	(19)	22,2
9	P 180	Andrzej Dalecki	6	3	1	4	10	(11)	22,9
10	P 13	Dariusz Kardas	2	4	2	7	9	(12)	23,5
11	SR 18	Tarmo Tuisk	1	2	10	3	9	(12)	23,6
12	SR 2	Matti Koulimann	6	10	8	6	(12)	2	31,8
13	SR 34	Asin Vilde	3	8	4	14	(21)	3	31,8
14	SR 10	Taru Haagma	3	4	11	3	16	(17)	36,8
15	P 75	Roman Swiderski	9	5	5	13	(30)	8	40
16	SR 25	Hileno Iyosari	11	7	10	10	(44)	4	42
17	G 425	Rolf Rathcke	11	5	5	11	(45)	14	46
18	P 11	Janusz Pietrzak	17	6	8	9	(23)	6	46
19	Z 24	Adam Bernard	13	7	7	4	20	(30)	51
20	SR 4	Raul Kyiv	8	11	14	5	19	(36)	57
21	SR 27	Aare Keep	9	7	12	6	(42)	23	57
22	SR 31	Visvaldis Briedis	17	8	14	7	14	(DNF)	60
23	P 78	Jaszczur Nowicki	6	10	7	26	(48)	15	64
24	SR 63	Gunar Rozenberg	13	13	15	15	8	(36)	64
25	SR 21	Helmut Leppik	7	10	5	19	(34)	28	69
26	SR 46	Matti Hool	15	11	6	10	28	(DNF)	70
27	SR 42	Vello Yurji	14	12	9	8	(39)	27	70
28	G 44	Peter Koppány	10	14	8	(25)	22	(31)	70
29	SR 6	Vaiko Vooremaa	10	12	10	13	25	(DNF)	70
30	SR 35	Jael Kukk	9	17	9	11	29	(DNF)	75
31	S 44	Gutta Johansson	15	14	12	17	17	(18)	75
32	SR 1	Yuri Sharashkin	16	16	16	8	(24)	20	76
33	SR 8	Avo Luuk	12	13	13	7	33	(35)	78
34	S 5	Ake Luuk	16	6	(32)	13	18	26	79
35	SR 38	Ena Normak	7	8	19	22	(26)	24	80
36	SR 5	Elmar Talbonen	18	16	12	9	(45)	25	80
37	G 201	Harald Stuerz	(22)	22	17	16	6	21	82
38	P 36	Martin Czalbowski	16	9	21	14	(41)	32	92
39	G 601	Thomas Bergner	20	20	15	9	32	(DNF)	96
40	SR 158	Nikolay Yakovlev	11	19	11	18	37	(DNF)	96
41	D 90	Erling Jensen	19	12	11	12	47	(DNF)	101
42	SR 20	Rein Veider	19	20	15	21	27	(DNF)	102
43	SR 37	Andres Syrak	24	14	20	16	31	(37)	105
44	US 9	Michael Boston	15	9	19	12	(DNF)	(DNS)	108
45	D 112	Thomas E.Hansen	13	19	16	22	40	(DNF)	110
46	OE 39	Rudi Bauer	19	18	25	22	(49)	34	118
47	G 107	Bernd Zeiger	14	29	24	17	36	(DNF)	120
48	SR 19	Arbo Kalk	24	17	26	25	(35)	33	125
49	SR 27	Vladimir Gribov	17	32	22	21	30	(DNF)	130
50	D 92	Hans E.Hansen	12	18	26	24	50	(DNF)	132
51	SR 44	Aleksey Khibushkin	18	27	27	20	46	(DNF)	138
52	SR 108	Aleksander Bologov	20	23	23	21	(DNF)	(DNF)	140

Visvaldis Briedis SR 31 Top Senior  
Rene Koulimann SR 22 Top Junior

1988 DN World Championship results  
Bronze Fleet

Pos	Sailnbr	Name	1	2	3	4	5	Total
1	G 487	Dieter Schulz	(DNF)	15	18	18	51	
2	SR 61	Yuris Krejgers	(DNF)	24	13	18	19	63
3	H 313	Hennie van den Brink	(DNF)	21	15	29	19	63
4	SR 88	Valery Zhukov	21	22	21	(DNF)	64	
5	OE 82	Gernot Weniager	(DNF)	21	25	20	66	
6	G 700	Max Maier	(34)	23	24	20	67	
7	G 108	Peter Scheilhorn	(DNF)	30	18	14	70	
8	G 190	Christoph Schmidt	18	30	23	(38)	71	
9	SR 57	Vasily Molokov	21	31	20	22	(35)	73
10	SR 89	Vadim Bikher	21	31	21	(36)	23	75
11	G 479	Jorg Minaveck	(31)	25	22	28	75	
12	G 517	Sebastian Obermaier	31	(35)	20	25	76	
13	D 157	Henning Rasmusen	(29)	22	26	28	76	
14	S 230	Leif Dolfie	28	21	29	(37)	78	
15	SR 50	Mark Margelevich	30	(DNF)	16	33	79	
16	L 37	Timoleon Caravitis	(DNF)	33	27	19	79	
17	S 278	Tomas Wikberg	23	26	(21)	30	79	
18	SR 24	Arkady Subbotin	(DNF)	24	26	30	80	
19	H 148	Bart Reedijk	23	(33)	31	27	81	
20	G 62	Willi Gerlinger	(30)	28	28	26	82	
21	L 1	Stig-Olof Sjoberg	34	35	(DNF)	15	84	
22	US 1202	Doug Harvey	27	(DNF)	34	24	85	
23	G 521	Rainer Melenthin	22	29	35	(39)	86	
24	SR 78	Oleg Ekimov	25	24	27	(39)	35	86
25	S 400	Kjell Andersson	25	30	(33)	31	86	
26	S 401	Sture Heldri	26	31	(31)	29	86	
27	H 31	Wim van Acker	33	26	(37)	28	87	
28	Z 42	Rudolf Fredi	29	(38)	35	24	88	
29	G 286	Friedrich Liese	29	30	(33)	29	88	
30	G 300	Klaus Maier	(39)	25	37	27	89	
31	G 244	Axel Forstmann	(DNF)	32	30	27	89	
32	H 13	Elbert Meijer	32	(36)	29	32	93	
33	SR 106	Valery Kuljnichev	(36)	27	34	36	97	
34	L 20	Harri Honkanen	28	37	34	(40)	99	
35	US 2000	Harold Lebeau	31	37	(DNF)	32	100	
36	G 444	Heiner Forstmann	26	33	(44)	42	101	
37	S 60	Elmas Smiltienies	26	36	41	(42)	103	
38	L 32	Gran Jahn	(DNF)	39	30	34	103	
39	SR 137	Sergey Kononov	32	(DNF)	40	34	103	
40	S 367	Klas Barne	25	39	42	(44)	106	
41	S 413	Soren Snackerstrom	(DNF)	31	36	39	106	
42	US 3720	Thomas A.Hendrickson	40	(DNF)	30	37	107	
43	L 29	Mikke Enoksson	38	37	(DNF)	33	108	
44	H 404	Martin van Wetum	39	34	(DNF)	36	109	
45	G 336	Benkt Enoksson	39	36	(DNF)	41	33	110
46	L 9	Jan Fagerberg	28	40	43	(49)	111	
47	S 300	Jan Fagerberg	32	(DNF)	(DNS)	37	112	
48	S 472	Johan Barne	(DNF)	41	39	32	112	
49	G 257	Ulrich Kuntze	(DNF)	28	23	(DNF)	114	
50	L 28	Jarmo Wessman	(DNF)	35	45	38	118	
51	US 3580	Gentry Stone	(DNF)	43	36	40	119	

Leif Dolfie S 230 Top Senior  
Mikael Enoksson L 29 Top Junior

Chairman of Race Committee

Pos	Sailnbr	Name	1	2	3	4	5	Total
1	OE 70	Heinz Schneeweiss	35	(DNF)	(DNF)		23	121
2	H 55	Jan Lindhoven	(DNF)	38	37		46	121
3	G 180	Klaus Czrycholl	(DNF)	41		46	43	130
4	S 500	Tony Ribblas	(DNF)		34	(DNF)	24	131
5	US 467	R.Michael Rian	27	(DNF)	(DNF)		43	133
6	SR 140	Yury Astashev	37	40		(DNF)	140	
7	G 428	Erwin Ipp	(DNF)	(DNF)	35		44	142
8	OE 112	Wiedel	(DNF)	(DNF)	17		(DNF)	143
9	US 3042	Sheldon Abrams	(DNF)	34	(DNF)		48	145
10	SR 12	Victor Potapov	(DNF)	(DNF)		30	45	146
11	L 11	Raimo Honkanen	(DNF)	(DNF)	(DNF)	43	41	147
12	S 355	Anders Olson	(DNF)	23	(DNF)		(DNF)	149
13	SR 23	Vladimir Chirkov	(DNF)	(DNF)	(DNF)	(DNF)	(DNF)	151
14	US 4080	Pricc Rinard	(DNF)	(DNF)	(DNF)	39	165	
15	K 1	Chris Williams	(DNF)	(DNF)	(DNF)		40	166
16	OE 140	Renato Kasseroller	(DNF)	(DNF)	(DNF)		41	167
17	G 634	Martin Platzer	(DNF)	42	(DNF)	(DNS)		168
18	G 401	Wilhelm Kohler	(DNF)	(DNF)	(DNF)	42	(DNF)	168
19	H 312	Bert Grubbee	(DNS)	(DNF)	(DNF)		45	171
20	US 3800	Reuben R Snodgrass	(DNF)	(DNF)	(DNF)		46	172
21	US 1925	Leo Healy	(DNF)	(DNF)	(DNF)		47	173
22	G 84	Peter Markmann	(DNF)	(DNF)	(DNF)		47	173
23	SR 11	Aleksandr Batyev	(DNF)	(DNF)	(DNF)	(DNF)		189
24	SR 47	Oleg Gloskov	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
25	G 75	Jurgen Wesjley	(DNF)	(DNF)	(DNF)	(DNF)		189
26	G 383	Karl Heinz Wagner	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
27	G 449	Peter Hellwig	(DNF)	(DNF)	(DNS)	(DNS)	(DNS)	189
28	G 695	Andreas Vohl	(DNS)	(DNS)	(DNS)	(DNS)	(DNS)	189
29	G 709	Wulfgang Breitzke	(DNS)	(DNS)	(DNS)	(DNS)	(DNS)	189
30	H 20	Antreas Veclentouf	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
31	H 60	Huis Van der Zee	(DNS)	(DNS)	(DNS)	(DNF)	(DNF)	189
32	H 116	Francois Vlugt	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
33	H 199	Peter van Rooij	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
34	H 208	Jan Iellema	(DNS)	(DNS)	(DNS)	(DNS)	(DNS)	189
35	H 353	Ernest Spinks	(DNF)	(DNS)	(DNF)			189
36	OE 200	Heberst Hoermann	(DNF)	(DNF)	(DNS)	(DNS)	(DNS)	189
37	US 2346	Harold Chambershan	(DNF)	(DNF)	(DNF)	(DNF)	(DNF)	189
38	Z 1	Robert Cummins	(DNF)	(DNF)	(DNS)	(DNS)	(DNS)	189
39	Z 7	Charlie Lambert	(DNS)	(DNS)	(DNF)	(DNS)	(DNS)	189
40	SR 70	Pyrrtel Heilmer	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
41	G 114	Manfred Kramer	(DNF)	(DNF)	(DNF)	(DNS)	(DNS)	189
42	G 131	Gunnar Dusener	(DNS)	(DNS)	(DNS)	(DNS)	(DNS)	189
43	G 308	Rainer Hinrichsson	(DNS)	(DNS)	(DNS)	(DNS)	(DNS)	189
44	S 207	Peter Anderson	(DNF)	(DNF)	(DNF)	(DNF)		189
45	S 72	Birger Andersson	(DNF)	(DNF)	(DNF)	(DNF)		189
46	OE 32	Bruno Gitschofer	(DNF)	(DNF)	(DNF)	(DNF)	(DNS)	189
47	G 621	Klaus Helgauer	(DNF)	(DNF)	(DNF)	(DNF)	(DNS)	189
48	L 7	Carl Sundstrim	(DNF)	(DNF)	(DNF)	(DNF)		189
49	L 8	Klaus West	(DNF)	(DNF)	(DNF)	(DNF)	(DNS)	189
50	S 453	Peter Jansen	(DNF)	(DNF)	(DNF)	(DNF)		189
51	S 325	Hans Adulfsson	(DNF)	(DNF)	(DNF)	(DNS)		189

Jan Einthoven H 55 Top Senior

Chairman of Race Committee

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**2022 World Champion Robert Graczyk P-31**

**2022 Europeans 2<sup>nd</sup> Tomasz Zakrzewski P-55**

**2022 Polish Champion Tomasz Zakrzewski P-55** (Gen6 mast)

**2022 Polish Championship 2<sup>nd</sup> Robert Graczyk P-31**

**2022 Grand Masters Champion Tomas Lindgren S-81**

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# IDNIYRA EUROPE

# MEETING MINUTES

INTERNATIONAL DN ICE YACHT RACING ASSOCIATION EUROPE

IDNIYRA EUROPE - EISSEGELVERBAND EUROPA

MINUTES AND DECISIONS OF THE 2022 NATIONAL SECRETARIES' MEETING

HELD: ONLINE VIA SKYPE, ON THE 23RD & 24TH OF APRIL, 2022 (10.00-17.00 CEST).

## Attendance list

### Present:

#### IDNIYRA Europe Executive Board

Commodore & Insurance Manager	Attila Pataky M100
Vice-Commodore	Mihkel Kosk C45
Treasurer	Jerzy Henke P58 (present only on the 2nd day)
Junior Programme Manager	Stan Macur P111
Secretary & Webmaster	Attila Pataki, Jr. M101

#### National Secretaries

Austria	Walter Koelbl OE119
Belarus	Ivan Fershal B5
China	Qiguang Wang CH999 (present only on the 1st day)
Denmark	Georg Juelsgaard D379
Estonia	Marek Lentsius C72
Germany	Bernd Zeiger G107
Hungary	Attila Pataki, Jr. M101
Netherlands	Hennie van den Brink H313
Lithuania	Antanas Gerasimavicius T4 (present only on the 2nd day)
Poland	Rafal Sielicki P254
Sweden	Stefan Karlsson S924
Switzerland	represented by Ueli Marti Z78
United Kingdom	David Howlett K13

### Also present:

Commodore IDNIYRA-NA	Jody Kjoller US5435 (present only on the 1st day)
Vice-Commodore IDNIYRA-NA	David Frost US5358
Secretary IDNIYRA-NA	Debra Whitehorse US2366
Auditor	Peter Uhlmann OE213 (present only on the 2nd day)
Auditor	Artis Berzins O2 (present only on the 2nd day)
Technical Committee	Jeff Kent US3535
	Tomasz Zakrzewski P55 (present only on the 1st day)
EDNIA member & VC candidate	Johnny Winquist L601

**Proxy votes:**

Czechia	represented by Austria
Finland	represented by Sweden
France	represented by Estonia
Latvia	represented by Estonia
Russia	represented by Hungary

Apologies were received from Beniot Marie representative of France, Sergey Pulkov representative of Russia, Girts Fisers-Blumbergs representative of Latvia, EDNIA & Technical Committee candidates.

**Agenda:****1. Ascertainment of Quorum****2. Opening Remarks**

- a. Changes to the Agenda
- b. Season Reports

**3. Approval of the Minutes of the National Secretaries' Meetings 2021 (Online)****4. Matters Arising from Topic 3 not covered by this Agenda****5. Financial & Auditors' Reports 2021/2022**

- a. Financial Report
- b. Auditors' Report

**6. Commodore's Report on the Season: WC/EC2022 and Lessons Learned****7. Junior Programme Report****8. Technical Committee Report****9. Insurance Report****10. Media & Communications Report****11. Event Sponsorship Report****12. EDNIA Report****13. IDNIYRA EU-NA Contact Report****14. Discharge of the Board of Officers****15. Election of Class Officers:**

- IDNIYRA - Europe Officers (for a 2 year period)
  - a. Commodore
  - b. Vice-Commodore
  - c. Treasurer
- Other European Officers
  - d. Auditor 2
  - e. Technical Committee Member (1 year remaining of term)
  - f. Technical Committee Member (3 years remaining of term)
  - g. EDNIA Member (4 years remaining of term)

**16. Letters to the Board**

- a. Recall of Péter Hamrák M53 - Technical Committee & NA Governing Committee
  - i. Organisation Rule Change - Recall
  - ii. Vote to Recall
- b. Technical Specification proposal: Sails - Karol Jablonski P36
- c. Technical Specification proposals - Péter Hamrák M53
  - i. Cockpit - Floor
  - ii. Cockpit - Material
  - iii. Runner Plank - Overall Length
  - iv. Runner Plank - Material
  - v. Runner Plank - Reinforcement with Carbon
  - vi. Sail - Material

Continued next page

- vii. Mast - Minimal Weight & Balance Point
- viii. Runner - Materials
- ix. Interpretations Changes
- d. Boom Stripe - Fredrik Lindgren S8

**17. Yearbook & Runner Tracks**

**18. EC 2023**

**19. Junior WC/EC 2023**

**20. WC/NAC 2023**

**21. National Secretaries' Meeting 2023**

**22. Proposals for changes in the Governing Documents**

- a. IDNIYRA-Europe Organisation Rules
  - i. Technical Committee European Member recall
  - ii. Membership deadlines
- b. EPIC Agreement
  - i. Technical Committee Chair
  - ii. Class Voting Rules
  - iii. Technical Committee Interpretations
  - iv. Technical Specifications Enforcement

**23. Determination of contributions to the IDNIYRA - Europe and event entry fees**

- a. Membership fee
- b. Entry fee

**24. Budget 2022/2023**

**25. Other Business**

- a. DN Italy Application for Membership
- b. Perpetual Trophies

**Topic 1 Ascertainment of a Quorum (Day 1 & 2)**

It was confirmed that the invitation to the Annual meeting was issued in time and a quorum of 5 National Secretaries was present. Attendance list of the meeting was collected by the Secretary.



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Mast, Plank, Sail, Runners**

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**Topic 2 Opening remarks**

The Commodore opened the meeting with a welcome to all present.

**a, Changes to the Agenda**

Due to a variety of circumstances including resignations finalised at the last minute in the Technical Committee, possible changes in EDNIA, approved absence of presenters on the first day, and also administrative errors; adjustments to the original agenda were required: Topics 15.e,f,g & 16.d were added. A change in the order of the topics was also necessary: Topics 5, 14, 15, 18, 19, 20, 21, 23, & 24 were postponed to the 2nd day of the conference.

**The changes to the agenda of the meeting were accepted unanimously.**

**b. The National Secretaries submitted a short review of their 2021-2022 Season:**

**Austria:** *"In our country, DN sailors had only the possibility to sail in Carinthia this season. Outside of Austria, we could sail in Italy, Hungary Czech Republic, Poland, Lithuania, and last but not least Sweden and Norway during WC and EC. We were very happy to meet all our friends again after the Covid pandemic travelling restrictions from the last season! Finally, it was a successful season with a lot of fun."*

-Walter Kölbl OE119

**Belarus:** *"A very good season for us. Ice became at the base, applied at the end of December. We were able to hold the first competitions during the New Year holidays. In total, clean ice, suitable for training, stood for 57 days this year. In total this season we held 3 competitions and 6 training camps. Unfortunately, due to external problems, our athletes could not participate in the World Championships. not in the Cup of Russia which was held in St. Petersburg. Also this year we were not able to explore a promising place - Lake Naroch, located approximately 130 km from Minsk. During training and racing, a lot of people were interested in our sport. This year we have 1 new member."*

-Ivan Fershal B5

**China:** *"First of all, we are very sorry that due to the new Corona epidemic, China did not participate in WC and EC this year. Over the past year, we have recruited 43 Chinese DN members who have a genuine love for DN ice sailing. According to NIA rules and RMA, we have carried out DN ice sailing-related activities and competitions on river ice and sea ice in Jinzhou, such as DN ice sailing training camp and DN ice sailing World Cup selection certification. A total of more than 30 people participated in the event. During the event, the film crew of CCTV's "Aerial Photography China" made a special trip to the icesailing training base to follow up on the DN ice sailing training and certification activities for 4 days. The program is expected to be broadcast on CCTV-1 and CCTV-9 in August/September, and the DN iceboats will be well known to the people of China. Jinzhou, China has a very broad and flat ice surface. The average temperature in winter is around -15°C, and it is windy all year round. It is very suitable for ice sailing. We look forward to the day when we can get together and share the joy of DN ice sailing."*

- Qiguang Wang CH999 & Ming Wu CH6

**Czechia:** *"There were 2 days of opportunity with good ice at Machovo lake on 28.12. and 29.12. 7 local sailors sailed but no regatta. No more training or races were held in the Czech Republic due to the lack of ice. One sailor took part in Worlds and Europeans."*

-Vladislav Ptasnik CZ112

**Denmark:** *"Unfortunately no icesailing at all in Denmark this season."*

- Georg Juelsgaard D371

**Estonia:** *"We had a good and long season. It started in early December and shortly after that, we held the Junior WC/EC 2021. From January we started our national season with all of the competitions. We had frequent visitors from Latvia and for the Estonian Championship, we also had sailors from Finland. We mainly sailed on sea ice this season because of snow on the mainland. In March we held Junior WC/EC 2022 in good conditions. In total, we were able to hold 6 local competitions and 2 international. Overall it was a good season with lots of sailing hours."*

- Marek Lentsius C72

**Finland:** *"The 2022 season was still controlled by Covid-19. The season started in week 47 in Kemi. Johnny Köhler cup 26.11.2021. 8 sailors 3 races the ice was not perfect.*

*(1. Ossi Pajja L-139 2. Oscar Lindell L-132 3. Olli Virta L-121)*

Continued next page

All Saints 2021 27.-28.11.2021 12 sailors 9 races. Ice conditions were like the day before.

(1. Winqvist John L-601 2. David Croner S-1 3. Oscar Lindell L-132)

Independence Day regatta 4-5.12.2021 Pyhäjärvi, Forssa. 27 sailors 6 races. Ice conditions were good.

(1. Jarek Radzki P-431 2. Lukasz Zakrzewski P-155 3. Darek Kardas P-13)

Kahakka Regatta 2022 8-9.1.2022 Kustavi, Kivivesi 18 sailors 6 races.

(1. Winqvist John L-601 2. Pesola Risto L-140 3. Pettersson Mikael L-65)

Finnish champion 5-6.3.2022 Mynälahti, Turku. 30 sailors 10 races. good wind and ice.

(1. Svensson Oskar S-714 2. Hamrak Peter M-53 3. Rantanen Teemu L-137.)

Kelluva Ranking 1. 19.3.2022 Mynälahti, Turku. 12 sailors 6 races.

(1. Winqvist John L-601 2. Pettersson Mikael L-65 3. Fagerlund Sam L-69)

BSS Sunny Ranking 26-27.3.2022 Pyhäjärvi, Säskylä. 29 sailors 5 races.

(1. Jablonski Karol P-36 2. Winqvist John L-601 3. Svensson Oskar S-714.)

April fool's ranking 2.4.2022 Klobben, Espoo. 16 sailors 5 races.

(1. Pettersson Mikael L-65 2. Winqvist John L-601 3. Rantanen Teemu L-137.)

We organised two DN testing days and they were a success. There were around 20 new sailors.

From these already 3 have purchased their first DNs."

- Jerker Sundström L371

**France:** "The French Fleet doubled this year with Caroline F2 joining at the Worlds. 1 frozen lake near Geneva where the Swiss are sailing. 1 boat apparently spotted by some media in the massif central, but no idea who that is. It is getting difficult to spend so much time and money with so little sailing time on the ice. I hope next year will be better."

- Benoit Marie F1

**Germany:** "No Ice again at German Lakes by very warm Weather. Then, Corona stops Icesailing to the German Fleet heavily again. Due to the Situation, our Training Sessions in Sweden and in the South were sailed at Lake Roxen-S and at Lipno CZ. Corona made a difficult Situation getting organised, however. That stopped many Sailors. Germans were sailing Rankings, Grandmasters, Worlds, and Europeans. Our Membership is constant, it is good to see that there are activities even without Ice, and-or, in all this „Look Down“ Situation. Germany is looking forward to 2022-2023."

- Bernd Zeiger G107

**Hungary:** "Despite the mild winter, Hungary had a decent icesailing season. We discovered a lake in North-Eastern Hungary (Rakaca Reservoir) that froze around the New Year and provided 20+cms of black ice and sufficient area for training for about 10 boats simultaneously throughout January and the beginning of February. Our members also sailed on Lake Tisza for 5 days. The possibility to be able to hold clinics and training with the appropriate COVID precautions proved beneficial to our membership and 5 new & old members joined our fleet. We also made contact with a DN icesailor in Slovakia and explored icesailing possibilities in the eastern region of the country. The team's participation at this season's World & European Championship has yielded the best results yet in Hungarian Icesailing history. Our Clinics and World & European Championship results attracted significant media attention and we were also invited to this year's Budapest Boat Show to introduce icesailing to a wider audience."

- Attila Pataki, Jr. M101

#### **Latvia:**

No prior report has been submitted. Artis Berzins O31 gave a short in-person summary of the season.

**Lithuania:** "This year we had a quite good season in Lithuania, had good ice conditions in Rekyva and Elektrenai lakes for nearly two months, few international events were organised in Rekyva-Monotype XV Europeans and Polish championship and local regattas. The popularity of ice sailing is growing as well. We have a few new senior and junior sailors starting to ice sail. 5 juniors participated in the JWC. Also, our main race officer assisted in the Gold Cup which will help to improve our local regatta level. In the upcoming year, we will have more Lithuanians participating in international events."

- Antanas Gerasimavicius T4

**Netherlands:** "Last year most ice sailors were too late when we unexpectedly had a nice ice sailing period. Now everyone was well prepared, but unfortunately, we didn't get a frost period. Local ice sailing clubs such as De Robben, Westeinderplas and De Gouwzee moved then to Sweden. They were there with various



iceboats from small to large. A total of 70 iceboats were there on the ice. It is a pity that the Netherlands is getting fewer and fewer competitive sailors. Because of the bad ice winters, most sailors find the costs too high for a competition DN. But we remain optimistic."

- Hennie van den Brink H313

**Poland:** "This season was, unfortunately, warmer than the previous one. We had ice only at one place - lake Siemianówka, we managed to organise two regattas there before the ice became unsafe.

As there was no more ice in Poland we've been forced to look for possibilities to organise the Polish Championship abroad (for the first time). We found good ice conditions at lake Rekyva in Lithuania. This lake is not very far from Poland and is big enough. We were warmly welcomed by our Lithuanian colleagues, who helped us to make this event happen. It is very likely that in case there will be similar ice conditions in Poland we will have to repeat it in the coming years."

- Rafal Sielicki P254

**Russia:** "This season we have held several competitions in different parts of Russia. The season began in Novosibirsk (South Siberia). They held the Big Siberian Cup with 7 participants. In January, the Far East region had a few events in Vladivostok. They have a fleet of 8-10 ice sailors. The most successful season was in Volgograd (the central region, river Volga, and water reservoirs). The road was within 2 months. Unfortunately, the European parts of Russia have been covered by a thick layer of snow since the beginning of December. The Moscow fleet did not have any days on the ice this winter. The biggest events were held in Saint-Petersburg and on Lake Baikal. The Russian Cup in St. Petersburg was organised on the ice of the Gulf of Finland. We have done 10 races during three nice days. It had 18 participants from the 4 regions of Russia. The winner is Oleg Vasiliev, R1. The traditional Baikal Ice Sailing Week was held at the end of March. It concluded with events: the Asian Cup, the Russian Juniors Championship, the Russian Championship, and the Baikal Cup with ice sailors from three eastern regions of Russia. This year there were 23 participants in the IceOptimist championship, 5 participants in the Junior DN championship (4 girls and 1 boy), and 17 participants in the Russian Championship. The new champion is Anton Didenko, R163. This year 4 new members joined DN Russia."

- Sergey Pulkov R5

**Sweden:** "We were blessed with an early winter with nice black ice, allowing sailing from late November. In December, regular training could start and we arranged the Swedish Championships 2021 at lake Ymsen, with plenty of friends from abroad. Thanks for sailing with us! During Christmas & New Year we sailed almost every day, it helps to sail close to residential areas to promote the sport, welcome new sailors, and get older boats in decent shape (again). Early in 2022, we had some snow dumps followed by weak winds. The Swedish fleet could rest and prepare for a busy February with the Ranking regatta weekend and the Grand Master Cup at lake Öljaren. A week later, the Gold Cup started at lake Hjämaren, a few days into the Gold Cup we got big amounts of snow. Thanks to Dideric van Riemsdijk, we found ice in Norway and could complete the Gold Cup and European Championship. Tomas Lindgren, who also won the Grand Master Cup, has put in a lot of effort coaching our top juniors the last few seasons, with an impressive result: Bronze in Gold Cup, Gold in Finland Championship, Gold and Bronze at Junior WC, and Gold and Silver at Junior EC. Excellent work by Oskar Svensson and Gustav Linden! Spring arrived early, 2022 Swedish Championships had to be postponed to December after two unsuccessful attempts. We have had quite some activities at local clubs: Görvälns Isjaktsegelare recruited new members and conducted several activities for juniors. Stockholms Isjaktklubb was very active all season with more than 20 organised training sessions and 5 (!! ) local regattas at the famous Ullnasjön. Probably a record! The three clubs in Uppsala put a lot of focus on juniors, arranging Swedish Championship for IceOptimist and organising frequent Fun Races for DN's."

- Stefan Karlsson S924

**Switzerland:** "21/22 has been a decent season for those who had time to go sailing outside the weekend as the lakes in the Jura area are overcrowded with ice skaters. 'Les Rousses' and l'Abbaye were usable during January and February. Unfortunately, the larger lake, like 'Vallée de Joux' never got a chance to freeze properly and no Swiss championship could be held. The swiss sailors were also active abroad, they went sailing in Resia, a typical 'hotspot' in December. Some attended the Master cup as well. Switzerland also helped to organize the WC/EU 22 world championship in Sweden/Norway. Unfortunately, the Swiss fleet has no junior for the time being despite several attempts to organize things for them. The foreseen Swiss participation in

Continued next page

the regatta on Lake Baikal has been cancelled due to the invasion of Ukraine by the Russians. The Swiss fleet is also looking for a new secretary as Ambroise Johnson has resigned.”

- Ambroise Johnson Z88

**United Kingdom:** “As we only have 3 members we have had very little activity K11 attended the WC/EC. K13 was injured in a crash in double ice at Julita Klrka (plank broke and hull/plank fuses let go) and had to stop for the rest of the season. K14 injured his foot as the non-slip on his plank was not fit for purpose (K13 also had the same problem with the non “nonslip”) and is still recovering. I am very disappointed in the treatment of K14 by the Swedish board who have acted in an unfriendly manner towards one of our members. I believe that this incident is being investigated by senior Swedish DN sailors.”

- David Howlett K13

#### **North America:**

United States:

“DN Class members were fortunate to have racing opportunities in the 2021 - 2022 season. In early December, we started the season on Lake Christina in Minnesota with fun races at the Western Challenge. Sailing took place on Thursday, Friday, and Saturday. Saturday night brought snow, then a big breeze on Sunday sent us home early. Central Region sailors were able to train for several weeks on Walled Lake in Michigan before the U.S. Nationals. The North American Governing Committee decided to again host a non-ranking U.S. Nationals instead of a North American Championship due to the pandemic restrictions. We sailed the U.S. Nationals on Lake Senachwine near Putnam, Illinois. After some postponements, we sailed six races in Gold and Silver fleets in two days. Ron Sherry placed first in the Gold fleet, and Karen Binder won the Silver fleet. In early March, the Western Region championship was held on a new venue for the class, Clear Lake in Iowa. Competitors enjoyed two days of racing, and Chris Berger won the Gold fleet while Alex Peterson placed first in the Silver Fleet.”

- Jody Kjoller, US5435, DN North America Commodore

Canada

“Despite an alarmingly warm autumn, we experienced abundant sharp cold in central Canada from December on. Montreal sailors enjoyed many days of early scrub racing near home on new ice sheets (albeit sub-regatta size). Kingston and Bay of Quinte sailors (joined as often as possible by Montreal as well as Hamilton sailors) got in many days of good scrub racing on nearby Hay Bay, which also saw some Skeeters (from Sarnia). It was agreed that the Hay Bay launch site at Cuthill Lane and the sheet itself are large enough to allow holding a major regatta there regardless of wind direction. People from the town of Napanee a few miles due north were willing to deliver pizza as dusk approached. There was no racing in the Maritimes, nor in the Thunder Bay area. In both Quebec and Ontario, we acquired a number of new participants (including some keen racers) and many used boats happily changed hands. Tiny Frenchman’s Bay near Toronto saw a new fleet materialize ably tracked by a drone. A few dismal attempts were made to sail Kingston harbour but drifted snow made it extremely bumpy at best until it was warm enough to become unsafe. This report was written the day after all that ice went downriver (March 20th). Those of us who travelled to the American midWest for regattas were extremely glad they did so, in spite of the lingering border crossing complications due to covid risks. Every Canadian who went to the US Nats on the Illinois River qualified for Gold Fleet and all did better than ever before. Overall, not a stellar season, but quite satisfying in many ways.”

-Colin Duncan, KC 5457, Regional Commodore for Canada

**No vote needed.**

#### **Topic 3 Approval of the Minutes of National Secretaries’ Meetings held in 2021 (online).**

The Minutes of the previous meetings were made accessible to the attendees at the time of the invitation »»

[See Attachments](#)

**The Minutes of the 2021 Online National Secretaries’ Meeting were AGREED and Approved by unanimous Vote.**

#### **Topic 4 Matters Arising from Topic 3 not covered by this Agenda**

**No remarks. No vote needed**

### **Topic 5 Financial Report & Auditor's Report**

The Treasurer's Financial Report and Auditors' Reports were uploaded and accessible before the meeting »  
See Attachments.

- a. The Treasurer presented the Financial Report for 2021/2022.
- b. The Auditors confirmed the validity of the Financial Report. Remarks were made regarding the necessity of raising membership and entry fees in the future to make up for past seasons' deficit due to COVID and inflation.

**The Accounts were approved unanimously.**

### **Topic 6 Commodore's Report on the Season: WC/EC 2022 and Lessons Learned:**

*"The 2021/22 season started well for many of the national fleets. Both in the Northern countries and in Russia there was sailable ice already at the end of November, beginning of December. Unfortunately, the originally planned primary site for WC/EC 2022, Poland, had very little ice and for a short period only.*

*Since the forecast for the Southern and Mid Regions of Europe predicted the weather to be too warm the Commodore and the Secretary planned their ice scouting tour to the North. We planned to check some places in Lithuania and Estonia.*

*Lake Rekyva looked promising, the only problem was that an international ice-fishing competition was planned for the same time we intended to have our championships. Therefore we decided to drive further north and check the places in Haapsalu Bay. The ice surface was soft and dangerous and in Topu Bay the area was limited by some cracks in the middle of the bay.*

*In the meantime we had information from Diderick, that Hjalmaren lake, west of Stockholm, has a promising ice surface, thus we took our direction to Sweden. We found relatively good accommodation and office places in Vilsta camping at Eskilstuna.*

*Friday and Saturday were two days of good training and we could arrange the registration at the camping conference facility. Thanks for the great help and give-aways for the Swiss team (The Letter of Thanks see in attachment). The COVID precautions were taken seriously by everybody. There was no violation of the ID-NIYRA EUROPE COVID-19 Protocol. Finally, we had 109 checked-in sailors, who were distributed in two fleets.*

*Sunday was the first day of racing, with moderate wind, which later became stronger. Each fleet had one valid race by the end of the day.*

*The weather forecast turned worse, promising snowstorms during the night. Some sailors started packing their DNs, while others were hoping to have not too much snow and sail the next day. Unfortunately, those who packed up were right. We had a day off on Monday and started to search for alternative sites. Dideric and Fredrik drove to different places to the North and to the West. A Thank you to both of them, as well as other members of the Swedish team for their work. Finally, they found a clear spot in Norway, so we all packed up and moved to Storsjøen, a lake near Kongsvinger.*

*The next day we managed to complete the 2022 DN World Championship with 5 races in Gold Fleet and 4 races in Silver Fleet.*

*On Thursday we planned to start the 2022 DN European Championship. Unfortunately because of heavy rain and freezing during the night, double ice formed, making the racecourse unsafe. We were forced to wait two days before we could sail the EC.*

*In one day the RC managed to sail four races in each fleet, thus we had a valid European Championship.*

Continued next page

Results:

2022 DN World Champion: P- 31, Robert Graczyk,

2nd: M-53, Péter Hamrák,

3rd: S-714, Oskar Svensson

2022 DN European Champion: P- 155, Łukasz Zakrzewski,

2nd: P-55, Tomasz Zakrzewski,

3rd: P-431, Jarek Radzki

Congratulations to All!!

### **Post-event Feedback Survey**

This year we also posted the Post-event Feedback Survey, for which we received only 16 answers. It is less than last time, but it makes up some 15% of the participants ... "

To read the report and survey answers in its entirety » [See Attachments.](#)

Remarks were made from the Swedish community regarding the speed of publishing the Results by the PRC. There was some misunderstanding during the event due to the Sailing Instructions being outdated and not including the EPIC and Race Management Agreements. This has been rectified by the time of the 2022 Juniors. The Standard Sailing Instructions may need to be updated as well in the Yearbook.

**The Commodore's Report was accepted unanimously.**

### **Topic 7 Junior Programme Report**

"Junior Programme 2021-22 Season:

*Covid-19 restrictions stopped all international icesailing activities during the 2021 Winter Season.*

*The Junior Programme Manager made a considerable decision not to cancel the JWC/EC 2021 and after a consultation with the Estonian DN Fleet as the event organiser, decided to postpone the regatta till December 2021.*

*Finally, the Juniors 2021 was organised in Haapsalu, Estonia between 12-16 Dec 2021 under full Covid-19 restrictions in a very good atmosphere and cooperation of the race committee with all DN-Junior and Ice-Optimist participants and trainers.*

*The success of the regatta mentioned above was a deciding factor in organising the most important European icesailing events in 2022:*

*EC Monotype XV and the DN WC&EC 2022, still under covid restrictions.*

*DN-Junior and Ice-Optimist WC&EC 2022 were sailed according to schedule on 6-9 March 2022 in Haapsalu, Estonia with young sailors from Sweden, Poland, Lithuania and Estonia in very good ice and weather conditions and organisation. Due to the war in Ukraine some sailors from Russia, Poland and other countries decided not to participate at the last moment.*

*The situation looks more optimistic for the next year's Juniors regatta and we hope to see more countries and participants on the ice in 2023!*

*2022 Junior WC/EC Report and the Lesson Learned:*

*The Regatta was organised according to schedule 6-11 March 2022 by the Estonian DN Fleet in Haapsalu, Estonia. Favourable ice conditions and very good organisation helped to accomplish the whole sailing program (27 races in both classes) in three days.*

Dinner for all participants with a great trophy presentation and many gifts for young icesailors on evening Wednesday March 9th finished the event.

Most of the International Race Committee has acquired sufficient routine during the DN Worlds, so the whole regatta went smoothly and safely. The regattas were sailed on a very high sport level, especially in DN-Junior class.

The chemical toilette on the ice was a very good idea, as we have many girls participating in junior events. The date was 2 weeks after DN World's and such a gap seems to be necessary in future plans for junior events, as many of them want to participate in both events."

Stan Macur was applauded for organising 2 Junior World & European Championships in one season.

### **The Junior Programme Report was accepted unanimously.**

#### **Topic 8 Technical Committee Report**

A '2022 TC Chairman's Report' was received on the 21st of April.

Although its necessity was understandable, remarks were made by the Commodore regarding the report focusing solely on Peter Hamrák's case and missing the annual activity of the Technical Committee.

A short in-person season summary was given by TC Chair Jeff Kent US3535:

"... The season has been relatively quiet. A few private inquiries were received on know-hows. The TC received an official inquiry from one of the measurers on how to measure things that are clearly not visible; unfortunately this got later sidetracked by the runner plank issue.

Richard Larsson has resigned from the TC due to various reasons including wanting to spend more time sailing and with his family.

Warren Nethercote has spent considerable time reviewing sections of the runner plank specification and submitted a revision of the wording to clarify the rules. This certainly needs to be done. It's a large task to do it properly with the intention of not changing (the meaning of) the specifications, but still be very coherent and straightforward. Warren has been doing this in his own free time which is certainly appreciated.

The online TC bulletin board has worked out very well. I think long term this is going to be very useful for the regular membership to get to know what's going on on an official basis and could serve as a vehicle to better understand the rationale of specifications which - pretty much I think everyone agrees - can be rather difficult to figure out from time to time..."

TC Member Tomasz Zakrzewski P55 added:

"I would like to emphasise that this rework of the specification and clarification of the section about the runner plank is quite a big project and the idea is to rewrite it section-after-section. Focusing on one section at a time. The runner plank is only the beginning and there will be works very soon focused on the runners. This requires a very careful approach. There is no possibility to change (the meaning of) specifications, only rewriting things in a more clear and transparent way and adding maybe on top some drawings and diagrams whenever needed to make things easier to understand when it comes to building or measuring or inspecting at check-in.

The wording proposal for invasive measurements was posted on the TC bulletin board. It sparked limited interest in Europe but more across the ocean. I've received many comments and emails. It turned out to be 'collectifying' people into two camps: There are people who believe that it's a good approach, and there are people strongly opposed to the idea of the proposal. In the end I think there's quite a bit of a challenge ahead of us. The matter is very sensitive and I'm looking forward to further discussions about it.

Other than that we didn't receive any request for interpretations, only simple questions that can be easily

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answered with yes or no ...”

European Secretary Attila Pataki, Jr. M101 shared the ‘ASD Simplified Technical English booklet’ as an example on the wording in guidebooks and user manuals understandable for English as Second Language speakers. This is widely used as a standard in the Aerospace, Defence, and Naval Architecture industries.

The documents mentioned above are readable in its entirety here: [» See Attachments.](#)

### **The Technical Committee Report was accepted unanimously.**

The TC member Recall case was discussed under [»Topic 16 a.»](#) after the Technical Committee Report.

### **Topic 9 Insurance Report**

*“With the new website started this season we had a new way to submit insurance confirmations. Most of the sailors could take advantage of the online insurance check and provided the data and the copy of the confirmation in the right format.*

*I as the Insurance Manager found the new system extremely useful and easy to use.*

*Received more than 200 insurance confirmations and only a few of them had to be refused.*

*The general recurring problems were:*

- *Missing coverage for racing and training*
- *The insured sailor could not be identified (Swedish club insurances)*
- *The date format in the period of validity was ambiguous (e.g. 01/08/2022)*

*SEI was provided for the Russian and North American sailors and on an exceptional basis for some UK sailors, as well as for Lithuanian juniors. Next year only the Russian and the North American sailors will be provided by SEI. All other sailors from the European continent must have proper insurance on their own.*

*This year I started checking insurances for sailors from the national fleets, which haven’t paid their membership dues. This was made on an exceptional basis, too. In the future, only confirmation from those sailors will be accepted, whose national fleets will not be in payment delay.*

*Unfortunately, this season there were several accidents, which involved insurance claims. As of today there was no information on any problems with accepting claims by the insurance companies.”*

Thanks to the developments on the website the InsuranceID system is no longer needed. A list of SailorIDs and corresponding Insurance validity information will be posted on the website accessible to National Secretaries for regattas.

### **The Insurance Report was accepted unanimously.**

### **Topic 10 Media & Communications Report**

#### **“Website:**

*Our previous website was in dire need of redevelopment. After many years of use and multiple virus attacks, vulnerabilities required to be fixed, visuals and features needed to be updated to today’s web standards.*

*After the 2021 SM the M&C Workgroup was asked by the board to publish a design brief, tender, and assess the incoming offers for the re-development of idniyra.eu.*

*We received 2 offers of which we provided a thorough assessment to the board.*

The development was awarded to the group led by Attila Pataki, Jr. and therefore was done partially in-house by IDNIYRA-EU.

The redevelopment was carried out during the summer-autumn period with the involvement of the outside developer firm HelloWP.

The servers were moved from HostEurope (Germany) to A2Hosting (Netherlands) to ensure a safer and more cost-effective operation.

The new website debuted in November with success. There are some features that need further development.

We value all constructive feedback regarding the website.

### **Social media:**

After a year of COVID lockdowns, this season's extremely busy DN activity was visible on all social media platforms. On Facebook, we received admin access to the DN Icesailing group from Timo Siluva and are using it along with the Iceboating group and the IDNIYRA Europe page for the better reach and distribution of information. The Iceboat Marketplace (1,6K Members) and Icereport (480 Members) groups have seen significant growth and vivid activity throughout the season.

The International DN Class' YouTube channel is growing steadily. After the curation of the DN & Iceboat videos available on the platform, we are happy to provide the most comprehensive playlists of the sport from Promotional videos, through Tutorials to Iceboating History.

These playlists are available to our member fleets for further distribution on their websites and youtube channels (if interested, please contact our webmaster). We are working on providing self-made content, and encourage everybody to send us footage for editing and publishing.

We are hoping to reach 1000 subscribers by next season's events in order to be able to Livestream the races on the platform. We would be grateful if the fleets would help the growth of the channel by propagating it to their membership.

We are incredibly grateful to Tomek Zakrzewski (DN TechTalk) and Mike Madge (SailJuice) who regularly publish icesailing content with high production value on their respective channels.

Athletes of our community sailed, trained, and supported each other on and off-season from Minnesota, USA to Sakhalinsk, Russia in the DN Class' Strava club. As more & more icesailors are using activity tracking apps, this platform is proving to be a very effective way for us to know what's going on where.

### **Calendars:**

Similar to last season, we are providing access to our Online Regatta and Event Calendars for each of our member fleets. This season we received calendars from the Swedish, Polish, Russian, Finnish, Lithuanian & Hungarian Fleets.

We'd like to encourage each of our fleets to add their events to the calendars provided. These are displayed on the idniyra.eu/calendar page for better visibility and are embeddable to any website. This calendar also gives the most comprehensive view of DN Class activities around the globe.

Instructions on how to use the Google Calendar were sent out to each National Secretary in our 2021 Apr. 24th email. If you need support in adding your fleet's events, please contact [webmaster@idniyra.eu](mailto:webmaster@idniyra.eu).

If you'd like to post an invitation to an event to our news feed, please contact [secretary@idniyra.eu](mailto:secretary@idniyra.eu) with the wording of the invitation and the cover picture of the post.

### **Photography:**

Our sport continues to attract the attention of talented photographers.

Throughout the season we received inquiries from multiple media agencies. These price-offers were around €1000+/day with fixed production dates.

Kasper & Gwindon Libera's and Icarus Sports' price offers

We would like to thank all photographers who contributed to the coverage of our events.

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Gilles Morelle, Carlo Borlienghi & Jean-Pierre Ferret of TFI (Télévision Française 1) travelled to Sweden free of charge but unfortunately decided not to join us when we moved to Norway.

We also received coverage in Sweden by the local photographer Fototummen and in Norway by the newspaper Glamdalen.

Oscar Lindell of the Finnish & Anna Pataki of the Hungarian Fleet made photos documenting the Gold Cup DN World & European Championships.

Valeri Larionov and Malle-Liisa Raigla published photos of the Junior World & European Championships.

Deb Whitehorse visited the Grand Masters in Sweden and provided media coverage of the event.

In line with the PRC's request, we would like to ask for support in the purchase of 360° cameras and media equipment at the value of €2000 for the 2023 season for better scoring and to provide possible online live coverage of our events."

Tomasz Zakrzewski P55 noted that a significant step up has been made with our media presence but we still have a long way to go so that the production converts into the interest and new sailors joining.

Mihkel Kosk C45 suggested that the media equipment may also be rented or borrowed from the community to help more easily make up for past years' financial losses.

Estonia has offered to provide videos for the Class' YouTube Channel.

### **The Media & Communications Report was accepted unanimously.**

#### **Topic 11 Event Sponsorship Report**

"After the group was formed Tomasz Zakrzewski contacted 4 agencies that could potentially help us with getting in touch with the right sponsor. These agencies would do the research on our behalf, make our offer as sexy as possible to the potential sponsor, and they charge between 20-30% commission of the total sponsorship account that they are able to obtain.

Step 1. Every single request begins with a request to show the media. The potential sponsor has to be visually attracted to our discipline. They want to see what our sport can deliver in terms of their corporate image combined with our discipline. All of them require a 90 second video of action, sweat, and enjoyment.

Step 2. They ask for facts & numbers. What is the reach of our social media presence? How many websites/clicks, etc ..."

The Media & Communications WG is in the process of compiling a list of the assets of the Class and the European Organisation which may be used to display potential sponsors. We will attempt to put together a multi-tier sponsorship offer.

Since Tomasz Zakrzewski is a member of both groups and the sole member of the Event Sponsorship WG he has agreed to integrate the Event Sponsorship into the Media & Communications WG.

### **The Event Sponsorship Report was accepted unanimously.**

#### **Topic 12 EDNIA Report**

"EDNIA has received no cases during the 2021/22 season."

### **The EDNIA Report was accepted unanimously.**

#### **Topic 13 IDNIYRA EU-NA Contact Report**

"During the summer Warren Nethercote was succeeded by Jody Kjoller as the new North American Commodore, and David Frost took over the Vice-Commodore post. We would like to thank Warren for his work, his valuable insights, and his cooperation in strengthening the ties between the two organisations on both sides of the pond. We congratulated the new Board and are looking forward to working together with them for the Class.

The Proposals greenlit by the SM 2020 and reworked in 2021 along with the ones from the NA CGG have



been sent to the TC in time (by May 31st), and were posted on the Ballot in October.

Regarding the Yearbook, I worked together with Deb Whitehorse on bringing the most accurate and balanced view of the Class to the publication. We were happy to introduce Hometown and Club affiliations to the National Membership lists bringing insight to our National Fleets. More info on the collaboration is readable in the Yearbook & Runner Tracks Report.

After the 2020/21 season of lockdowns and travel restrictions - to much appreciation of our community - it was again possible to participate in international regattas.

Bernd Zeiger (G107) originally intended to compete at the US Nationals, but after the event was moved from Monona decided to visit icesailing friends in the New England region instead.

Deb came over for the Grand Masters and provided live coverage and daily reports of the event.

The 2022 Gold Cup DN World & European Championships were held with COVID precautions in place. We were pleased that North American members Chris Berger (US5166), David Frost (US5358) & Jacek Marzenski (KC5247) competed at the event and hope to see more members of the DN Community crossing the pond to take part in each other's events next year.

We would like to thank Deb for her conscientious work throughout the season."

### **The IDNIYRA EU-NA Report was accepted unanimously.**

#### **Topic 14 Discharge of the Board of Officers (Only National Secretaries vote)**

It was agreed that the Board of Officers (Commodore, Vice Commodore, Treasurer, Junior Programme Manager and Secretary) had discharged their duties according to the Constitution in a very satisfactory manner.

**This was confirmed by a unanimous vote.**

#### **Topic 15 Election of Class Officers**

The following are the results of the IDNIYRA-Europe Officer elections for a 2 year period:

- a. Commodore:** **Mihkel Kosk was elected and approved with 11 votes, (Attila Pataky received 10 votes, Abstentions: 2.)**
- b. Vice-Commodore:** **John Winqvist was elected and approved with a unanimous vote.**
- c. Treasurer:** **Jerzy Henke was re-elected and approved with a unanimous vote.**

#### **Other European Officers:**

- d. Auditor:** **Peter Uhlmann was reelected by unanimous vote for a 2 year period.**
- e. Technical Committee Member (replacement - 1 year remaining of term):**  
**David Croner S1 was elected and approved with a unanimous vote.**
- f. Technical Committee Member (replacement - 3 year remaining of term):**  
**Argo Vooremaa C36 was elected and approved with a unanimous vote.**
- g. EDNIA Member (replacement - 3 year remaining of term):**  
**Mike Petterson L65 was elected and approved with a unanimous vote.**

Outgoing Commodore Attila Pataky M100 congratulated the new Commodore-elect and all elected members:

"After 20 years of serving the Class in different positions, it is time for me to finally have fun in icesailing, step back and pass all duties to the younger generation. I congratulate Mihkel Kosk and Johnny Winqvist on their  
Continued next page

*new positions and wish them success, patience, and wisdom in their work. I hope they will bring new, fresh ideas and more young sailors to our Class.*

*I started my work in the DN Class in 2002 when the Hungarian Fleet rejoined IDNIYRA Europe after a 25 year break. I can state that it was a pleasure to work with all of you on the former and recent Boards. Thank you for your support!*

*I would also like to say thank you to the whole ice sailing community. Though sometimes my decisions were questioned by some of the sailors, I assure you that they were always driven by my willingness to serve the Class to the best of my ability.*

*I wish the new Board success and lots of energy to serve the Class. I will always support them in the future and provide advice if they require so."*

The National Secretaries applauded and thanked Attila Pataky for all his work in IDNIYRA-Europe.

Commodore-elect Mihkel Kosk C45 added:

*"The votes were counted. The race was very tight - which is a good thing - people are active. Before Attila there's already been ambassadors for DN Iceboating Class, however he leaves big shoes to fill in, and I hope to fill them. I've been in the Board now for two years, but I've been in the Class for quite a bit now. Some of us can remember me when I was a Junior. It's a long way to come and I hope to work for a better future of this class and in the spirit of what Attila intends to do: Sail & Have Fun! We should try to get the class into that direction, that we're sailing and having fun. The time on the ice is very little ... I hope to see everybody this winter and hopefully we'll have the next meeting face-to-face."*

With the approval of the Commodore-elect, the rest of the meeting was continued to be chaired by Attila Pataki M100.

### **Topic 16 Letters to the Board**

a. Recall of Péter Hamrák M53 - Technical Committee & NA Governing Committee

(Commodore Attila Pataky recused himself from the discussion and the vote of this Topic. The Topic was chaired by Mihkel Kosk C45)

*"... On March 12, 2022, a runner plank which was constructed and sold under the auspices of Peter Hamrak failed catastrophically revealing that plank was knowingly built and distributed with the clear intent to violate the specifications of the DN Class. Mr. Hamrak has also demonstrated a clear disregard for the specifications with regard to fairings on the ends of planks, and there are ongoing issues with other components manufactured and distributed by Mr. Hamrak that fail to meet the specifications. It is not reasonable to believe he had no knowledge of the clearly visible violations as well as the egregious violation that was revealed on March 12. These ongoing and consistent efforts to build, promote, and distribute, illegal DN components cause the North American Governing Committee and the Chairman of the DN Class Technical Committee to request that the National Secretaries demand the immediate resignation of Mr. Hamrak from the Technical Committee. Furthermore it is the position of the North American Governing Committee and the Chairman of the Technical Committee that runner planks built by or distributed by Peter Hamrak shall not be allowed in any IDNIYRA sanctioned events unless such plank has been demonstrated through invasive testing by the European or North American measurers or other agreed upon technical experts to contain only verifiably legal materials.*

*We believe that Mr. Hamrak has severely violated the trust placed in him by the class and that his removal from the Technical Committee and the disallowance of planks built or distributed by him is justified and demands action by the National Secretaries at their upcoming meeting April 23 - 24, 2022.*

*We thank you for your cooperation in this matter.*

Signed,  
Jody Kjoller - North American Commodore  
David Frost - North American Vice Commodore  
Deb Whitehorse - North American Secretary/Treasurer  
Robert Cummins - North American Past Commodore  
Jeff Kent - Technical Committee Chairman"

**The Board of IDNIYRA Europe exchanged multiple emails, and held meetings on the subject, including a preliminary hearing with the parties involved. They have also received many emails from our regular membership on the subject. It has found that according to the RMA & the Technical Specifications it is the responsibility of each competitor to ensure the compliance with the Official Technical Specifications of his/her equipment.**

**However, it has become clear that due to his conduct as a TC member and the recent events involving the equipment sold by him, Mr. Hamrák no longer enjoys the trust of his colleagues in the TC and the North American Governing Body.**

**This may seriously limit his ability to represent the interests of IDNIYRA-Europe and according to the TC Chairman's Report may be detrimental to the work of the TC itself:**

*"... Along with above examples, further examples of his comments when discussing within the TC such as "what difference does it make" if a specification is satisfied or not. 'It Doesn't matter attitude'... In reality, (...) make a major difference on how we uphold / refine and enforce the specifications. His addition to the TC in short has been very disappointing and unproductive to the rest of the (committee).*

*Additional proof of this questionable behaviour is the content of ballot proposals he has put forth in recent years essentially attempting to change the DN class into a Box rule type spec. As a member of the class he has every right to do so. Having demonstrated an attitude of deliberately building and selling illegal equipment into the class at the same time holding a seat on the TC is unthinkable. However, the path he has taken to get on the TC has caused nothing but issues and left us ineffective. This has caused division within the class and is possibly the cause for some of the stated criticisms lobbied against the TC in recent years.*

*Peter would be better off working behind the scenes on his quest by making legal equipment while making concise arguments towards his ultimate goals with like-minded enthusiasts to influence members. The TC is no place for this.*

*By continuing to allow Peter Hamrák on the TC as one of the 3 EU representatives, the faith and trust we are charged to uphold and manage the specifications essentially makes us an ineffective body. We simply cannot have one member who has proven numerous times that he has no regard for the class specifications. Peter has caused damage by polluting the class and its members with illegal equipment.*

*We ask the EU GC to remove P Hamrák for the TC. Until this is resolved we as the members of the TC will neither acknowledge nor entertain his statements, opinions, or actions.*

Respectfully,  
Jeff Kent TC chair  
Steve Orlebeke  
Tomek Zakrzewski  
Paul Goodwin"

## i. Organisation Rule Change - Recall

According to the existing Org. Rules it was uncertain if a Technical Committee member can be removed from the position before his/her term expires or he/she resigns. A suggestion arose to amend the [Org. Rules, Section 4.b.i. \(Duties of other European Officers - Technical Committee\)](#) to make this action clearly possible:

... **“The National Secretaries Meeting can recall a European Technical Committee member with a 2/3 majority vote effective immediately.”**

**The amendment was passed with a unanimous vote.**

## ii. Vote to Recall

**The majority of the IDNIYRA-Europe National Secretaries’ Meeting has voted to recall Mr. Hamrák from the position of European Member to the DN Class’ Technical Committee.**

**(For: 16, Against:1, Abstentions:5)**

This action takes effect immediately. A new member is to be elected to fill out the rest of his term (3 years) (>>under topic 15g.)

Additional actions may be taken by the Board.

[\(Corresponding materials »»Attached here\)](#)

(post meeting note: currently the Board has found no immediate additional actions necessary.)

## b. Technical Specification proposal: Sails - Karol Jablonski P36

**The proposal did not pass. (Yes:1, No:20, Abstentions:2)**

## c. Technical Specification proposals - Péter Hamrák M53

## i. Cockpit - Floor

**The proposal did not pass. (Yes:1, No:21, Abstentions:1)**

## ii. Cockpit - Material

**The proposal was refused unanimously.**

## iii. Runner Plank - Overall Length

**The proposal was refused unanimously.**

## iv. Runner Plank - Material

**The proposal did not pass. (Yes:3, No:20, Abstentions:0)**

## v. Runner Plank - Reinforcement with Carbon

**The proposal was refused unanimously.**

## vi.. Sail - Material

**The proposal was refused unanimously.**

## vii. Mast - Minimal Weight &amp; Balance Point

**The proposal did not pass. (Yes:2, No:22, Abstentions:1)**

## viii.Runner - Materials

**The proposal did not pass. (Yes:3, No:22, Abstentions:0)**

## ix. Interpretations Changes

**Since none of the proposals above passed, none of the interpretations are affected.**

## d. Boom Stripe - Fredrik Lindgren S8

**The proposal was accepted unanimously.** It is to be forwarded to the TC to finalise wording for the Ballot.

The original letters and supplementary materials can be read here: [»» See Attachments.](#)

**Topic 17 Yearbook & Runner Tracks**

*“The IDNIYRA Yearbook and Runner Tracks continue to be published on schedule. The IDNIYRA board is re-*

viewing the and considering rewriting the history page. We encourage class members to contribute articles to Runner Tracks. The class is grateful to the support from our advertisers:

Composite Concepts

DN-Parts.com

Econaway Abrasives

Moore Brother Company/CSI Composite Solutions

Hale Performance Coatings

Hamrak Icewise

RKR Composites

System Three Resins

Toledo Ice Yacht Club

Ullman Sails Detroit.

The class also benefits immensely from our relationship with some wonderful professional photographers whose photos help tell the story of the DN class.

Igor Bessarab, Russia

Gretchen Dorian, USA

Catherine Firmbach, USA

Connie Hartviksen, Canada

Sean Heavey, USA

Gwidon Libera, Poland

Sophia Marc-Martin, Switzerland

Oscar Lindell, Finland

Anna Pataki, Hungary"

Remarks were made regarding the order of the governing documents in the Yearbook by the Commodore.

**The report on the Yearbook & Runner Tracks was accepted unanimously and IDNIYRA-Europe congratulated and thanked Deb for all her wonderful work.**

#### **Topic 18 EC 2023**

**The 2023 European Championship will be organised by Latvia & IDNIYRA EU between February 18th & 25th on the best ice in Europe.**

**The NOR is to be published no later than 2022 Dec. 15th.**

**Regular Entry closes: Jan. 22nd . Insurance Check suspends between: 2023 Jan. 20th- 22nd.**

**The 50. Gold Cup DN World & European Championships is set to be organised by Sweden in 2024.**

#### **Topic 19 Junior WC/EC 2023**

**The 2023 Junior Championships are planned to be organised by Lithuania & IDNIYRA EU between March 4th-10th.**

**The NOR is to be published no later than 2022 Dec. 21st.**

**Regular Entry closes: Feb. 10th . Insurance Check suspends between: 2023 Feb. 8th-10th.**

**JWC 2024 was offered to be organised by Finland.**

#### **Topic 20 WC/NAC 2023**

**The 2023 Gold Cup DN World & North American Championships will be organised by the IDNIYRA and is going to be hosted by the Western Region. Regional Commodore Chris Berger is in charge of preparations.**

**On-site registration: January 21st**

**Races: January 22nd - 27th (& 28th if necessary)**

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2023 will also mark the 50th anniversary of the first Gold Cup and the 70th anniversary of the first North American Championship.

### **Topic 21 National Secretaries' Meeting 2023**

**The 2023 National Secretaries' Meeting will be held online on the 22nd & 23rd of April.**

### **Topic 22 Proposals for changes in the Governing Documents**

- a. IDNIYRA–Europe Organisation Rules
    - i. Technical Committee European Member recall
- Discussed and voted on under Topic 16.a.i
- b. Membership deadlines

The Secretary noted that the National Membership lists tend to arrive too late from the National Secretaries. An end of August deadline (as was earlier) would be preferable for the workflow of the Yearbook and for IDNIYRA–Europe as well (see [Season Schedule](#)). This would incentivise National Fleets to hold their annual meetings earlier (during the summer) and/or would alleviate time pressure in submitting their lists if their meetings are held later and would align the lists with the IDNIYRA–EU Membership by publishing in the past season's complete state.

The National Fleet IDNIYRA–EU Membership fees should be paid by a date set by the NSM.

### **Proposed wording of the Organisation Rules:**

#### **"4. Duties of other European Officers**

##### **a. National Secretary:**

- i. Represent their National organisation and individual IDNIYRA – EUROPE members from his/her country.
- ii. Attend National Secretaries Meeting. If unable to attend, a representative with written authorisation may attend as National Secretary or a proxy vote may be given to another attending National Secretary.
- iii. Assignment of National sail numbers.
- iv. To provide a National membership list annually by **August 31st** to the IDNIYRA – EUROPE Secretary.(1)
- v. Remit National annual dues to IDNIYRA – EUROPE Treasurer before the **date set by the Annual National Secretaries Meeting**. (2) Such annual due will be equal to one individual fee.
- vi. Confirm National participants in World Championship, European Championship and Europa Cup regattas.

### **Reasoning:**

1 In accordance with the wishes of the NA Organisation, we have included a separate EU Membership List in the Yearbook since the 2020/21 season. Since this EU Membership List is based on the Ballot Electorate from the "previous season" (Sept 1st state, see EPIC). It would make sense to have the National Lists reflect that state as well. This would make Debbie's and the whole editorial team's job easier as we would be able to supply them with the proper content possibly in September, and wouldn't have to wait for the individual fleets to hold their annual meetings (which is sometimes postponed until later in the season). This might also incentivise Fleets to hold their meetings and their members to pay their fees earlier, in order to be included in the National Membership List.

2 This would better reflect the practice we have had for years since the Organisation was relocated to Vienna."

**The proposal to change the Organisation Rules was accepted unanimously.**

The supplementary materials can be read here: [» See Attachments.](#)

### **b. EPIC Agreement**

Attila Pataky M101 has submitted the following proposal for changes in the EPIC Agreement:

- i. Technical Committee Chair

**“Proposed Changes:****Article VI - TECHNICAL COMMITTEE**

The DN Class Technical Committee (TC) shall consist of six members, three elected from North America and three elected from Europe. TC members shall be elected according to the respective Continental Governing Documents (CGD). One member will be elected every year, alternating between continents. North American members will be elected in even years and European members in odd years. The term of office is six years. Term of office will begin July 1, and expire June 30 of the appropriate year. Each **second** year the TC shall elect from its membership a chairperson to serve until June 30 of the following **appropriate** year. **The chairperson must be elected alternately from American and European members. If the chairperson resigns or cannot fill out his/her term, a new chairperson from the appropriate continent is to be elected by the TC to fill out the remainder of the term.** Upon resignation of a member prior to six years, a replacement member will be elected by the respective CGG to complete the unexpired term.

All decisions of the TC require 2/3 majority vote of all members. The Chairperson shall report to the CGGs on all recommendations of the TC. The TC may initiate changes in the OFFICIAL SPECIFICATIONS or OFFICIAL PLANS by proposing a change to both CGGs.

**Reasoning:**

We believe that serving one year as chairperson is not enough to carry out long-term plans. At least a two-years term must be given to the chairperson.

The European Community would like to have more even representation of their interests in the TC, therefore requires the election of the TC chairperson alternately from the American and the European members. Without examining the reason why in the past there were only NA chairpersons elected in the TC, we would like to enshrine this rotating principle in the EPIC Agreement.”

**The proposal was accepted by a majority vote. It is to be forwarded to the Class Ballot.**

(Yes: 7, No: 1, Abstentions: 15)

## ii. Class Voting Rules

**“Proposed Changes:****Article X VOTING**

The DN Class will submit a ballot to the membership, no later than Oct 1. All members having valid membership at Sept. 1 and a valid e-mail address on file will have the right to vote. The method of voting will be an electronic ballot or any other safe and reliable method. The voting will be closed 21 days after the submission. The chosen electronic voting system should be able to certify and to verify the voting results. DN Class members that are members of both the North American and European organizations may only cast one ballot.

*Passage:* To pass a proposal must get at least 2/3 yes votes. Only valid, casted votes must be counted. Abstentions count as votes that have not been cast and are therefore invalid. Any proposal which does not pass may not be resubmitted for one year.

*Effective Date:* Changes in the Official Specifications or Official Plans shall become effective May 31 unless both Governing Groups agree an earlier effective date, and that the effective date is on the ballot proposal.

**Reasoning:**

The previous wording was not clear and could have given rise to misunderstandings and controversy. The current wording is clearer and fully in line with Robert’s Rules of Order.”

**The proposal was accepted unanimously. It is to be forwarded to the Class Ballot.**

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## iii. Technical Committee Interpretations

**“Proposed Changes:****Article XI - INTERPRETATIONS OF THE OFFICIAL SPECIFICATIONS OR PLANS**

The intended meaning and the basic principles of maintaining the DN as a one-design class shall be considered in interpreting any point not covered. Specifications, however complete, cannot anticipate every possible situation that may arise. If a point is not covered, a ruling can be obtained from the any of the CGGs through the TC. The TC shall, upon the request of any member, or at the direction of either CGG, provide interpretation of the OFFICIAL SPECIFICATIONS. In interpreting any point not covered, or wording of obscure meaning, the TC shall consider the intended meaning of the specifications which is to maintain the DN ~~within reasonable limitations as a standard one-design class. The findings shall be published by both CGGs. Such interpretations shall prevail as Supplements to the OFFICIAL SPECIFICATIONS unless and until voided by a simple majority of the membership. Proposals for such a vote will be initiated in the same way a Specification Change Proposal is made.~~

~~If an interpretation changes the OFFICIAL SPECIFICATIONS the TC shall initiate a proposal to both CGGs.~~

~~Such interpretations may only be of a temporary nature and shall be submitted to a Class Vote in accordance with paragraphs VIII-X of this Document, which Class Vote shall be initiated by the TC at the next annual ballot. Accepted changes must be incorporated into the Official Specifications, while unaccepted interpretations become invalid. Interpretations that TC does not circulate for class voting will also become invalid.~~

**Reasoning:**

The growing number of interpretations makes the Official Specifications opaque and untraceable, making it difficult to fully comply with it.

The European Community has been pushing for years for a clear, transparent and comprehensible document of the Official Specifications that has not been drawn up to this day.

The original wording allows six people to unilaterally change the interpretation of the Official Specifications and leave it entirely up to them to interpret and decide whether to put these changes to a Class Vote. This contradicts the democratic principles set out elsewhere in this document.”

**The proposal did not pass. (Yes: 1, No: 13, Abstentions: 4)**

## iv. Technical Specification Enforcement

**“Proposed Changes:****Article XII - ENFORCEMENT OF THE SPECIFICATIONS**

**Each and every sailor has the full responsibility to ensure that his/her iceboat is maintained to comply with her class rules and is in full compliance with the OFFICIAL SPECIFICATIONS**

The OFFICIAL SPECIFICATIONS shall be enforced in two ways;

1. Through a protest filed by any contestant, Judge or Race Committee member at the Gold Cup, European Championship or North American Championship Regattas against any competing yacht; or,
2. By the Race Committee measuring yachts during a regatta described in the CGDs, NIA rules, or the regatta Sailing Instructions.

**Reasoning:**

There was no proper responsibility defined earlier in the Governing Documents.”



**The proposal was accepted unanimously. It is to be forwarded to the Class Ballot.**

The supplementary materials can be read here: [» See Attachments.](#)

**Topic 23 Determination of contributions to the IDNIYRA – Europe and event entry fees**

**a. It was agreed to have both the individual and national fleet membership fees of €30. A 2 year consecutive membership will be required at registration and a re-admission fee of €30 will apply in case of prior broken individual membership. Additional banking costs will be borne by the sailors.**

**National Fleet Membership fees should be paid before the next NSM. Insurance Check will only be approved for members of fleets in good standing.**

**The determination of membership fees accepted by a majority vote.**

**(Yes: 32, No: 2, Abstentions: 0)**

**b. It was agreed to have the EC entry fee of €220**

**The determination of entry fees was accepted by a majority vote.**

**(Yes: 32, No: 2, Abstentions: 0)**

Topic 24 Budget 2022/2023

» See Attachments.

**The 2022/23 Budget was approved by unanimous vote.**

**Topic 25 Other Business**

a. DN Italy Application For Membership

We received a letter from Libero Medici I-9. There is a resurgence of interest in icesailing in Italy. Their fleet has young icesailors. The Italian DN Fleet would like to rejoin IDNIYRA-Europe.

The supplementary materials can be read here: [» See Attachments.](#)

**DN Italy's application to join IDNIYRA-Europe was accepted unanimously.**

b. Perpetual Trophies

**It has been agreed that the Vice-Commodores of each continental organisation cooperate on streamlining the number of perpetual trophies and ensure that they arrive to the upcoming events.**

**The new and the outgoing Commodore thanked all the participants for joining and invited them to an in person meeting during the next event.**

**IDNIYRA-Europe wishes everybody a successful and responsible preparation for the season. Stay Safe and Think Ice!**

**Attila Pataki, Jr. M101 Secretary**

IDNIYRA Europe

**Mihkel Kosk C45 Commodore**

IDNIYRA Europe

Balatonfüred-Pärnu, 2022.

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